

AGENDA PLANNING ADVISORY COMMITTEE

Date: **Wednesday, August 20, 2025, 7:00 p.m.**
Location: **Council Chamber and Via Web Conferencing**

1. **APPROVAL OF MINUTES**

Regular Meeting – July 16, 2025

2. **ZONING APPLICATIONS**

2.1 Gibson Gardens c/o Eric Price - 501 Gibson Street

- Rezoning a portion of the subject property from Residential Zone 1 (R-1) to Multi-residential Zone Two (MR-2) and 4.9 metre lot frontage variance, to permit a 52-unit stacked townhouse and townhouse development.
- Additionally, tentative plan of subdivision to create two new R-1 lots from the subject property.

2.2 G. George Construction Ltd. - 199 Serenity Lane

- Rezoning from R-1 and R-3 to R-5, tentative plan of subdivision to create 12 townhouse lots and a 0.75m lot depth variance (for one lot) to allow construction of 12 townhouses within 3 buildings.

2.3 Fredericton Direct Charge Co-operative Ltd. - 170 Doak Road

- Amendment to terms and conditions of By-law Z-5.27 to permit a child care centre – large use for up to 163 children (existing terms and conditions permit up to 83 children).

2.4 Sonia Wilson - 214-218 McKnight Street

- Rezoning from Residential Zone One (R-1) to R-3; and,
- A 0.56 metre lot frontage variance and a 0.29 metre lot frontage variance,

to permit the subdivision of the existing semi-detached dwelling along the common party wall.

2.5 City of Fredericton - South Core Secondary Municipal Plan Adoption

- Municipal Plan Amendments, Zoning By-law Amendments, and Subdivision By-law Amendments

3. SUBDIVISION APPLICATIONS

3.1 City of Fredericton c/o Ryan Seymour - 231 Regent Street

- Tentative plan of subdivision to add a 75.6 square metre parcel of land to the Regent Street right-of-way from a portion of PID 75493858.

4. VARIANCE APPLICATIONS

4.1 S.T.A.R.S Children's Home c/o Sue Ellen English - 348 Gibson Street

- Temporary Use Variance application to permit Group Home and Office use in the Multi-Residential Zone 2 (MR-2). No additions to the existing building footprint are being proposed.
- The proposal would accommodate short-term stays for up to 12 children aged 0 to 8 under staff supervision, at a ratio of 2 children per 1 staff.
- Onsite therapeutic services (including speech therapy and education assessments) would be provided for the children in the home, and the proposed Office use would be exclusively for the practitioners that provide services to the children.

4.2 Creative Childcare in Pepper Creek Inc c/o Javed Khan - 148 Mataya Drive

- Similar Use Variance application to add 15 more childcare spaces to an existing commercial daycare in the Residential Zone under the Noonan Lower St Mary's Rural Plan.
- In 2014, a Similar Use Variance was granted to permit a commercial daycare, and it is currently licensed for childcare for up to 60 children. With the increased demand for childcare spaces, the applicant is seeking permission to allow for a total of 75 childcare spaces.

5. OLD BUSINESS

6. NEW BUSINESS

7. BUILDING PERMITS

To receive building permits for July 2025

8. ADJOURNMENT

ORDRE DU JOUR COMITÉ CONSULTATIF SUR L'URBANISME

Date : **le mercredi 20 août 2025, 19 h 00**

Endroit : **salle du conseil municipal et participation via conférence web**

1. **ADOPTION DE PROCÈS-VERBAL**

Séance ordinaire – 16 juillet 2025

2. **DEMANDES DE ZONAGE**

2.1 Gibson Gardens a/s de Eric Price - 501, rue Gibson

- Modification du zonage d'une portion de la propriété visée, passant de Zone résidentielle 1 (R-1) à Zone multirésidentielle 2 (MR-2) et dérogation visant à réduire la façade minimale du lot à 4,9 mètres, afin de permettre un projet de 52 logements en rangée superposés et en rangée.
- De plus, un plan provisoire de lotissement est proposé afin de créer deux nouveaux lots R-1 à partir de la propriété visée.

2.2 G. George Construction Ltd. - 199, allée Serenity

- Modifier le zonage pour le faire passer des Zones R-1 et R-3 à la Zone R-5, approuver un plan provisoire de lotissement afin de créer 12 lots destinés à des maisons en rangée, accorder une dérogation visant à réduire la profondeur minimale d'un lot de 0,75 mètre pour permettre la construction de 12 maisons en rangée réparties dans trois bâtiments.

2.3 Fredericton Direct Charge Co-operative Ltd. - 170, chemin Doak

- Modification des conditions de l'arrêté Z-5.27 afin d'autoriser l'exploitation d'une garderie de grande taille pouvant accueillir jusqu'à 163 enfants (les conditions actuelles permettent un maximum de 83 enfants).

2.4 Sonia Wilson - 214-218, rue McKnight

- Changement de zonage de Zone résidentielle 1 (R-1) à Zone R-3;
- Une dérogation de 0,56 mètre et de 0,29 mètre aux largeurs minimales de façade du lot,

afin de permettre le lotissement du bâtiment jumelé existant le long du mur mitoyen.

2.5 Ville de Fredericton - L'Adoption d'un Plan Municipal Secondaire de Noyau Sud

- Modifications au plan municipal, modifications au règlement de zonage et modifications au règlement de lotissement

3. DEMANDE DE LOTISSEMENT

3.1 Ville de Fredericton a/s de Ryan Seymour - 231, rue Regent

- Plan provisoire de lotissement visant à ajouter une parcelle de 75,6 mètres carrés à l'emprise de la rue Regent, à partir d'une portion de la propriété portant le NID 75493858.

4. DEMANDE DE DÉROGATION

4.1 S.T.A.R.S Children's Home a/s de Sue Ellen English - 348, rue Gibson

- Demande de dérogation pour usage temporaire afin de permettre l'usage de foyer de groupe et de bureau dans la Zone multirésidentielle 2 (MR-2). Aucune modification à l'empreinte du bâtiment existant n'est proposée.
- Le projet vise à offrir un hébergement temporaire à un maximum de 12 enfants âgés de 0 à 8 ans, sous la supervision du personnel, à raison de deux enfants par membre du personnel.
- Des services thérapeutiques sur place (notamment en orthophonie et en évaluation pédagogique) seraient offerts aux enfants hébergés, et l'usage de bureau proposé serait réservé exclusivement aux professionnels fournissant ces services.

4.2 Creative Childcare in Pepper Creek Inc a/s de Javed Khan - 148, promenade Mataya

- Demande de dérogation pour usage similaire visant à ajouter 15 places additionnelles dans une garderie commerciale existante située en zone résidentielle, conformément au Plan rural de Noonan Lower St. Mary's.
- En 2014, une dérogation pour usage similaire avait été accordée afin de permettre l'exploitation d'une garderie commerciale, actuellement autorisée à accueillir jusqu'à 60 enfants. Compte tenu de la demande croissante de places en garderie, la présente demande vise à porter ce nombre à 75.

5. AFFAIRE COURANTE

6. AFFAIRE NOUVELLE

7. PERMIS DE CONSTRUIRE

Recevoir les permis de construire pour le mois de juillet 2025

8. LEVÉE DE LA SÉANCE

MINUTES OF A PLANNING ADVISORY COMMITTEE

Date: Wednesday, July 16, 2025, 7:00 p.m.
Location: Council Chamber and Via Web Conferencing

Members: Rodney Blanchard
Neill McKay
Councillor Greg Ericson
Councillor Margo Sheppard
Councillor Mark Peters
Melissa Dawe
Anna Patterson
Scott McConaghy
Julie Baker

Staff: Marcello Battilana
Fredrick VanRooyen
Melisa Tang Choy
Jill Durling
Tyson Aubie
Annick Blizzard
Marley McLellan
Dane Frenette
Ryan Seymour
Felix McCarthy
Elizabeth Murray

APPROVAL OF MINUTES

Regular Meeting – June 18, 2025

Moved by: Julie Baker

Seconded by: Councillor Margo Sheppard

That the minutes of the regular meeting of the Planning Advisory Committee of June 18, 2025, be approved.

CARRIED

ZONING APPLICATIONS

Bella Properties Inc. - 224 York Street

- Rezone the subject property from Institutional Zone One (I-1) to Comprehensive Development District (CDD) Zone to permit the conversion of the existing place of worship into an event facility and boutique hotel.

Public Hearing

Gabriel Elzayat, owner of Bella Properties, addressed concerns about parking, noise, and hours of operation, stressing his commitment to maintaining the building's historical value while enhancing the downtown area.

Greg Gillis, president of the Lofts on York Condominium Corporation, emphasized the importance of containing any noise-making activities within the building's walls to prevent disturbances to the neighborhood and asked for clarity on the size, timing, and nature of events in the refurbished facility, as well as how these would be controlled and insured.

Richard Burns expressed concerns about commercial encroachment into residential Town Plat and emphasized the importance of maintaining a vibrant downtown residential neighborhood to support the business district. He appreciated the owner's commitment to the property but highlighted the need for noise control, restricted hours of operation, and indoor events only. Mr. Burns suggested including these conditions in Section 59 of the *Community Planning Act*.

Diane Kent-Gillis referred to an earlier incident where the noise Bylaw was used as a legal instrument to address noise issues with a downtown club. Ms. Kent-Gillis was told by City police, Bylaw Enforcement, and the Community Safety Services Unit that it did not apply as it was a use approved by the city.

The Chair allowed Gabriel Elzayat to provide additional comments. Mr. Elzayat assured the committee of his commitment to operating responsibly and respecting neighbors, he has a long-term interest in the area as there will be tenants living in the building.

Marcello Battilana spoke to the noise bylaw concerns. By-law Enforcement is responsible for regulating the noise bylaw. Mr. Battilana confirmed that the noise bylaw is applicable, with a tolerance level around 11 pm, adding that exemptions are typically granted by council for specific circumstances such as facilitating construction.

Moved by: Scott McConaghy

Seconded by: Councillor Greg Ericson

BE IT RESOLVED THAT at the meeting of the Planning Advisory Committee on July 16, 2025, the application submitted by Bella Commercial Properties Inc. on property located at 224 York Street to rezone the property from Institutional Zone One (I-1) to Comprehensive Development District Zone (CDD) to permit the conversion of the former place of worship into an event facility and boutique hotel, be approved subject to terms and conditions.

CARRIED

Bella Properties Inc. – 984 Prospect Street

- Zone amendment to permit a place of worship use in the Commercial Corridor Zone Two (COR-2) Zone on property located at 984 Prospect Street.

Public Hearing

The application Gabriel Elzayat was available to respond to questions.

Moved by: Neill McKay

Seconded by: Anna Patterson

BE IT RESOLVED THAT at the meeting of the Planning Advisory Committee on July 16, 2025, the application submitted by Bella Commercial Properties Inc. c/o Gabriel Elzayat, on behalf of the Redeemed Christian Church of God, at 984 Prospect Street for a zone amendment to permit a place of worship within the Commercial Corridor Zone Two (COR-2), be approved subject to terms and conditions.

CARRIED

SUBDIVISION APPLICATIONS

City of Fredericton – 435 Brookside Drive

- Tentative plan of subdivision to add 104.7 sqm of land to the public Right-of-Way; and,
- Variance to reduce the required landscaped buffer between the parking lot and the public street,

to permit upgrades to the multi-use trail system along Brookside Drive.

Public Hearing

The application Ryan Seymour was available to respond to questions.

Moved by: Councillor Margo Sheppard

Seconded by: Neill McKay

BE IT RESOLVED THAT at the meeting of the Planning Advisory Committee on July 16, 2025, the application submitted by Ryan Seymour, on behalf of the City of Fredericton, for a variance to reduce the required landscaped buffer between the parking lot and the public street to 0 metres as shown on Map III on property located at 435 Brookside Drive, and for a tentative plan of subdivision to add 104.7 square metres of land to the Brookside Drive right-of-way from two portions of PID 01488261 with a recommendation that the location of the public street as shown on Map II, be approved.

BE IT FURTHER RESOLVED THAT the final plan of subdivision prepared by Surtek Group Ltd. entitled Dalin Investments Limited Subdivision 2025-1 Creating Brookside Drive (Public), Brookside Drive & Reynolds Street, City of Fredericton, County of York, Province of New Brunswick, receive the Assent of Council pursuant to Section 88(4) of the *Community Planning Act*.

CARRIED

VARIANCE APPLICATIONS

Hossack Ventures Ltd. - 235 Bishop Drive

- Conditional use variance at 235 Bishop Drive to permit Vehicle Sales in the COR-2 zone.

Moved by: Scott McConaghy

Seconded by: Julie Baker

BE IT RESOLVED THAT at the meeting of the Planning Advisory Committee on July 16, 2025, the application submitted by Hossack Ventures Ltd at 235 Bishop Drive for a conditional use variance to permit Vehicle Sales in the Commercial Corridor Zone Two (COR-2), be approved subject to terms and conditions.

CARRIED

OLD BUSINESS

NEW BUSINESS

BUILDING PERMITS

To receive building permits for June 2025

Moved by: Councillor Greg Ericson

Seconded by: Julie Baker

That the building permits for the month of June 2025 be received.

CARRIED

ADJOURNMENT

Moved by: Neill McKay

Seconded by: Julie Baker

That the meeting of the PAC be adjourned at 7:55 PM.

CARRIED

Rodney Blanchard, Chairman

Elizabeth Murray, Secretary

PROCÈS-VERBAL D'UNE RÉUNION DU COMITÉ CONSULTATIF SUR L'URBANISME

Date : mercredi 16 juillet 2025 à 19 h
Lieu : salle du conseil municipal et conférence en ligne

Membres : Rodney Blanchard
Neill McKay
Greg Ericson
Margo Sheppard
Mark Peters
Melissa Dawe
Anna Patterson
Scott McConaghy
Julie Baker

Personnel : Marcello Battilana
Fredrick VanRooyen
Melisa Tang Choy
Jill Durling
Tyson Aubie
Annick Blizzard
Marley McLellan
Dane Frenette
Ryan Seymour
Felix McCarthy
Elizabeth Murray

APPROBATION DU PROCÈS-VERBAL

Réunion ordinaire – 18 juin 2025

Proposé par : Julie Baker

Appuyé par : Margo Sheppard

Approuver le procès-verbal de la réunion ordinaire du Comité consultatif sur l'urbanisme tenue le 18 juin 2025.

ADOPTÉ

DEMANDES DE ZONAGE

Bella Properties Inc. – 224, rue York

- Changer le zonage du terrain visé de Zone de services collectifs 1 (I-1) à Zone d'aménagement intégré (CDD) afin de permettre la conversion du lieu de culte existant en salle d'événements et en hôtel-boutique.

Audience publique

Gabriel Elzayat, propriétaire de Bella Properties, aborde les préoccupations liées au stationnement, au bruit et aux heures d'ouverture, tout en réaffirmant son engagement à préserver la valeur historique de l'immeuble et à contribuer au dynamisme du centre-ville.

Greg Gillis, président de la Lofts on York Condominium Corporation, souligne l'importance de limiter toute activité générant du bruit à l'intérieur des murs de l'édifice afin d'éviter de déranger le voisinage. Il demande également des précisions sur la taille, l'horaire et la nature des événements qui auront lieu dans l'installation rénovée, ainsi que sur les mesures prévues pour les encadrer et les assurer.

Richard Burns exprime ses inquiétudes au sujet de l'empiètement commercial sur le secteur résidentiel et souligne l'importance de préserver un quartier résidentiel dynamique au centre-ville afin de favoriser la vitalité du secteur commercial. Il se dit reconnaissant de l'engagement du propriétaire envers l'immeuble, mais rappelle la nécessité de mesures de contrôle du bruit, de restrictions quant aux heures d'ouverture et de la tenue exclusive d'événements à l'intérieur. M. Burns suggère d'inclure ces conditions à l'article 59 de la *Loi sur l'urbanisme*.

Diane Kent-Gillis fait référence à un incident antérieur où l'arrêté municipal sur le bruit a été utilisé comme outil juridique pour régler des problèmes avec un club du centre-ville. Mme Kent-Gillis précise que la police municipale, le service d'application des arrêtés et l'Unité de services de sécurité communautaire lui ont indiqué que l'arrêté ne s'appliquait pas parce qu'il s'agissait d'un usage approuvé par la Ville.

Le président donne la parole à nouveau à Gabriel Elzayat pour des commentaires supplémentaires. M. Elzayat assure le comité de son engagement à exploiter l'établissement de façon responsable et à respecter les voisins, précisant qu'il a un intérêt à long terme dans le secteur, puisque des locataires vivront dans l'immeuble.

Marcello Battilana prend la parole au sujet de l'arrêté sur le bruit. Il explique que le service d'application des arrêtés est responsable de sa mise en œuvre. M. Battilana confirme que l'arrêté sur le bruit s'applique, avec un seuil de tolérance autour de 23 h, et précise que des exemptions sont généralement accordées par le conseil municipal dans des cas particuliers, comme pour faciliter des travaux de construction.

Proposé par : Scott McConaghy

Appuyé par : Greg Ericson

IL EST RÉSOLU, à la réunion du Comité consultatif sur l'urbanisme du 16 juillet 2025, que la demande présentée par Bella Commercial Properties Inc. visant le terrain situé au 224, rue York, afin de changer le zonage Zone de services collectifs 1 (I-1) à Zone d'aménagement intégré (CDD) pour permettre la conversion de l'ancien lieu de culte en salle d'événements et en hôtel-boutique, soit approuvée sous réserve de conditions.

ADOPTÉ

Bella Properties Inc. – 984, rue Prospect

- Modification de zonage afin de permettre l'usage de lieu de culte dans la Zone de corridor commercial 2 (COR-2) sur le terrain situé au 984, rue Prospect.

Audience publique

Le demandeur, Gabriel Elzayat, est présent pour répondre aux questions.

Proposé par : Neill McKay

Appuyé par : Anna Patterson

IL EST RÉSOLU, à la réunion du Comité consultatif sur l'urbanisme du 16 juillet 2025, que la demande présentée par Bella Commercial Properties Inc., par l'entremise de Gabriel Elzayat, au nom de la Redeemed Christian Church of God, visant le 984, rue Prospect, pour une modification de zonage afin de permettre un lieu de culte dans la Zone de corridor commercial 2 (COR-2), soit approuvée sous réserve de conditions.

ADOPTÉ

DEMANDES DE LOTISSEMENT

Ville de Fredericton – 435, promenade Brookside

- Plan provisoire de lotissement visant à ajouter 104,7 m² de terrain à l'emprise publique;
- Dérogation visant à réduire la bande paysagée exigée entre le stationnement et la voie publique,

afin de permettre l'amélioration du réseau de sentiers polyvalents le long de la promenade Brookside.

Audience publique

Le demandeur, Ryan Seymour, est présent pour répondre aux questions.

Proposé par : Margo Sheppard

Appuyé par : Neill McKay

IL EST RÉSOLU, à la réunion du Comité consultatif sur l'urbanisme du 16 juillet 2025, QUE la demande présentée par Ryan Seymour, au nom de la Ville de Fredericton, visant une dérogation pour réduire à 0 mètre la bande paysagée exigée entre le stationnement et la voie publique, tel qu'indiqué sur le plan III, sur le terrain situé au 435, promenade Brookside, ainsi qu'un plan provisoire de lotissement pour ajouter 104,7 m² de terrain à l'emprise de la promenade Brookside à partir de deux portions du NID 01488261, avec la recommandation que l'emplacement de la voie publique indiqué sur le plan II soit approuvé.

QUE le plan définitif de lotissement préparé par Surtek Group Ltd., intitulé *Dalin Investments Limited Subdivision 2025-1 Creating Brookside Drive (Public), Brookside Drive & Reynolds Street, City of Fredericton, County of York, Province of New Brunswick*, reçoive l'agrément du conseil municipal conformément à l'article 88(4) de la *Loi sur l'urbanisme*.

ADOPTÉ

DEMANDE DE DÉROGATION

Hossack Ventures Ltd. – 235, promenade Bishop

- Dérogation d'usage conditionnel au 235, promenade Bishop afin de permettre la vente de véhicules dans la zone COR-2

Proposé par : Scott McConaghy

Appuyé par : Julie Baker

IL EST RÉSOLU, à la réunion du Comité consultatif sur l'urbanisme du 16 juillet 2025, QUE la demande présentée par Hossack Ventures Ltd visant le 235, promenade Bishop, pour une dérogation d'usage conditionnel afin de permettre la vente de véhicules dans la Zone de corridor commercial 2 (COR-2), soit approuvée sous réserve de conditions.

ADOPTÉ

AFFAIRE COURANTE

AFFAIRE NOUVELLE

PERMIS DE CONSTRUIRE

Recevoir les permis de construire émis au cours du mois de juin 2025

Proposé par : Greg Ericson

Appuyé par : Julie Baker

Recevoir les permis de construire pour le mois de juin 2025.

ADOPTÉ

LEVÉE DE LA SÉANCE

Proposé par : Neill McKay

Appuyé par : Julie Baker

Procéder à la levée de la réunion régulière du Comité consultatif sur l'urbanisme à 19 h 55.

ADOPTÉ

Rodney Blanchard, président

Elizabeth Murray, secrétaire

To: Planning Advisory Committee

From: Melisa Tang Choy, Planner

Proposal: Rezoning to MR-2 and lot frontage variance to permit a 52-unit stacked townhouse and townhouse development; and tentative plan of subdivision to create two new R-1 lots.

Property: **501 Gibson Street (PID 01427780)**

OWNER: Peter J. Adams
905 Springhill Road
Fredericton, NB E3C 1R4

APPLICANT: Gibson Gardens (c/o Eric Price)
30 Hughes Street Unit 2
Fredericton, NB E3A 2W3

SITE INFORMATION:

Location: Large interior lot parcel off Gibson Street, between Ashfield Street and Irvine Street

Context: Mostly low-rise residential neighbourhood, with some townhouse developments to the east and to the west

Ward No: 6

Municipal Plan: Established Neighbourhoods

Zoning: Residential Zone One (R-1)

Existing Land Use: Vacant land

Previous Applications: P.R. 84/23

EXECUTIVE SUMMARY:

The Applicant is proposing a 52-unit stacked townhouse and townhouse development comprising 5 building lots on a portion of 501 Gibson Street, as well as the creation of two new Residential Zone One (R-1) lots fronting Gibson Street. The property is subject to a purchase and sale agreement with the current owner, pending planning approval. The proposal would require the rezoning of a portion of the subject property from R-1 to Multi-Residential Zone Two (MR-2). The 52-unit development will be comprised of one-bedroom units (4 of which will be accessible), with the intent of making 26 of the units affordable. A treed buffer is being proposed along the shared property lines with the neighbouring R-1 lots in order to screen the development, and an internal sidewalk system would

provide pedestrian connectivity to Gibson Street. The Applicant is also proposing to create two new R-1 lots on a portion of the property fronting Gibson Street, which would result in a 4.9 metre lot frontage deficiency for the MR-2 lot.

In 2023, Council denied an 88-unit stacked townhouse and townhouse development proposal, which included a portion of Irvine Street Park. This new proposal does not include any parkland while still making an efficient use of the land that is sensitive to the low-rise residential character of the area and provides options for affordable housing. Staff support the application subject to terms and conditions.

APPLICATION:

Eric Price, on behalf of Gibson Gardens, has made application for the following:

- Rezoning a portion of land from R-1 to MR-2, and
- 4.9 metre lot frontage variance, to permit a 52-unit stacked townhouse and townhouse development; and,
- a tentative plan of subdivision to permit the creation of two new R-1 lots,

on land located at 501 Gibson Street.

PLANNING COMMENTS:

Background:

- In 2023, a proposal was presented for an 88-unit stacked townhouse and townhouse development on the subject property, which contained 26 affordable housing units. Buildings ranged between 2 and 3 storeys, with the 2-storey buildings located closer to the neighbouring R-1 lots. In a land trade with the City that was contingent on planning approval, the proposal saw a portion of Irvine Street Park being used to accommodate the multi-residential development. In exchange, the City would receive a portion of 501 Gibson Street that included the existing watercourse on the western side of the property and would provide better access to the stormwater infrastructure and provide future trail connectivity from Gibson Street. The sale of the subject property and of the portion of the parkland were both contingent on planning approval.
- During the public hearing process, concerns were raised regarding the use of parkland for residential development, the proposed density and compatibility with the neighbouring R-1 properties. The Planning Advisory Committee recommended the proposal be denied and Council ultimately denied the application.

Proposal:

- The Applicant is proposing a 52-unit stacked townhouse and townhouse development, and the creation of 2 new R-1 lots. As shown on the Concept Plan (Map II), the R-1 lots would be located at the front of the property and would be subdivided from the multi-residential portion (see Map III). The multi-residential development would be located at the rear of the property and would be comprised of 4 stacked townhouse buildings and 1 townhouse building (see Maps IV, V and VI). Each of the 2.5-storey stacked townhouse buildings would be modular and contain 12 one-bedroom units, and the 1-storey

townhouse building would contain 4 accessible one-bedroom units that would be built onsite (see Maps VII and VIII). In order to permit the stacked townhouses and townhouses, a rezoning to the MR-2 zone is being requested.

- The Applicant intends to provide 50% of the units as affordable housing and has applied for funding from the Province, as well as from the City under the Housing Accelerator Fund. In order to accelerate the building process, the Applicant has noted that he intends to use modular housing construction methods for the stacked townhouses, which would be built off-site. The townhouses will be built onsite, as they will be built on slab.
- Regarding the overall multi-residential development, as shown on Map II, an internal sidewalk system would connect the multi-residential buildings with an outdoor common space and Gibson Street. A minimum 3-metre tree buffer is being proposed along the shared property lines with the neighbouring R-1 lots in order to provide visual screening. A total of 58 parking spaces are being proposed for the overall development, and bike racks will be provided at each of the stacked townhouse buildings.
- A watercourse is located along the western side of the property. Any development or disturbance to the ground within 30 metres of a watercourse is subject to a Wetland and Watercourse Alteration (WAWA) permit from the NB Department of Environment and Local Government.

Municipal Plan:

The subject property is designated Established Neighbourhood. The Municipal Plan contains the following relevant policies for the Established Neighbourhood designation:

Section 2.2. 1(17): Lands within the Established Neighbourhoods Designation may include a full range of residential dwelling types, community facilities, parks and open space, institutional uses and neighbourhood-supporting uses intended to serve local residents, such as local retail and service commercial uses.

Section 2.2. 1(18): The City shall support the stability of Established Neighbourhood by:

- Encouraging the maintenance of the existing housing stock;*
- Discouraging the encroachment of incompatible uses;*
- Routing higher volume traffic along arterial and collector roads;*
- Maintaining community services and facilities at a scale appropriate for the neighbourhood;*
- Encouraging the relocation of existing compatible uses;*
- Enforcing by-laws to ensure acceptable maintenance and occupancy standards; and,*
- Requiring that new or infill development be compatible with adjacent properties.*

Section 2.2.1(21): To maintain the stability of residential neighbourhoods, while allowing for incremental change through sensitive new development and redevelopment, new development will respect and reinforce the existing pattern, scale, and character of the Established Neighbourhoods, by ensuring that:

- Any new lots are consistent with the lot pattern in the neighbourhood;*
- Building design is compatible with the surrounding area and contributes positively to the neighbourhood;*
- Adequate servicing, road infrastructure, and other municipal services be readily and efficiently provided; and*

- iv. *Healthy, mature trees are protected whenever feasible.*

Section 2.2. 1(22): Infill development should be appropriately scaled and oriented with the primary entrance facing the public street.

The proposed development is a low-rise residential infill opportunity that includes a significant number of affordable units and that provides variety in dwelling types on a minor arterial road that is serviced by public transit (route 16N/120). Building heights have been sensitively considered to ensure that the with townhouse dwelling types that are compatible with the neighbourhood, and a treed buffer provides screening from the neighbouring R-1 lots. While the general area is composed of single-detached dwellings, there already are low-rise multi-residential developments within one block of the subject property. The neighbouring properties are generally 1 to 2 storeys in height, and the proposed buildings are up to 2.5 storeys.

The proposed development is supported by several community amenities, which includes Irvine Street Park, transit service along Gibson and Irvine Street, schools, and the nearby Nashwaak Trail. Site design has been carefully considered to create a functional internal pedestrian network that will be able to connect with the surrounding active transportation infrastructure and also provide an onsite amenity to its residents.

Regarding the new R-1 lots, in March 2025, Council approved changes to the Zoning By-law that merged the Residential Zone One (R-1), Residential Zone One Narrow (R-1N) and Residential Zone Two (R-2) into a new R-1 zone, allowing for a wider variety of lot sizes. With regards to the two new R-1 lots proposed, the lot frontage of properties along this portion of Gibson Street vary, with narrower lots located to the east and west of the subject property, as well as the presence of double lots across the street.

Rezoning from R-1 to MR-2:

The proposal complies with the standards of the MR-2 zone as follows:

Standard	Required	Provided	Variance
Density (max.)	56 dwelling units*	52 dwelling units	n/a
Lot Frontage	34 m	29.1 m	4.9 m
Lot Depth	30 m	~104 m	n/a
Lot Coverage (max.)	35% of total lot area (3,097.5 m ²)	~12.9% (~1,142.5 m ²)	n/a
Parking (min.)	52 spaces (1 space per unit)	58 spaces	n/a
Landscaping	35% of total lot area	57% of total lot area	n/a
Building Height (max.)	14 m	8.1 m	n/a
Building Setbacks (min.)			
Front yard	6 m	39 m	n/a
Side yard (facing Park zone)	4 m	7.5 m	n/a
Side yard (facing R-1 zone)	7.5 m	7.5 m	n/a
Rear yard	7.5 m	7.5 m	n/a

*Section 4.1(4) of the Zoning By-law regarding steep slopes notes that "any part of a lot in which grade is sloped 15% or more shall not be considered as part of the lot for the purposes of calculating minimum lot area". When applied, this would translate to approximately 500 m² of the 9,350 m² of the MR-2 lot.

- Staff would note that, from a zoning perspective, the viability of this project is not contingent on the approval of funding for affordable housing. With the exception of the lot frontage, the proposal is able to meet the standards of the MR-2 zone without the application of parking reductions or any density bonusing for affordable housing.

Lot Frontage Variance

- A 4.9-metre lot frontage variance is being requested for the proposed MR-2 portion, in order to accommodate the creation of two new R-1 lots fronting Gibson Street. The subject property is U-shaped, and the majority of the buildable area is located in the interior of the lot. Access to the property from Gibson Street can only take place from the eastern side of the property, as a watercourse runs along the western side. The minimum driveway width requirement for a two-way operation is 6 metres, and the Applicant is proposing an 8-metre driveway that is narrowed to 6-metres as it gets closer to the parking lot. Staff have no concerns with the proposed variance, as proper access from Gibson Street can still be maintained.

Building Design & Layout

- Since there will be multiple buildings on site, the Zoning By-law requires that no more than two buildings appear the same with regard to overall design and colour. Due to the modular nature of the stacked townhouse buildings, the building façade colour will be the primary method for variation in design.
- Staff would note that the proposed location of the garbage and a portion of the sidewalk on the western side of the property are located within 15 metres from the watercourse and might require revision, pending WAWA permit approval. Staff are not concerned by a potential relocation, as there is an excess of parking spaces provided.

Parking & Landscaping

- A total of 58 parking spaces are being proposed, with 14 of the spaces being barrier free, which exceeds the minimum parking requirements. A total of 16 bicycle parking spaces will be required, and Staff will continue to work with the Applicant on their final location and design to meet the standards in the Zoning By-law.
- To visually screen the development from the neighbouring R-1 properties, the Applicant is proposing a 3-metre tree buffer along the shared property lines.

Tentative Plan of Subdivision:

- The proposal complies with the standards of the R-1 zone (narrow lot option) as follows:

Standard	Required	Provided	Variance
Lot Frontage	11.5 m	13 m	n/a
Lot Depth	30 m	30.5 m	n/a
Lot Area	345 m	397 m ²	n/a

- As shown in the table above, the proposed R-1 lots would meet the lot standards of the zone, and no additional variances are being requested.

Public land dedication

- Public land dedication is required for the two new R-1 lots. Staff recommend this be taken as 8% cash-in-lieu.

Engineering & Operations:

- Following approval of this application, Engineering & Operations staff along with planning staff will meet with the applicant and their consultant to review access, lot grading, sanitary servicing, water servicing, and storm water management to ensure that there are no issues as design progresses. The following comments are based on the conceptual site layout provided by the Developer and reviewed by Engineering & Operations for the proposed development.
- The two lots and the larger development would each require a design plan stamped by a professional engineer for site services and lot grading.

Traffic

- Transportation staff have reviewed the application and the traffic generated by the proposed development is well below the available capacity of the adjacent road network. Gibson Street prior to the construction of the route 8 bypass was designed as a highway segment that carried considerably more traffic volume than it does today. Given this the volume of traffic generated from the proposed development does not pose any operational or safety challenges to the adjacent street network. Staff would also note that there is a long-term objective of narrowing Canada Street to help with speed management and better suit its current needs as an urban street as opposed to a provincial highway.

Municipal Services

- Infrastructure for water, sanitary, and storm connection is available on Gibson Street. The driveway and water and sanitary services will be private. The developer will be responsible for the cost of construction, operation, and maintenance of infrastructure, in accordance with municipal specifications.

Stormwater management

- An approved Stormwater Management Plan (SWMP) will be required in conjunction with approval of construction drawings for the townhouse development. The purpose of the SWMP is to ensure that surface water is managed and attenuated such that there will be no negative impact on the City's public storm system, adjacent properties, or wetlands/watercourses. Stormwater attenuation will not be required for the two R-1 lots fronting on Gibson Street.

Wetland and watercourse

- There is an existing watercourse on the west side of the property, based on Service New Brunswick mapping. Any work within 30 metres of a watercourse or wetland requires a permit from Department of Environment and Local Government (DELG). Surface water

runoff must be managed during all phases of construction so as not to negatively impact adjacent properties, or wetlands/ watercourse water quality.

Public Engagement:

- The Applicant's team dropped off letters to adjacent property owners, inviting them to contact the Applicant to learn more about the proposal. The Applicant has reported that the feedback has been mainly positive, especially around the provision of affordable housing. Concerns were brought up regarding speeding issues on Gibson Street.

Summary:

- The proposal represents a significant departure from the application from 2023, with the key difference that the current proposal would be contained within the subject property and no portion of Irvine Street Park would be used for the development. Additionally, the stacked townhouse buildings would be more compatible with the surrounding properties, as they would be 2.5 storeys in height (8.1 metres), whereas the previous proposal had 3-storey stacked townhouse buildings (11.8 metres). The present application better integrates into the low-rise residential character of the area while still providing affordable housing options in a well-connected neighbourhood in close proximity to amenities. Staff support the application subject to terms and conditions.

RECOMMENDATION:

1. It is recommended that the application submitted by Eric Price on behalf of Gibson Gardens as it relates to property located at 501 Gibson street for:
 - Rezoning a portion of PID 01427780 from R-1 to MR-2, and
 - 4.9 metre lot frontage variance for the MR-2 lot, to permit a 52-unit stacked townhouse and townhouse development,

be approved subject to the following terms and conditions:

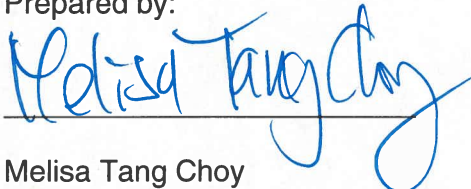
- a) The site be developed generally in accordance with Map II attached to P.R.72/25 to the satisfaction of the Development Officer;
- b) Final building design to be generally in accordance with Maps IV, V, VI, VII and VIII attached to P.R. 72/25 to the satisfaction of the Development Officer;
- c) Final landscape and parking plan, including a minimum 3-metre wide tree buffer, details on the garbage enclosure screening and bicycle parking, be provided to the satisfaction of the Development Officer prior to the issuance of a building permit;
- d) The Developer provide confirmation of support for affordable housing through a government authority, or that the Developer and City enter into an agreement that the Housing Accelerator Fund will be used for the provision of affordable housing within the approved development;
- e) Servicing, access, lot grading, and storm water management to be provided to the satisfaction of the Director of Engineering & Operations;

- f) All design, construction, and inspection to be in accordance with the City's General Specifications for Municipal Services. Record drawings, stamped by a Professional Engineer, are required at completion of the project;
 - g) Easements are to be located/designed to the satisfaction of the Director of Engineering & Operations and granted gratuitously to the relevant authorities.
 - h) The Developer is required to obtain a Watercourse and Wetland Alteration (WAWA) permit from the Department of Environment and Local Government for any development or disturbance to the ground within 30 metres of the watercourse;
2. It is further recommended that the tentative plan of subdivision create two new Residential Zone One (R-1) lots be forwarded to Council with a recommendation that the public land dedication be taken as cash-in-lieu.

Additional Information Pursuant to Section 77(1) of the Community Planning Act, the following terms and conditions will be imposed on the subdivision by the Development Officer:

- a) The final plan of subdivision be submitted substantially in accordance with Map III attached to P.R. 72/25 to the satisfaction of the Development Officer;
- b) Servicing and lot grading plans are to be provided to the satisfaction of the Director of Engineering & Operations at time of building permit application.

Prepared by:

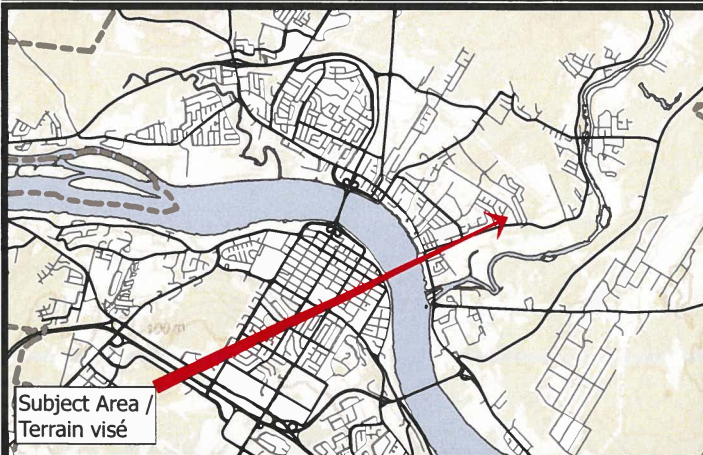
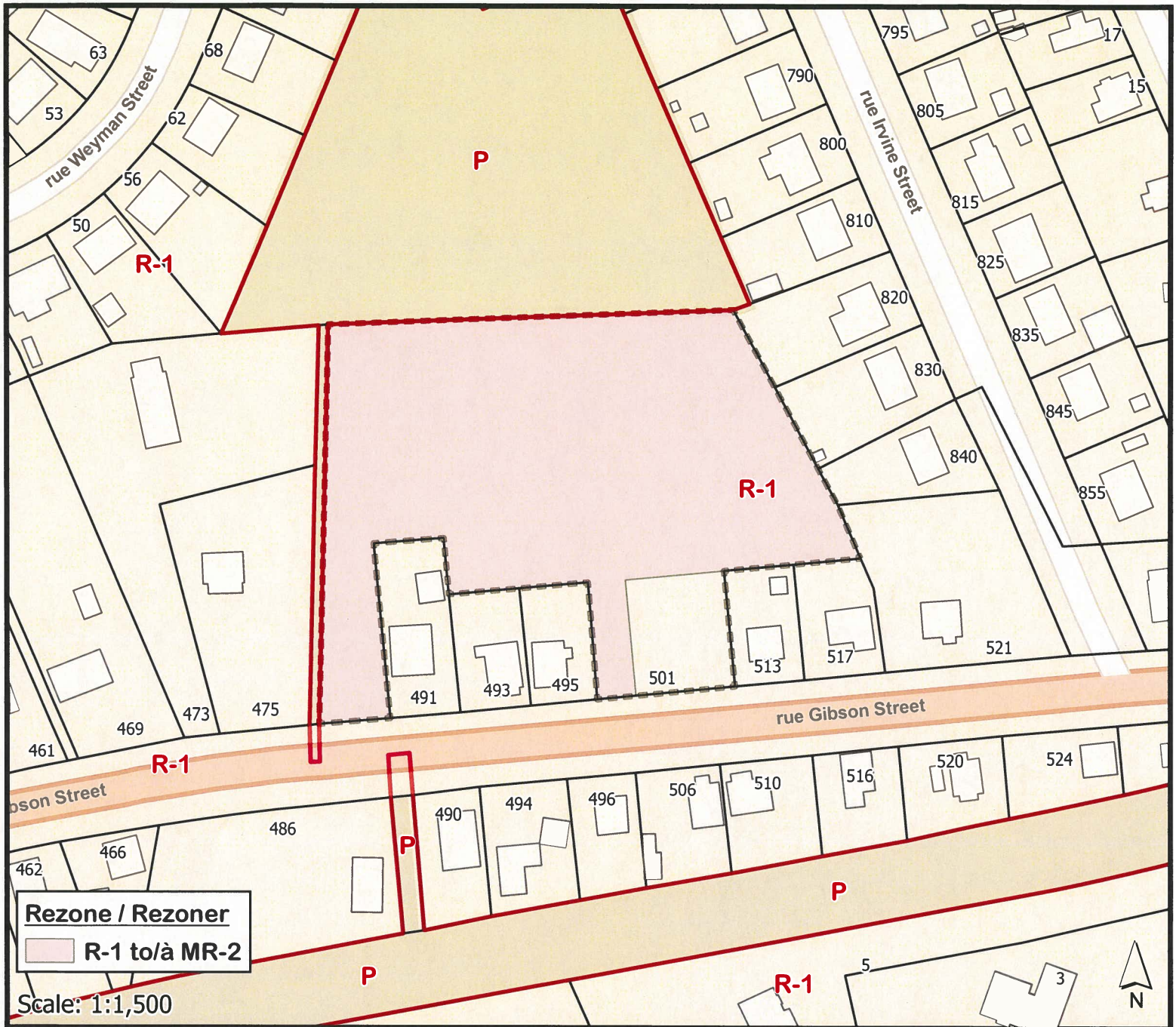


Melisa Tang Choy
Planner, Community Planning

Approved by:



Marcello Battilana, MCIP
Assistant Director, Planning & Development



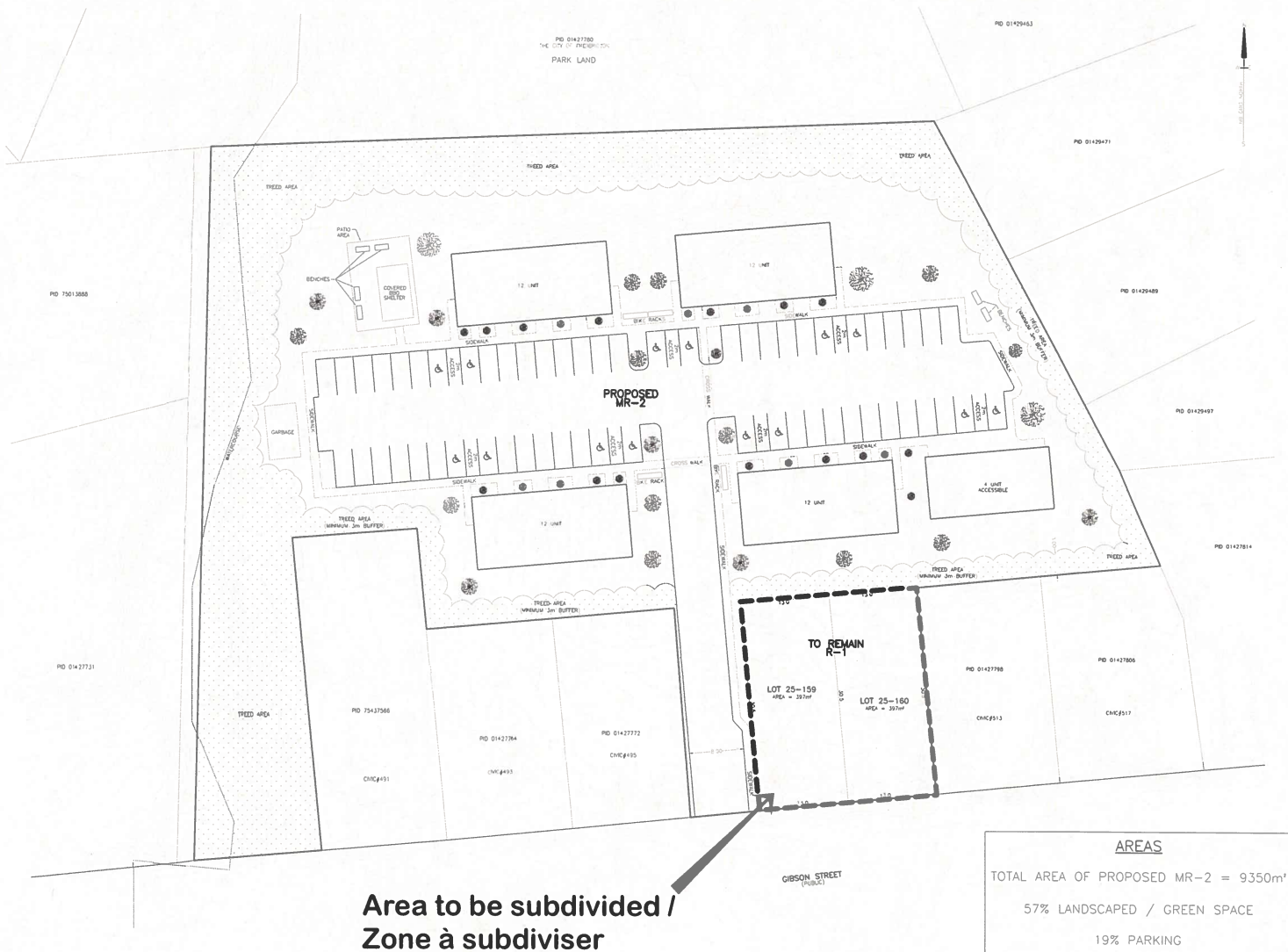
Subject Property / Propriété visée
 R-1 to/à MR-2

Application to rezone a portion of PID 01427780 from R-1 to MR-2, to permit a 52-unit stacked townhouse and townhouse development and 4.9 metre lot frontage variance. Additionally, tentative plan of subdivision to create two new R-1 lots from PID 01427780. /
 Demande de rezonage d'une portion de la propriété portant le NID 01427780 de la zone R-1 à la zone MR-2, afin de permettre un projet résidentiel de 52 unités comprenant des maisons en rangée superposées et des maisons en rangée, ainsi qu'une dérogation de 4,9 mètres à la largeur minimale de façade. De plus, un plan de lotissement provisoire est proposé pour créer deux nouveaux lots R-1 à partir du NID 01427780.

Fredericton

Community Planning
 Planification urbaine

Map \ carte # I
 File \ fiche: PR-72-2025
 Date \ date: août \ August 20, 2025
 Subject \ sujet: rue 501 Gibson Street
 Gibson Gardens
 (c/o Eric Price)



Site Plan / Plan du site

[illegible]

1:200 SCALE (METRES)

LOT 25-159
AREA = 397m²

LOT 25-160
AREA = 397m²

CMC#513
PID 01427788
ERN, ERN, TOLLOPS
TINOITY JAMES SCHRIER
DOC. #635898
REG. 2003-05-30
LOT 4
PLAN 182
REG. 1947-11-24

UNION STREET

GIBSON STREET
20.17m WIDE
(RUBEL)

CANADA STREET

PID 01427772
CMC#495

PID 0142780

[illegible]

DEED DATA:
PID PID 01427780
DOC 338571 BK 1408 PG 354
REG. 1990-XX-XX

PETER J. ADAMS

I, STEVEN D. COOK, DO HEREBY CERTIFY THAT THE INFORMATION SHOWN FOR THE BOUNDARIES BEING DEALT WITH ON THIS PLAN IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

CERTIFICATION IS NOT MADE AS TO LEGAL TITLE, BEING THE DOMAIN OF A LAWYER, NOR TO THE ZONING BY-LAWS OR REGULATIONS, BEING THE DOMAIN OF A DEVELOPMENT OFFICER.



FILE N°: 25012-1

**TENTATIVE
SUBDIVISION PLAN
GIBSON GARDENS PHASE 1
SHOWING
LOT 25-159 & LOT 25-160**

GIBSON STREET
CITY OF FREDERICTON
COUNTY OF YORK
PROVINCE OF NEW BRUNSWICK

APPROVED	APPROVAL VALID FOR ONE YEAR ONLY UNLESS FILED
For - Development Officer City of Fredericton	
Date	

TENTATIVE SUBMISSION PLAN NOTE:

THIS PLAN IS A TENTATIVE SUBDIVISION PLAN AND IS NOT A SURVEY OF THE SUBJECT PROPERTY. THE INTENTION OF THIS PLAN IS TO SHOW A PROPOSED SUBDIVISION FOR REVIEW BY THE DEVELOPMENT OFFICER.

THE INFORMATION SHOWN ON THIS PLAN IS DERIVED FROM
SINR REAL PROPERTY INFORMATION, SINR DIGITAL
PROPERTY MAPPING, AND OTHER DATA ACQUIRED FROM
THE GEOIR WEBSITE. NO INFORMATION SHOWN ON THIS
INITIATIVE PLAN WAS DERIVED FROM FIELD OBSERVATIONS.

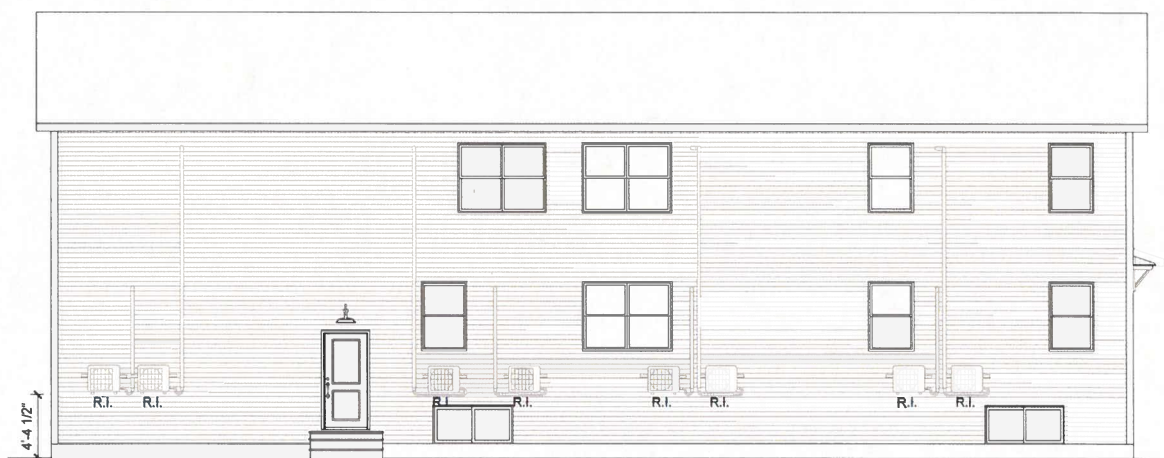
TENTATIVE SUBDIVISION PLAN REV1: 2025-07-29

PURPOSE OF PLAN:

TO CREATE LOT 25-159 & LOT 25-160
FROM A PORTION OF PID 01427780.



Front / Façade



Rear / Arrière

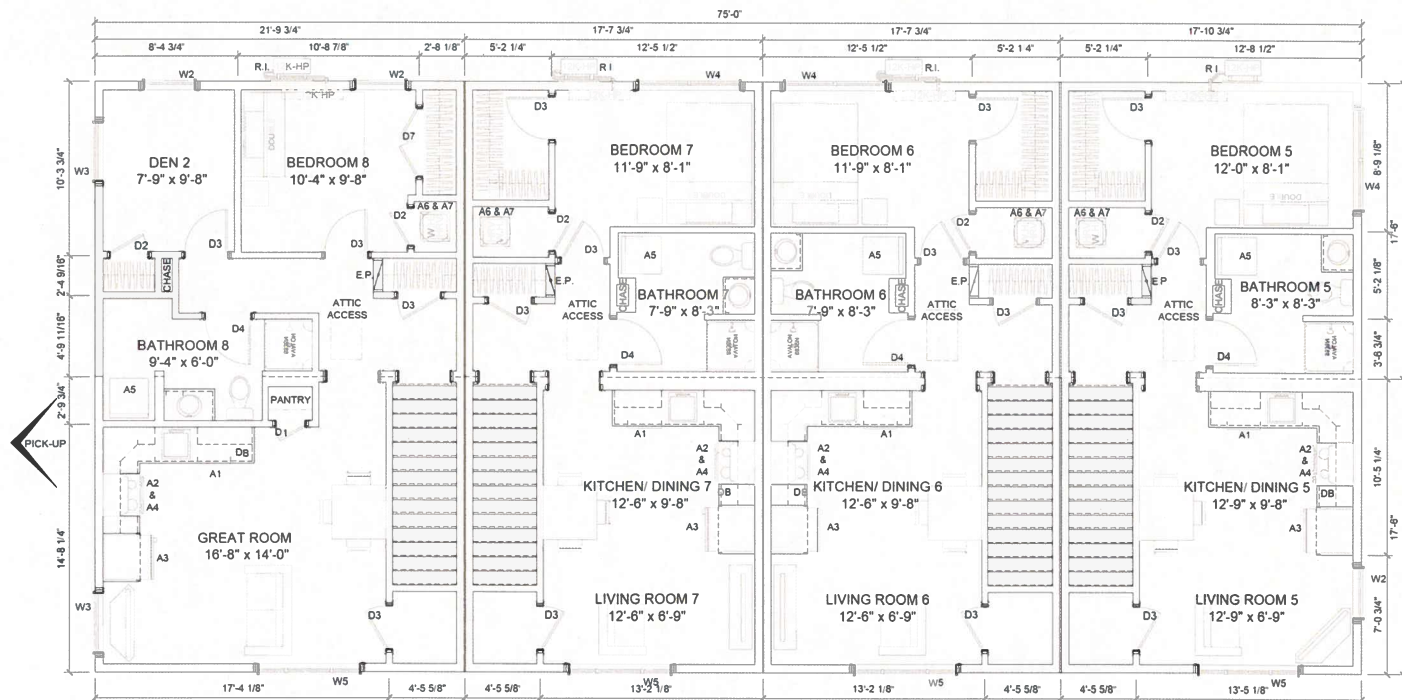
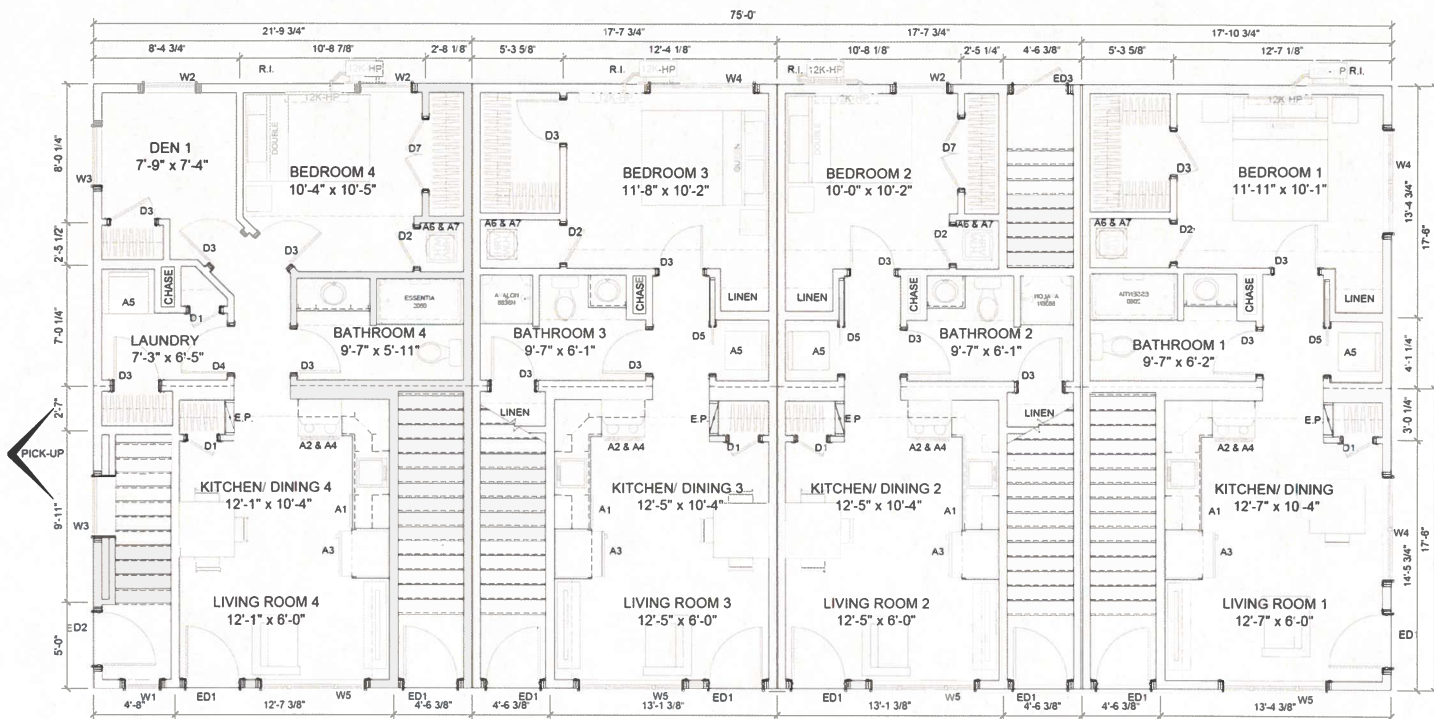


Right / Droite



Left / Gauche

Elevations - Townhouse / Élévations - Maison en rangée

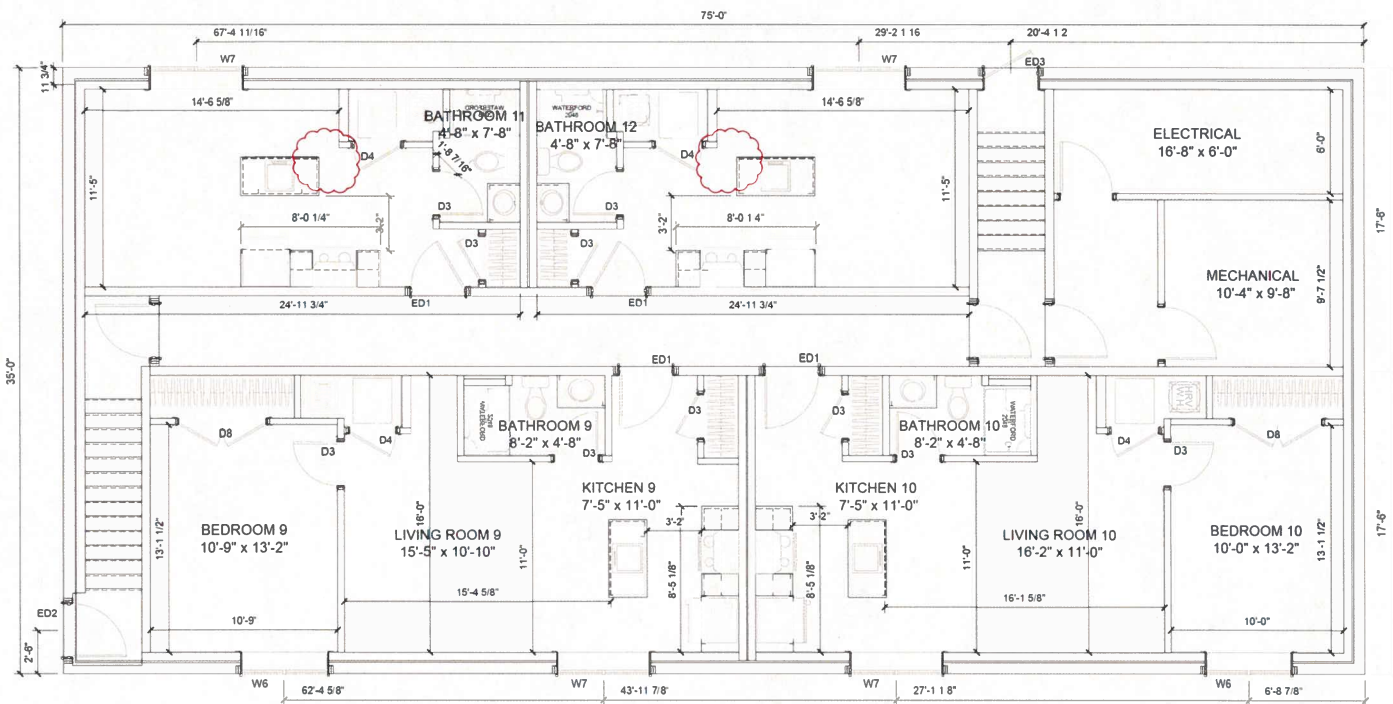


Floor Plans - Townhouse / Plans d'étage - Maison en rangée



Community Planning
Planification urbaine

Map \ carte # V
File \ fiche: PR-72-2025
Date \ date: août \ August 20, 2025
Subject \ sujet: rue 501 Gibson Street
Gibson Gardens
(c/o Eric Price)



Floor Plans - Townhouse / Plans d'étage – Maison en rangée

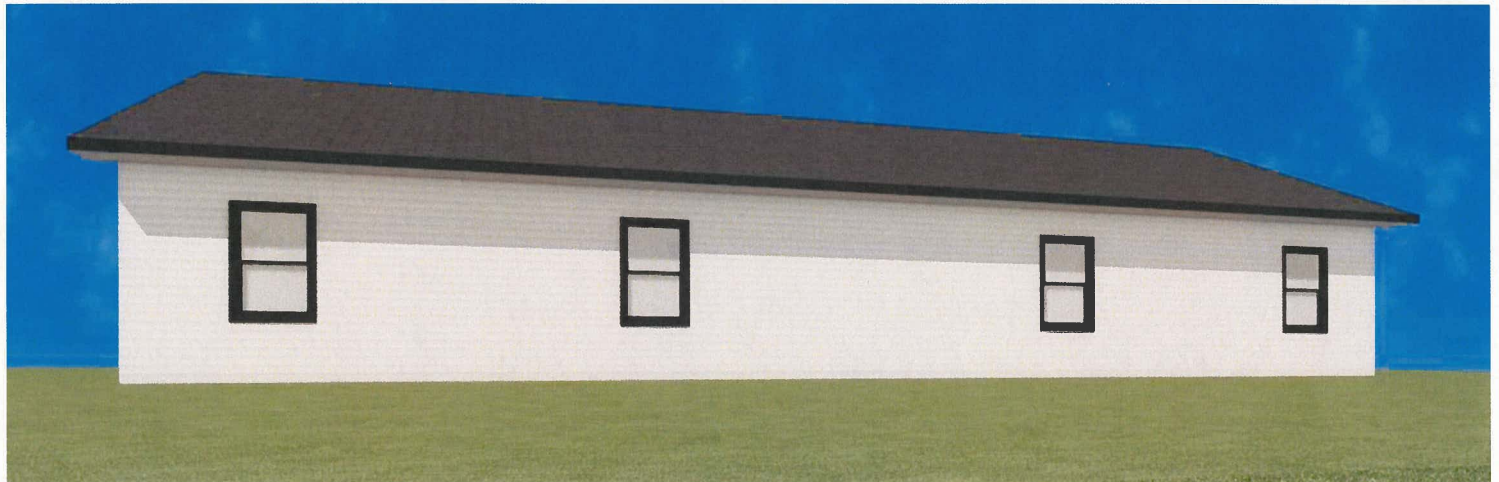
Fredericton

Community Planning
Planification urbaine

Map \ carte # VI
File \ fiche: PR-72-2025
Date \ date: août \ August 20, 2025
Subject \ sujet: rue 501 Gibson Street
Gibson Gardens
(c/o Eric Price)

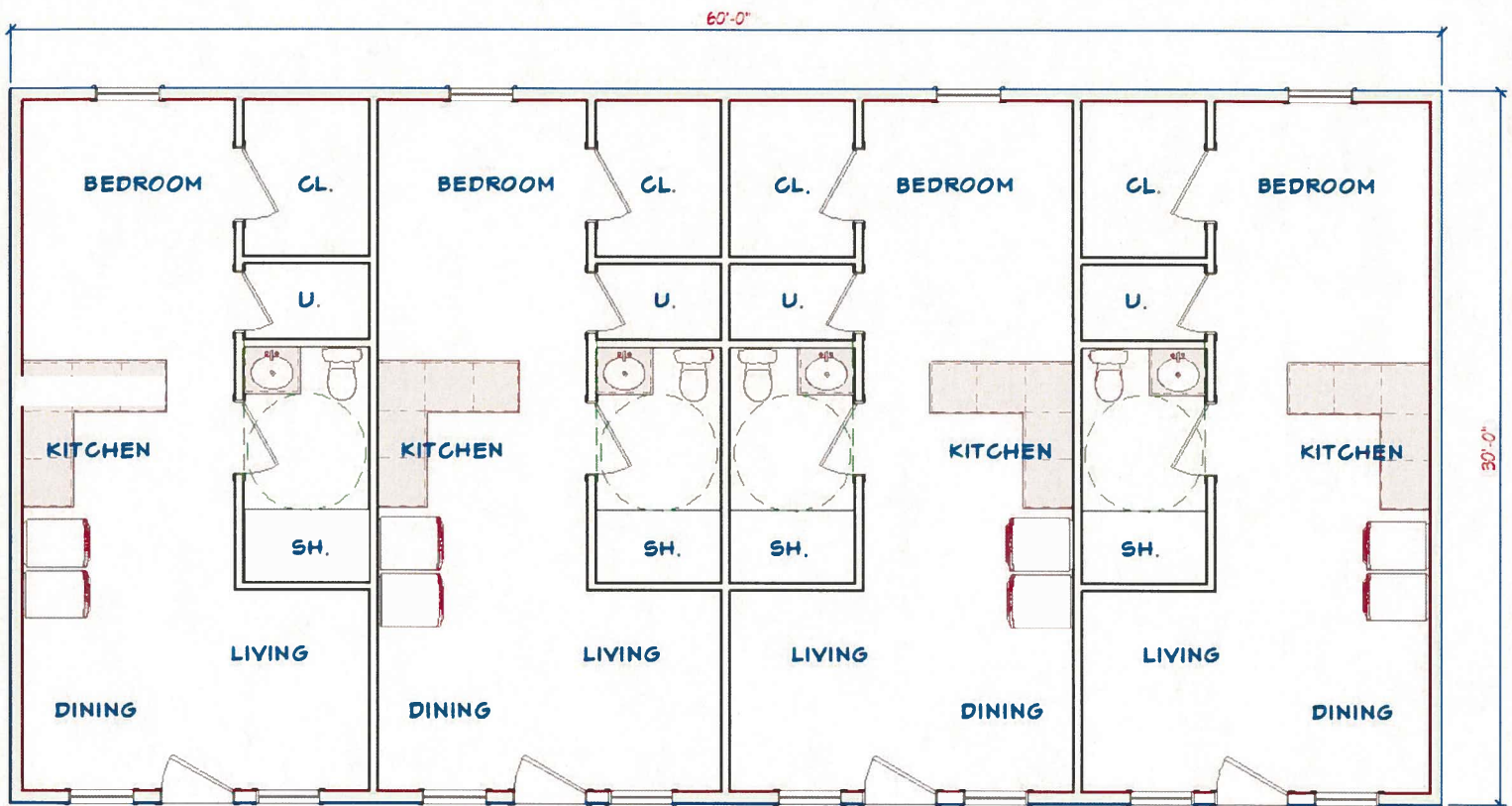


Front / Façade



Rear / Arrière

Elevations - Accessible Unit / Élévations – Unité accessible



Floor Plans - Accessible Unit / Plans d'étage - Unité accessible

Fredericton

Community Planning
Planification urbaine

Map \ carte # VIII
File \ fiche: PR-72-2025
Date \ date: août \ August 20, 2025
Subject \ sujet: rue 501 Gibson Street
Gibson Gardens
(c/o Eric Price)

To: Planning Advisory Committee

From: Tony Dakiv, Senior Planner

Proposal: Rezoning and tentative plan of subdivision with variance to allow construction of a 12-unit townhouse development.

Property: 199 Serenity Lane (PID 75422444)

APPLICANT: G. George Construction Ltd.
592 Squires Street,
Fredericton, NB, E3B 3V4

OWNER: Same as above

SITE INFORMATION:

Location: East side of Serenity Lane south of Woodside Lane

Context: Single detached dwellings to the east and south, apartment building to the north and multi-residential development under construction to the west.

Ward No: 12

Municipal Plan: Established Neighbourhoods

Zoning: R-1 and R-3

Existing Land Use: Vacant

Previous Applications: Z-17-2004, S-13-2004, Z-18-2006, S-21-2006

EXECUTIVE SUMMARY:

The proposal is to rezone the property to the R-5 zone and a tentative plan of subdivision to create 12 townhouse lots requiring a 0.75m lot depth variance (for one lot) to permit the construction of 3 buildings containing a total of 12 townhouses.

The proposal is consistent with the Growth Strategy for Areas of Stability and Minor Change and Municipal Plan criteria for low rise multi-residential development. In this case the key consideration in assessing the proposed rezoning is the site context. The subject property is ideally located to provide an appropriate transition in residential building form and density between the four storey apartment building to the north, the developing multi-residential project to the west and the existing low-rise neighbourhood to the east and south.

Townhouses are considered to be an appropriate built form between higher density residential buildings and single detached dwellings. The proposed buildings are modest in scale and massing and fits within the site context. Based on this the proposal is considered to be a compatible infill development and should not adversely impact neighbouring properties. The proposal meets all by-law requirements of the R-5 zone which reverts to the MR-1 zone for townhouses. The only exception is a 0.75m lot depth variance for one proposed lot (Lot 25-L), which is considered to be minor, and Staff support the application subject to terms and conditions.

APPLICATION:

G. George Construction Ltd. has made application for the following on property located at 199 Serenity Lane:

- rezone land from R-1 and R-3 to R-5 in order to construct 12 townhouse units within 3 buildings, and
- a tentative plan of subdivision to create 12 townhouse lots requiring a 0.75m lot depth variance for Lot 25-L.

PLANNING COMMENTS:

Background:

- In 2004, a tentative plan of subdivision was approved to create 5 residential building lots on the subject property as part of the larger Red Maple Court development. In 2006, the majority of the subject property was rezoned to R-3 in order to create 4 semi-detached building lots and one single detached building lot for a total of 9 dwelling units on the site.

Proposal:

- The proposed townhouse development comprises 3 buildings containing 4 townhouse units in each. The middle building has been sited 2m farther back from the street than the buildings on either side to provide an offset which helps to visually break up the long wall effect along the street. The buildings will incorporate a walk-out basement layout with a single attached garage and a separate 4m wide driveway for each unit.
- The proposal complies with the MR-1 zone building design requirements (which applies for townhouses). The exterior finish materials will incorporate an alternating pattern of masonry and vinyl siding on the buildings front façade facing Serenity Lane.
- The 8% public land dedication has already been taken as cash in lieu as a requirement of the 2004 subdivision approval noted earlier. Based on this the tentative plan of subdivision does not require PAC and Council approval, however, because one of the proposed lots requires a lot depth variance, the tentative plan of subdivision has also been included with the application.

Growth Strategy:

- The proposal is consistent with the direction of the Growth Strategy for Areas of Stability and Minor Change which states that "...modest forms of intensification may be permitted at the edges of neighbourhoods, along main roads, eg., townhouse developments and low-rise apartment buildings", and that "All such changes, however, will respect and reinforce the existing pattern, scale and character of the neighbourhood."

Municipal Plan:

- The subject property is designated Established Neighbourhoods which contains criteria (Section 2.2.1(21)) to maintain the stability of residential neighbourhoods as follows:

To maintain the stability of residential neighbourhoods, while allowing for incremental change through sensitive new development and redevelopment, new development will respect and reinforce the existing pattern, scale, and character of the Established Neighbourhoods, by ensuring that:

- Any new lots are consistent with the lot pattern in the neighbourhood;*
- Building design is compatible with the surrounding area and contributes positively to the neighbourhood;*

- iii. Adequate servicing, road infrastructure, and other municipal services be readily and efficiently provided; and,
- iv. Healthy, mature trees are protected whenever feasible.

The proposal is consistent with the above criteria as the lot pattern reflects the layout of the previous approvals and represents a small increase in the number of dwellings (12 instead of 9 as of right). Proposed building design is compatible with the surrounding area with a two storey building height and walk-out basement. Adequate servicing is in place along Serenity Lane which has been recently upgraded and the building location will enable a large amount of existing trees in the rear to be retained as a buffer. Overall, the site provides an appropriate transition between the mid-rise multi-residential development to the north and west and the existing low-rise neighbourhood on Red Maple Court.

Zoning By-law:

The proposal compared to the standards of the MR-1 zone is as follows:

Standard	Required	Provided
Lot Area	2160m ²	5560.7m ²
Lot Frontage	6m min. per lot	8.7m
Lot Depth	30m min.	29.25m Lot 25-L only*
Building Setbacks front	6m min	6.55m
side	1.8m min.	1.83m
rear	6m min.	6.35m (smallest)
Building Height	10m max.	7.5m approx.
Lot Coverage	45% of lot max.	33% approx.
Landscaped Area	540m ² min.	3340m ² approx..
Parking	1 space min. per unit	1 space per unit

* variance required

- The minor lot depth variance of 0.75m is considered to be marginal and only applies to the end lot (Lot 25-L) where the property narrows down. This would not create any adverse impacts on the neighboring properties or on proposed Lot 25-L which still has adequate lot depth.

Access and servicing

- In 2006 the site was re-zoned with the report outlining the recommendations that site drainage and stormwater management is to be to the satisfaction of the developer, servicing costs shall be the responsibility of the developer/owner, and developer/owner shall be responsible for the costs of sidewalks, curbing and gutters. In 2024, municipal servicing and roadway infrastructure was installed to City standards along the frontage of 199 Serenity Lane. This creates the requirement that the developer/owner will be required to pay the associated Infrastructure Fee for the servicing and infrastructure installed.
- The design of the access, service laterals, lot grading and associated reinstatement are to be to the satisfaction of the Director of Engineering and Operations and will be the responsibility of the developer. Due to the amount of service laterals that will cross the roadway in close proximity, the

developer will be required to mill and pave the entire street section over the length of the service lateral disturbance area.

- Due to the proposed layout of the units, stormwater management on-site will be difficult to achieve. The City is recommending that the developer complete a stormwater management report outlining the necessary amendments required to capture the attenuation of the proposed development in a downstream City owned pond. It will be the developer's responsibility to construct the amendments to the stormwater pond to the satisfaction of the Director of Engineering and Operations.
- Traffic has reviewed the application, and the proposed development does not represent a significant traffic generator for the area. The area has been studied recently as part of large adjacent development. Operational deficiencies noted have been corrected (adding a left turn phase for Woodside lane and Bishop Drive). The existing road network can easily support the volume that would come from these homes.

RECOMMENDATION:

It is recommended the application submitted by G. George Construction Ltd., to rezone property located at 199 Serenity Lane from R-1 and R-3 to R-5 and a tentative plan of subdivision to create 12 townhouse lots requiring a 0.75m lot depth variance (Lot 25-L), to allow construction of a 12-unit townhouse development be approved subject to the following terms and conditions:

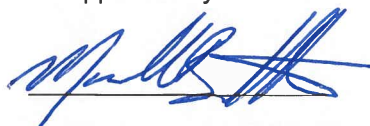
- a) The site be developed generally in accordance with Map II attached to P.R. 66/25 to the satisfaction of the Development Officer;
- b) Final building design be generally in accordance with Map III attached to P.R. 66/25 to the satisfaction of the Development Officer;
- c) A final plan of subdivision be submitted substantially in accordance with Map II attached to P.R. 66/25 to the satisfaction of the Development Officer;
- d) Prior to the approval of the subdivision plan for registration and building permits, the following are required by Engineering & Operations:
 - An infrastructure fee to the satisfaction of the of the Director of Engineering and Operations;
 - Servicing, lot grading, and storm water management plans are to be provided to the satisfaction of the Director of Engineering & Operations;
 - Storm water management report required to the satisfaction of the Director of Engineering and Operations.
- e) A license agreement is required to alter the City's stormwater management pond;
- f) Electrical site servicing is to be shown on site plans;
- g) Record drawings prepared by a Professional Engineer are required at completion of the project;
- h) The Applicant/Landowner, their Engineering Consultant and a planning representative are to participate in a design start-up meeting with Engineering staff upon approval of this application.

Prepared by:

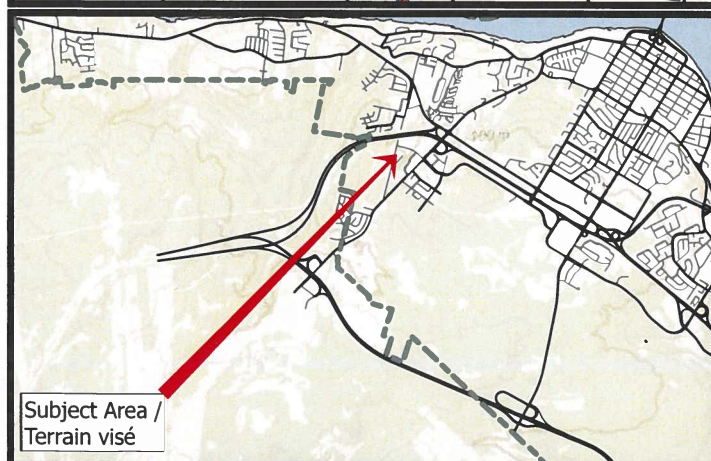
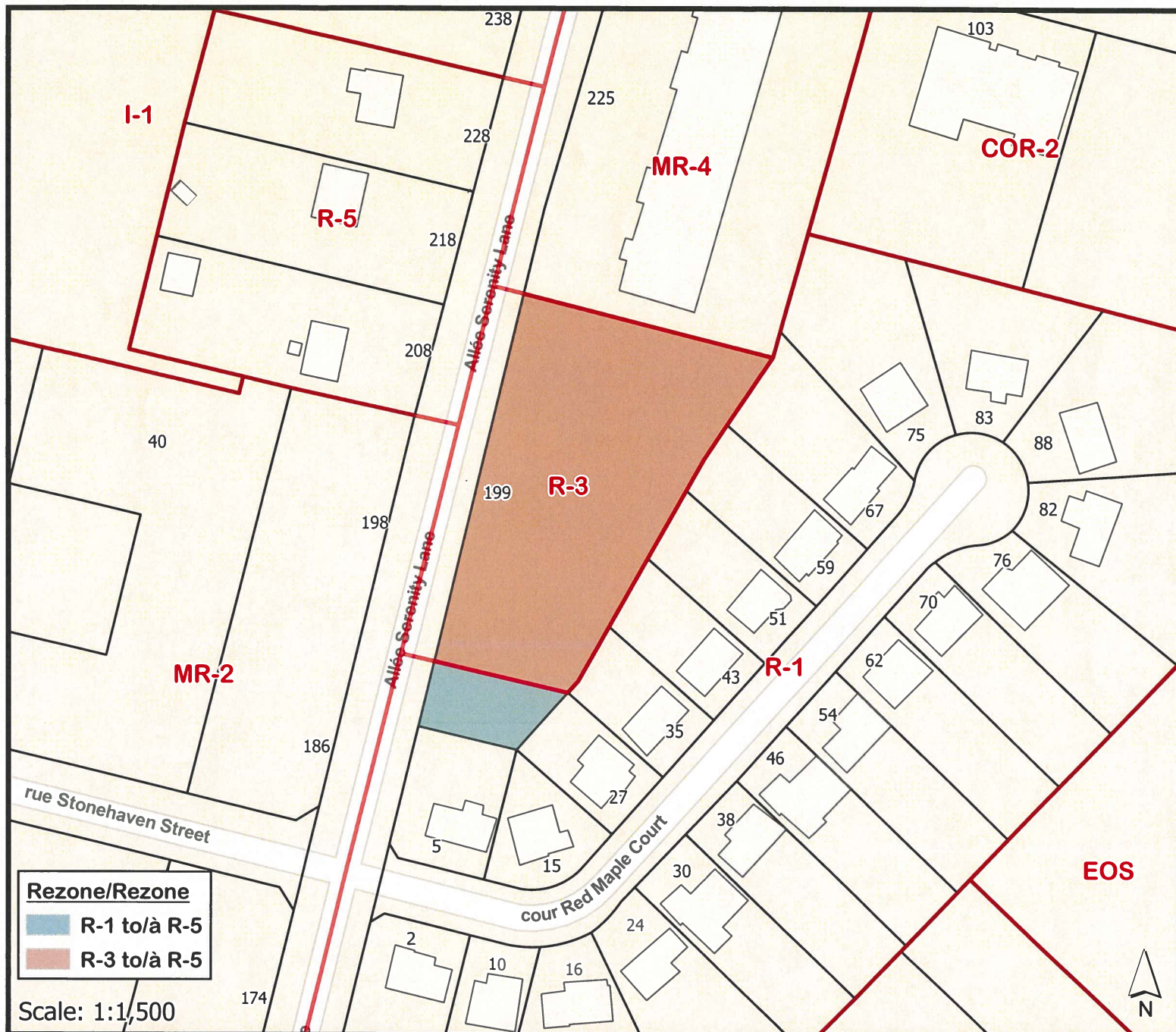


Tony Dakiv, RPP, MCIP
Senior Planner, Community Planning

Approved by:



Marcello Battilana, RPP, MCIP
Manager, Community Planning



Subject Property / Propriété visée

Rezoning from R-1 and R-3 to R-5 to permit the construction of a 12 unit townhouse development.

Rezoner de R-1 et R-3 à R-5 afin de permettre la construction d'un ensemble de 12 maisons en rangée

Fredericton

Community Planning
Planification urbaine

Map \ carte # I

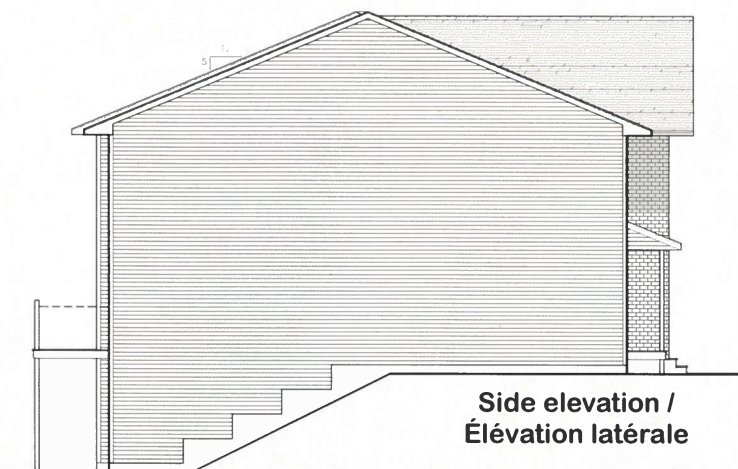
File \ fiche: PR-66-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: allée 199 Serenity Lane
G. George Construction Ltd.



Rear (East)/ Arrière (Est)



Side elevation /
Élévation latérale



Front - facing Serenity Lane (West) / Façade – orientée vers l'allée de la Sérénité (Ouest)

Elevations / Élévations

Fredericton

Community Planning
Planification urbaine

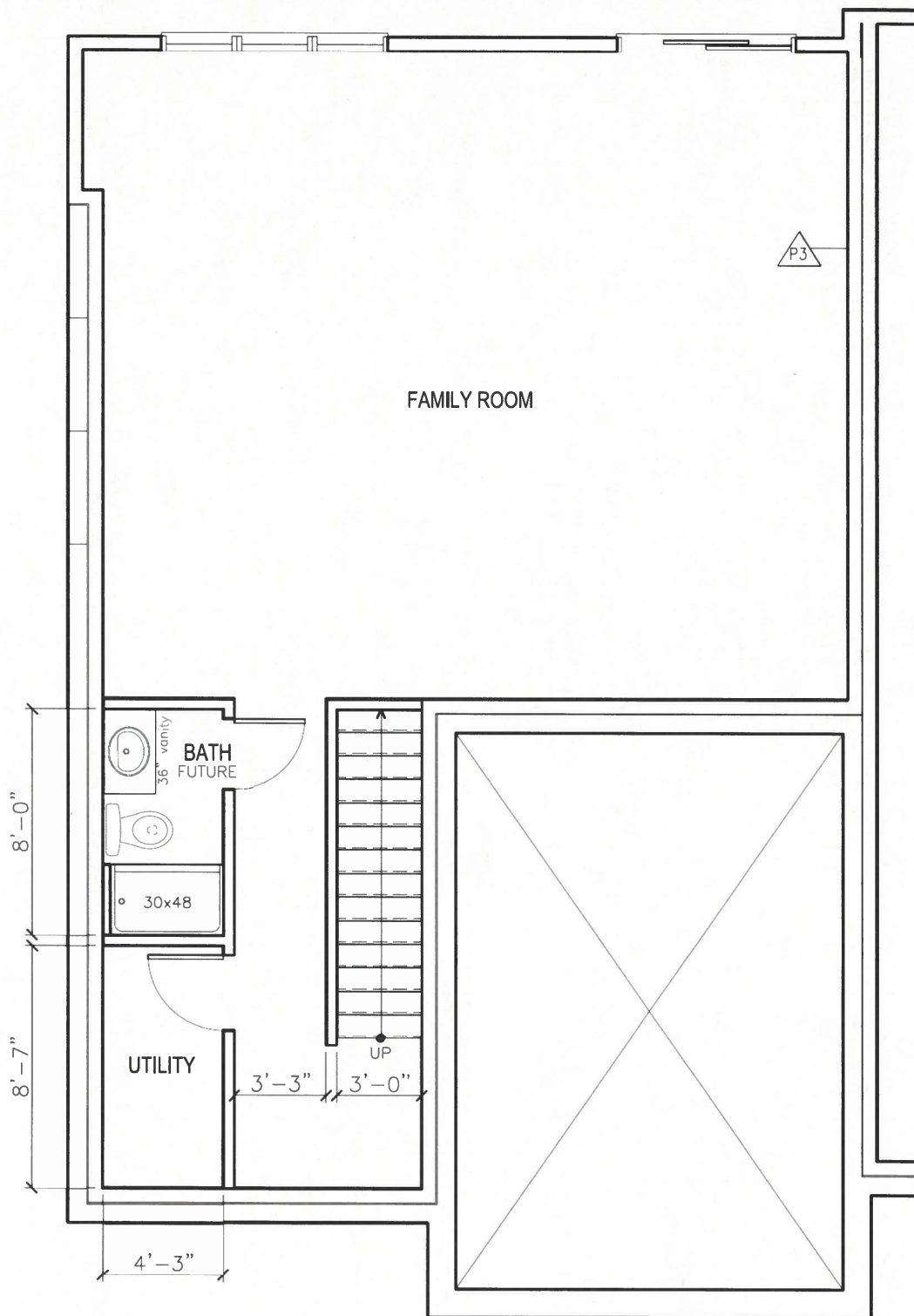
Map \ carte # III

File \ fiche: PR-66-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: allée 199 Serenity Lane

G. George Construction Ltd.



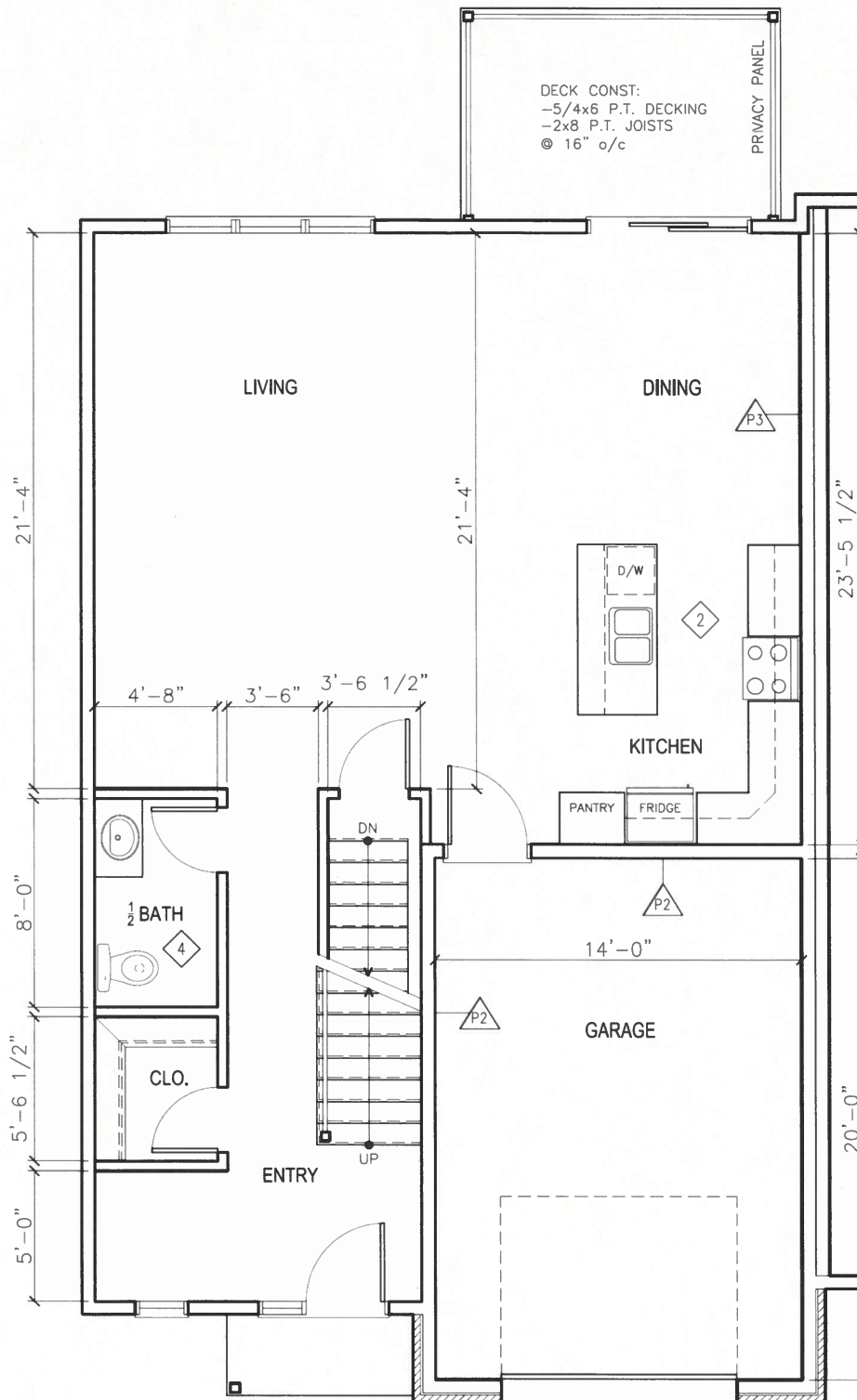
Basement / Sous-sol

Floor Plans / Plans d'étage

Fredericton

Community Planning
Planification urbaine

Map \ carte # IV
File \ fiche: PR-66-2025
Date \ date: août \ August 20, 2025
Subject \ sujet: allée 199 Serenity Lane
G. George Construction Ltd.



First floor / Rez-de-chaussée

Floor Plans / Plans d'étage

Fredericton

Community Planning
Planification urbaine

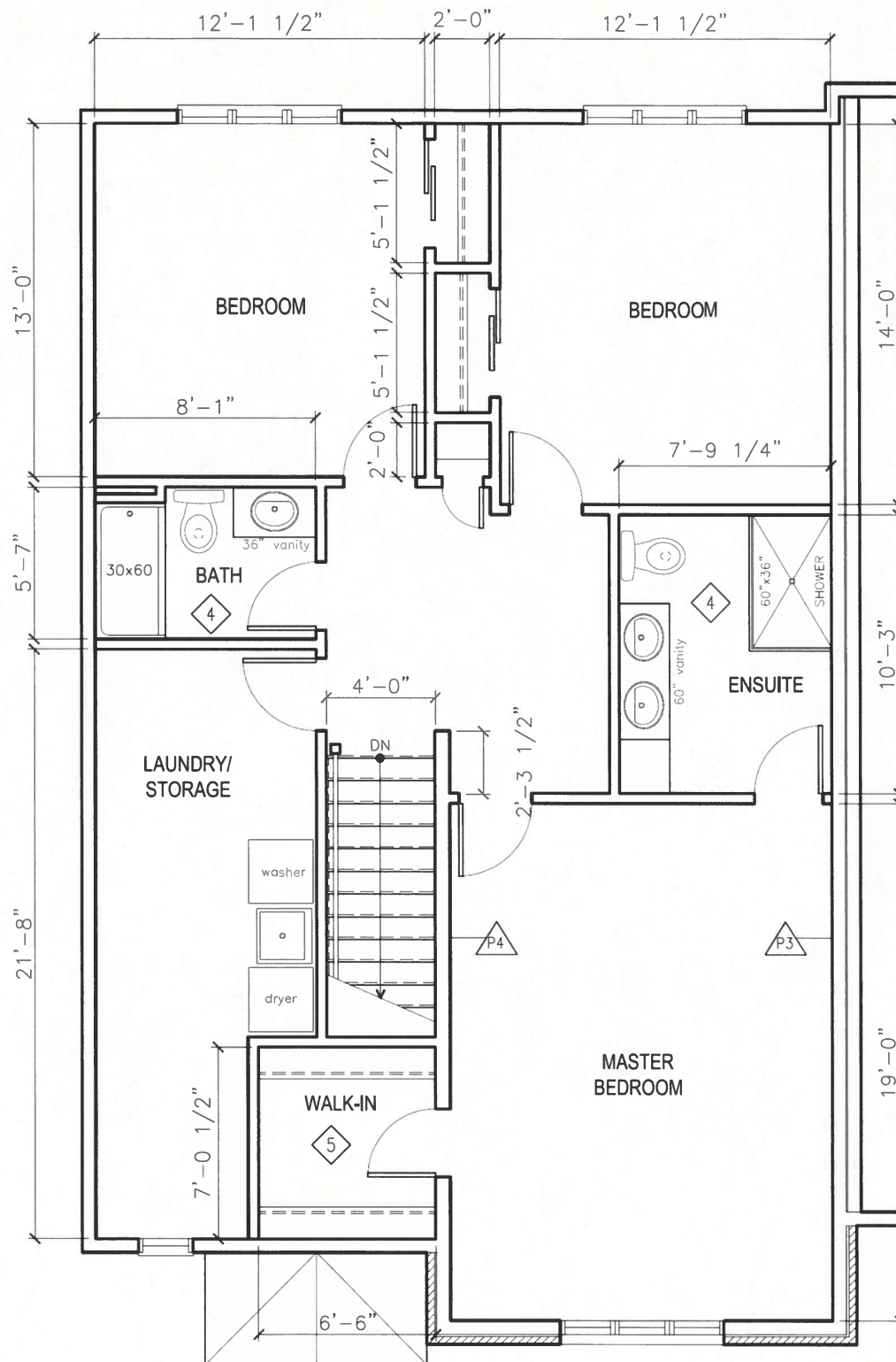
Map \ carte # V

File \ fiche: PR-66-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: allée 199 Serenity Lane

G. George Construction Ltd.



Second floor / Premier étage

Floor Plans / Plans d'étage

Fredericton

Community Planning
Planification urbaine

Map \ carte # VI

File \ fiche: PR-66-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: allée 199 Serenity Lane

G. George Construction Ltd.

To: Planning Advisory Committee

From: Fredrick Van Rooyen, Senior Planner

Proposal: Amend previously approved terms and conditions to permit a Child Care Centre – Large use for up to 163 children

Property: **170 Doak Road (PID 75392282)**

OWNER: Fredericton Direct Charge Co-operative Ltd. c/o Teena Noble
170 Doak Road
Fredericton, NB, E3C 2G2

APPLICANT: As above

SITE INFORMATION:

Location: East side of Doak Road, north of the Doak Road and Alison Boulevard intersection

Context: Mix of commercial, industrial, and residential uses; largely undeveloped lands to the south and east

Ward No: 7

Municipal Plan: Commercial Centres & Corridors

Zoning: Retail-Large Format Zone (RLF)

Existing Land Use: Grocery Store & Child Care Centre – Large

Previous Applications: P.R. 01/14

EXECUTIVE SUMMARY:

The Applicant is proposing to amend the terms and conditions of By-law Z-5.27 in order to permit a new child care centre space that would allow for an additional 80 children (163 children total on the property), as the existing terms and conditions establish a maximum of 83 children. As the property is zoned Retail Large Format (RLF), a zone amendment was required in 2014 to permit the initial Child Care Centre – Large use. Staff have no concerns with the new child care centre space as the proposal meets the intent of the Municipal Plan, parking can be accommodated for the additional child care centre space, and the existing child care centre use has been operating on site for over a decade without any concerns. Staff support the applicant and recommend approval of the amended terms and conditions.

APPLICATION:

Fredericton Direct Charge Co-operative Ltd. has made application on property located at 170 Doak Road to amend previously approved term and condition (b) of By-law Z-5.27, to permit a Child Care Centre – Large use with a maximum of 163 children total on the property.

PLANNING COMMENTS:

Background:

- In 2014, Council approved a zone amendment to permit a Child Care Centre – Large use for up to 83 children (P.R. 01/14) as the Retail Large Format (RLF) Zone did not permit the use. The existing child care centre is a 417 square metre, single storey stand-alone building separate from the main grocery store building and has 15 staff members at full capacity. Through By-law Z-5.27, the approved zone amendment was subject to terms and conditions. Term and condition b) of the approval outlined that *“the number of children served by the child care centre be limited to a maximum of eighty-three (83)”*. As this proposal looks to create a new child care centre space that would exceed the 83 maximum children, an amendment to the terms and conditions is required.

Proposal:

- The proposed child care centre space would be approximately 3,200sq.ft. (297sq.m.) in size and located at the back of the main grocery store building (see Maps III & IV). The child care space would be operated by Oakland Academy, who is the operator of the existing child care centre on site, and would accommodate up to 80 children (for a total of 163 children on the property). A maximum of 8 staff would be on-site during operating hours being 7:30am to 5:30pm, Monday through Friday. The child care centre space would also feature an outdoor play area, which would be designed in accordance with regulations of the NB Department of Education and Early Childhood Development, including fencing as required for security and safety purposes (see Map II). Parking for the new child care centre space would be provided at the back of the building near the child care centre entrance. The applicant has noted minor modifications to the existing parking area would be required to accommodate the outdoor play area. The operator has indicated that the new child care space is being proposed given the area’s ongoing population growth and significant demand for licensed child care services, especially for children under the age of two. This child care space would look to meet the urgent community need and support working families, especially mothers returning to the workforce.

Municipal Plan:

- The subject property is designated Commercial Centres and Corridors within the Municipal Plan. As per subsection 2.2.1(29) *“Lands within the Commercial Centres and Corridors Designation may include a full range of retail, commercial, institutional, small-scale office and service industry uses, and other uses deemed compatible with commercial uses.”* The proposed use falls into the category of institutional and the use would be considered compatible with the commercial uses on site, particularly as there is already a child care centre use on site.
- Staff would note that child care centres provide an important function for a growing city and support the creation of complete communities. This area in particular is part of the Southeast New Neighbourhood Secondary Plan Area, which is currently being drafted.

With this area potentially being considered a new neighbourhood in the future, a significant amount of residential development and new residents would be brought to the area. Accordingly, further child care centre space would be considered appropriate and encouraged to support the area's future growth.

Zoning:

- While the Child Care Centre – Large use is not permitted in the Retail Large Format (RLF) Zone, the 2014 zone amendment established it as a permitted use, subject to the approved terms and conditions as part of By-law Z-5.27. The Child Care Centre – Large use is permitted in several commercial zones (i.e. LC, NC, DC, RC, COR-1, CC, CCI), and given significant demand in recent years, the use has been approved in other commercial zones through zone amendment applicants (i.e. COR-2 and RLF), provided the site can appropriately accommodate the use.
- With the proposed new child care centre space proposing an additional 80 children within the existing main building, the primary criteria from a zoning perspective is whether the site can accommodate the additional parking required. The existing parking area has approximately 273 vehicle parking spaces and the required parking would be as follows:

Uses	Parking Rate	Required
Existing Child Care Centre	1 sp/employee + 1 sp/10 children	15 employees + 83 children = 23.3 spaces
Existing Grocery Store	3.5 sp/100m ² nfa	4,645m ² nfa = 162.57 spaces
Proposed Child Care Centre	1 sp/employee + 1 sp/10 children	8 employees + 80 children = 16 spaces
Total Vehicle Parking Spaces Required	202 spaces	

- The existing parking on site can easily accommodate the required 16 vehicle parking spaces required for the new child care centre space, including any adjustments with the required outdoor play area.
- Staff would note that the existing terms and conditions from the original approval (By-law Z-5.27) would still remain valid, with the exception of term and condition b), which would be amended to allow for the new maximum of 163 children. The existing terms and conditions would require that a detailed parking and landscape plan be provided prior to the issuance of a building permit (term and condition d); this would speak to ensuring that the outdoor play area is sited and screened appropriately and that any adjustments to the parking area are outlined on the plan. Additionally, there is a term and condition to ensure that all necessary approvals from the NB Department of Education and Early Childhood Development are obtained for the new child care centre space (term and condition g).
- Overall, staff would consider the proposed expansion of the child care centre use appropriate for the site as the existing child care centre use has been operating on site for over a decade without any concerns, parking can be accommodated for the additional space, and it is a reasonable use for the vacant space within the main building.

Traffic:

- The subject property is located at the north end of Doak Road with ingress access from the Vanier Highway and Alison Boulevard, with egress to Alison Boulevard only. It is expected that the existing road network has capacity to accommodate the increase of traffic. As part of the ongoing planning for the Southeast New Neighbourhood Secondary Municipal Plan, Transportation staff would be evaluating the future traffic impact and trip generation for the broader neighbourhood. Within that transportation evaluation, staff would factor in the AM and PM peak generation for this daycare expansion to help inform the impacts to operational capacity and safety for the surrounding road network.

Servicing:

- Whereas the application is proposed in an existing serviced commercial building, there are no concerns anticipated from a water and sewer perspective.

RECOMMENDATION:

It is recommended that the application submitted by Fredericton Direct Charge Co-operative Ltd. on property located at 170 Doak Road to amend previously approved term and condition (b) of By-law Z-5.27, which reads as follows:

- b) That number of children served by the Child Care Centre be limited to a maximum of 83.

be repealed and replaced with the following term and condition:

- b) The Child Care Centre – Large use permit a maximum 163 children in total on the property.

Prepared by:

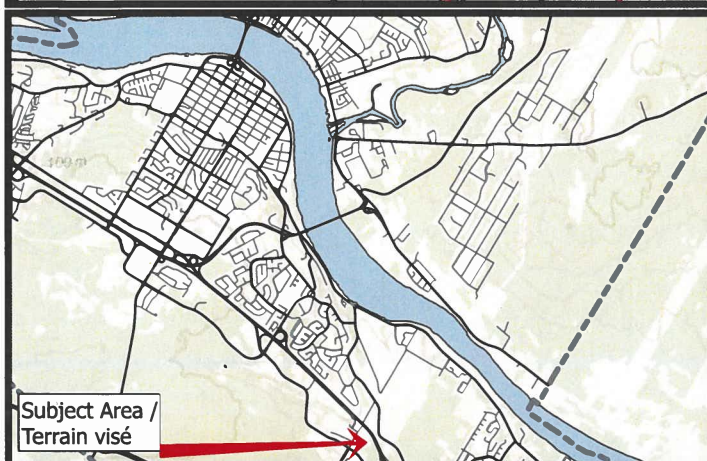
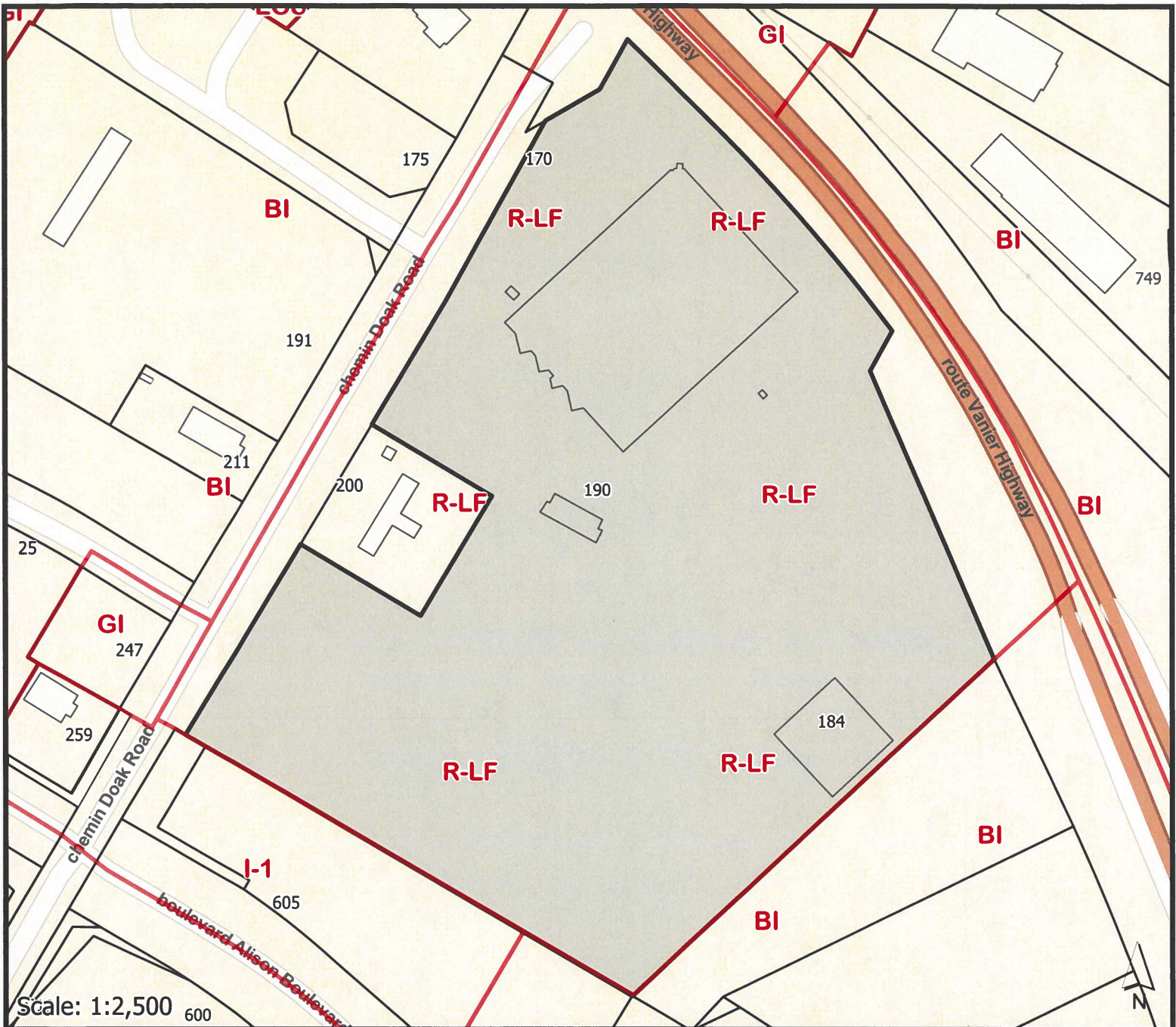


Fredrick Van Rooyen, RPP, MCIP
Senior Planner, Community Planning

Approved by:



Marcello Battilana, RPP, MCIP
Assistant Director, Planning & Development



Subject Property / Propriété visée

Amendment to terms and conditions of By-law Z-5.27 to permit a child care centre – large use for up to 163 children (existing terms and conditions permit up to 83 children).

Modification des modalités du règlement municipal Z-5.27 pour permettre l'exploitation d'une garderie de grande taille accueillant jusqu'à 163 enfants (les modalités actuelles permettent jusqu'à 83 enfants).

Fredericton

Community Planning
Planification urbaine

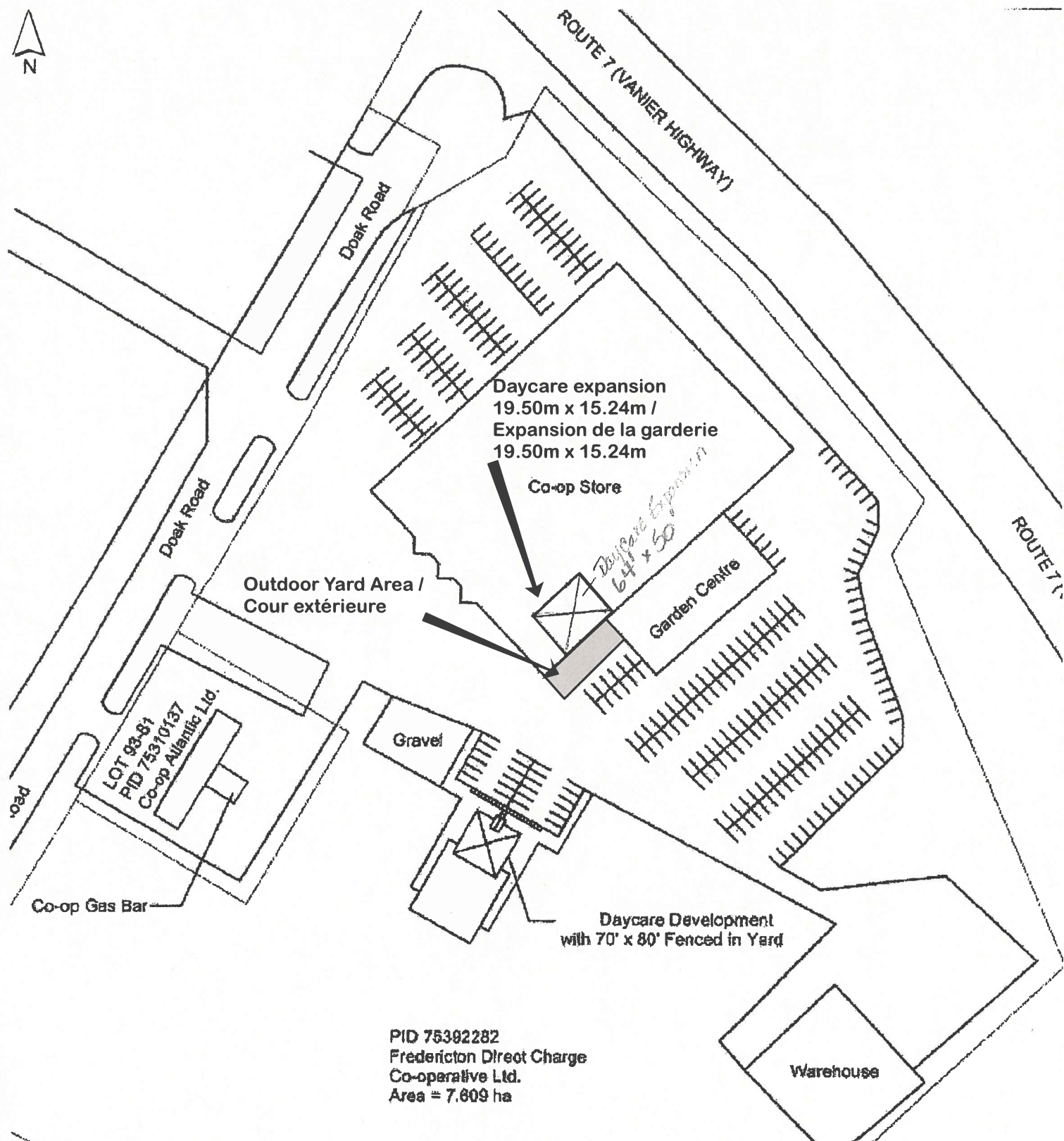
Map \ carte # I

File \ fiche: PR-65-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: chemin 170 Doak Road

Fredericton Direct Charge Co-operative Ltd.
c/o Teena Noble



Site Plan / Plan du site

Fredericton

Community Planning
Planification urbaine

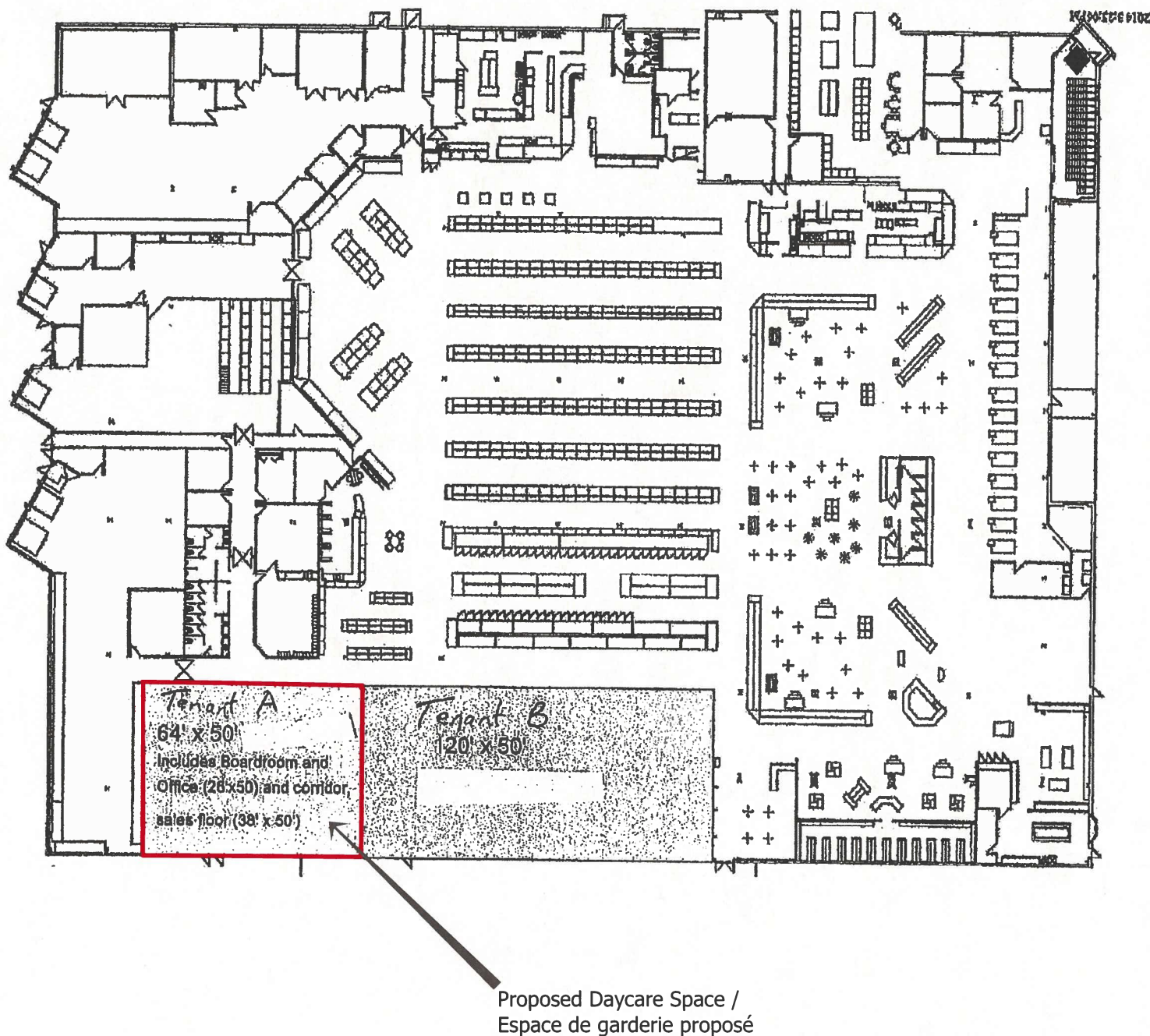
Map \ carte # II

File \ fiche: PR-65-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: chemin 170 Doak Road

Fredericton Direct Charge Co-operative Ltd.
c/o Teena Noble



Floor Plan - Co-Op / Plan d'étage - Co-Op

Fredericton

Community Planning
Planification urbaine

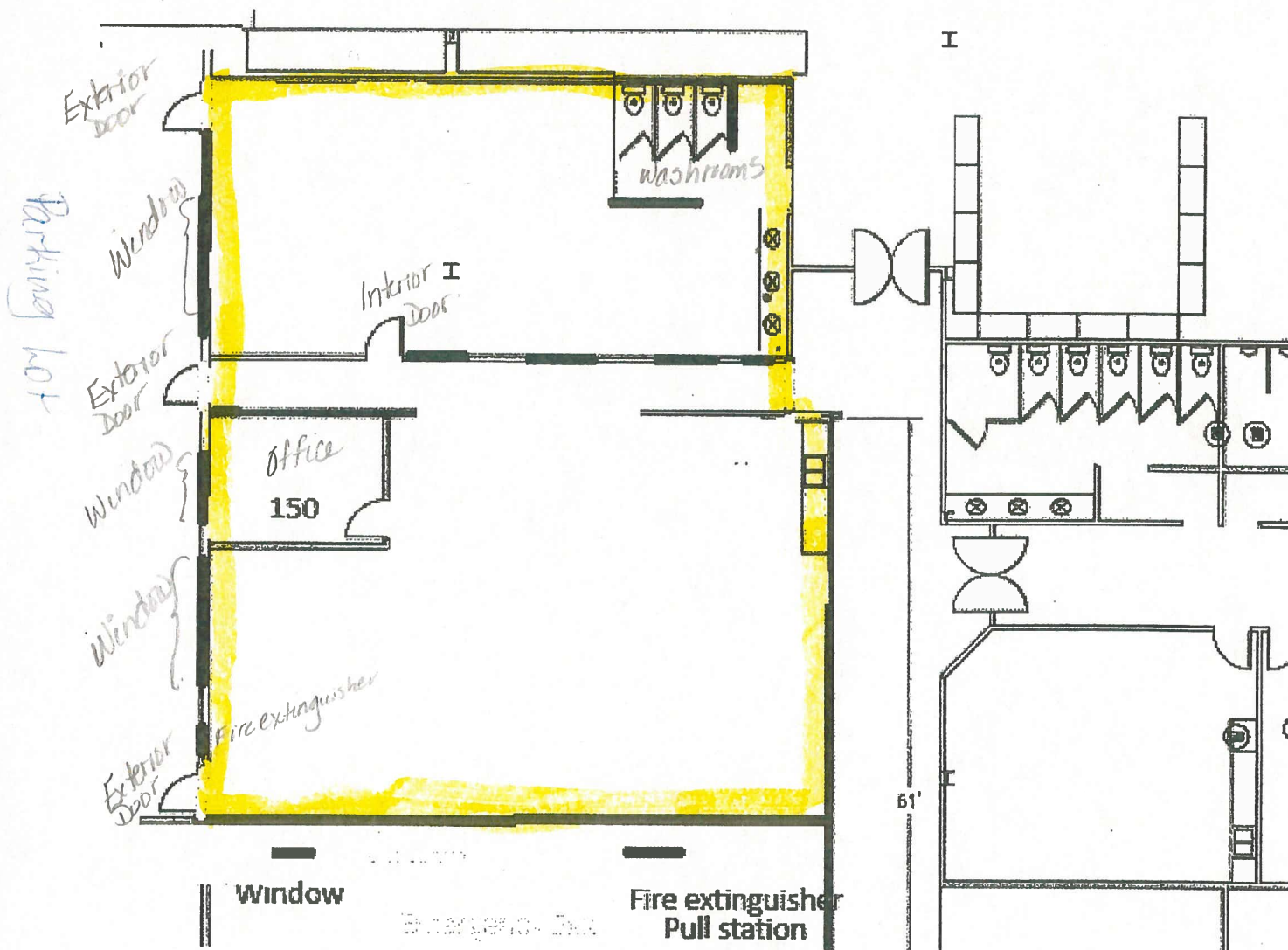
Map \ carte # III

File \ fiche: PR-65-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: chemin 170 Doak Road

Fredericton Direct Charge Co-operative Ltd.
c/o Teena Noble



Floor Plan / Plan d'étage

Fredericton

Community Planning
Planification urbaine

Map \ carte # IV

File \ fiche: PR-65-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: chemin 170 Doak Road

Fredericton Direct Charge Co-operative Ltd.
c/o Teena Noble

To: Planning Advisory Committee

From: Alicia Brown, Planner

Proposal: Rezone property from R-1 to R-3 to permit existing semi-detached, subdivision and associated variances

Property: **214-218 McKnight Street (PID 75332668)**

OWNER: Sonia Wilson & Cindy Dempsey
214-218 McKnight Street
Fredericton, NB
E3A 5W4

APPLICANT: Sonia Wilson

SITE INFORMATION:

Location: South side of McKnight Street, approaching intersection of Ashley Crescent and Attenborough Drive

Context: Mix of low to medium density residential neighbourhood. Semi-detached divided ownership properties to the rear of subject property along Ashley Crescent

Ward No: 2

Municipal Plan: Established Neighbourhoods

Zoning: Residential Zone One (R-1)

Existing Land Use: Semi-Detached Dwelling

Previous Applications: None

EXECUTIVE SUMMARY:

The Applicant is proposing to rezone the property containing a semi-detached dwelling from Residential Zone One (R-1) to Residential Zone Three (R-3), as well as to subdivide the lot into two lots for divided ownership, and lastly, for two lot frontage variances to facilitate the subdivision. The request will not alter the existing use of the property and Staff feel that the request to rezone the property and recognize the existing use is reasonable and support the application subject to terms and conditions.

APPLICATION:

Sonia Wilson has made application to:

- Rezone the property from Residential Zone One (R-1) to Residential Zone Three (R-3);
- A tentative plan of subdivision to create two lots for divided ownership of the existing semi-detached dwelling units; and
- Lot frontage variances of 0.56 metres and 0.29 metres to create each new lot.

PLANNING COMMENTS:

Proposal:

- The existing semi-detached building was constructed in 1999, the City of Fredericton issued a building permit for its construction. There is no record in the files as to why a semi-detached was allowed to be constructed in the R-2 Zone as it was not a permitted use at the time.
- The property owners wish to subdivide the semi-detached dwelling for divided ownership as it will facilitate the current upkeep of the property and future re-sale of the units in the future.

Municipal Plan:

- The request meets the intent of the Established Neighbourhood Land Use Designation of the *Municipal Plan*. The proposal is in keeping with Section 2.2.1(18)(vii) with respect to the requirement that infill development be compatible with adjacent properties. Staff would note that while this isn't a new infill development, the continued use of the property as a semi-detached fits with the existing residential land use pattern in the area.
- Development in this area ranges from low to medium density, this includes a mix of single detached dwellings and semi-detached divided ownership properties to the rear of the property along Ashley Crescent.

Zoning By-law:

The chart below compares the compliance of the property to both the Residential Zone One standards and the Residential Zone Three (R-3) standards:

	<u>R-1 Standard</u>	<u>R-3 Standard</u>	<u>Existing</u>
Lot Area	450 m ²	720 m ²	783 m ²
Lot Frontage	15 m	12 m (divided ownership)	11.44m & 11.71m (variances required)
Lot Coverage	40%	40%	14 %
Parking	1 space	2 spaces	2 spaces

- Staff support this application because it is meant to recognize an existing structure that has fit in with the surrounding built form for over twenty years and has not had any negative impacts on the neighbourhood.

- There is ample space on the outer sides of each unit to provide for the required driveway and each lot meets the lot area for separately owned lots. The request for lot frontage variances of 0.56 metres and 0.29 metres is minimal and would not affect the use of the properties.
- Staff would note that there are two existing sheds on the property, one meets the requirements of *Zoning By-law Z-5* and the other one is within the required 1.2 metre side yard setback and within the required 1m setback from a main building. Staff have reviewed this with the Applicant, and a term and condition of the final approval of the subdivision will require the Applicant to make application for a development permit to ensure that the shed is brought into conformance with the setback requirements.
- In addition, there is an existing fence located within a public utility easement. The onus is on the Applicant to receive approval from the utility allowing the fence to exist or to move the fence off the easement before the final approval of the subdivision. These requirements are noted in the terms and conditions below.

Engineering & Operations

- Water and sanitary sewer servicing records indicate that there is one water and sanitary sewer service lateral installed to the property, installed in 1999. The sanitary lateral was constructed with a wye between the property line and the building, providing a maintenance opportunity for each unit. Water and sewer staff indicate that a second water service was also installed between the property line and the building in 2025 to provide a water meter for each unit.
- Individual servicing to each unit is a typical requirement for a multi-unit building that contains separate ownership of units to ensure that services can be adequately maintained throughout their lifecycle. At the time of future renewal of the water and sewer mains in the right-of-way, a second service to fully separate the services will be installed.
- There are no concerns from a traffic perspective.
- The 8% land dedication is not required for the subdivision as there is an inhabited building on the property.
- In summary, Staff support the request as it will not alter the existing use of the property and will bring the property into conformance with the existing zoning of the neighbouring properties.

RECOMMENDATION:

It is recommended that the application submitted by Sonia Wilson to rezone the property at 214-218 McKnight Street from Residential Zone One (R-1) to Residential Zone Three (R-3); as well as to subdivide the lot into two lots for divided ownership, and lastly, for lot frontage variances of 0.56 metres and 0.29 metres, be approved subject to the following terms and conditions:

- a) The site be developed substantially in accordance with Map II attached to PR 71/25 to the satisfaction of the Development Officer;
- b) Each of the two lots are to be encumbered by a private easement to facilitate the maintenance of the water and sanitary services if the services. Locates will be required to

identify the location of the water and sanitary sewer services to the units. The easement is to be in place in conjunction with the approval of the subdivision plan.

- c) Municipal services (water and sewer) shall be to the satisfaction of the Director of Engineering and Operations, prior to the approval of the subdivision plan;
- d) The Applicant is required to obtain a development permit to facilitate the placement of the shed in a location that meets the Zoning By-law; and,
- e) The Applicant is required to move the existing fence off the public utility easement or obtain a letter of comfort from the owner of the public utility easement permitting the location of the fence.

Prepared by:

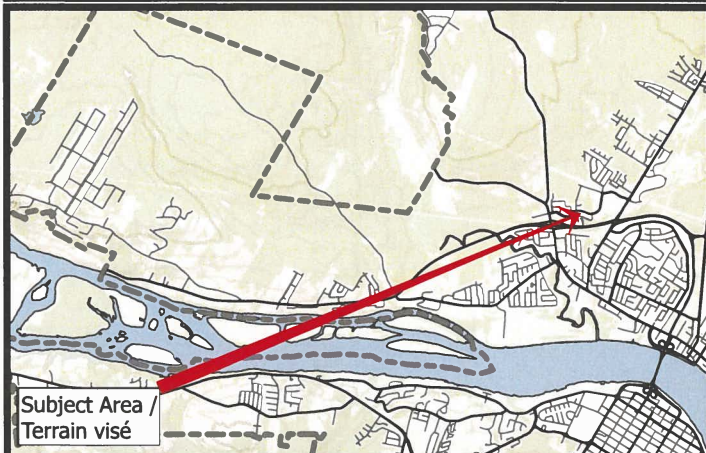
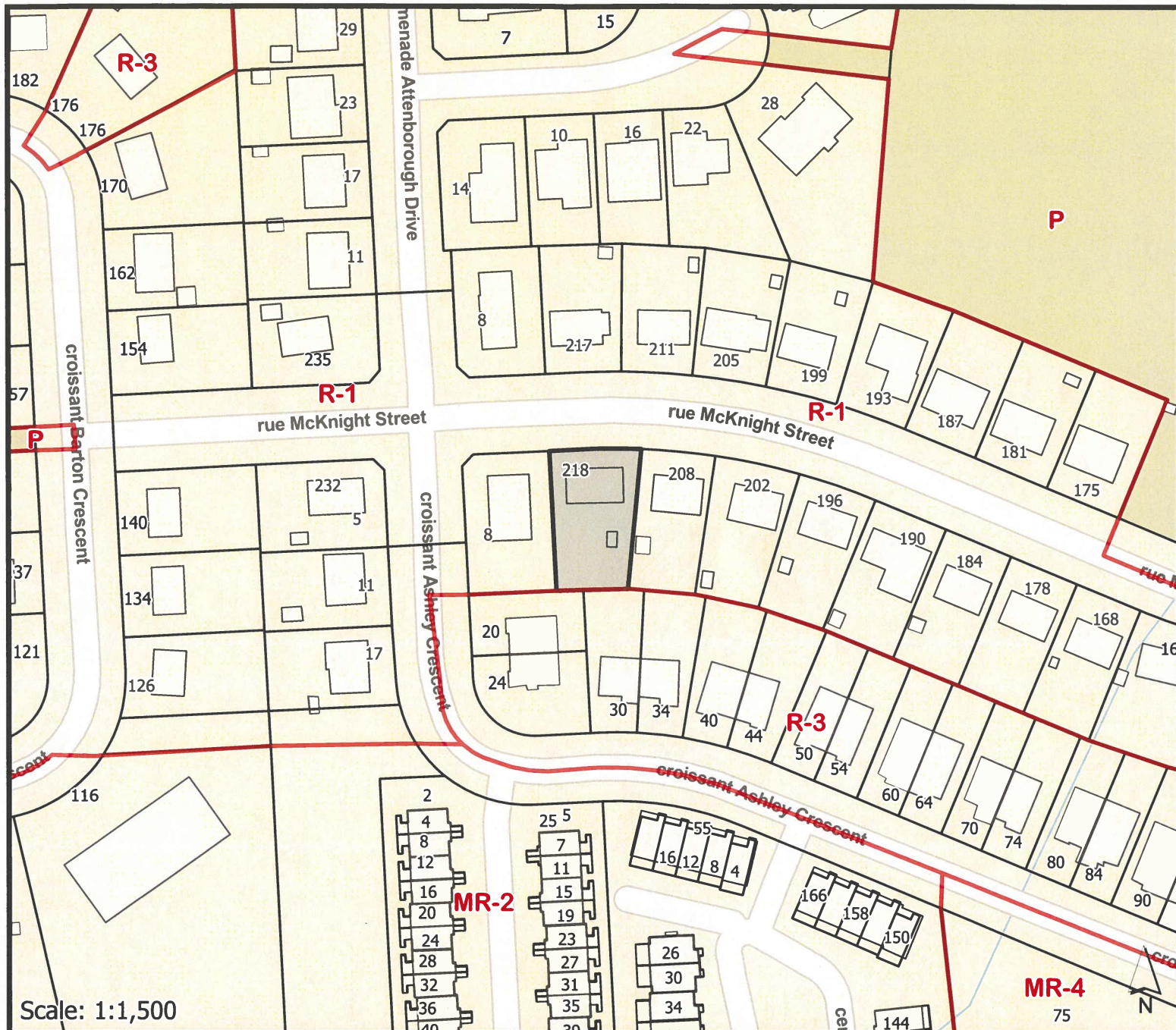
Approved by:

For Melisa Tang Awy

Alicia Brown, MCIP
Planner, Community Planning

Marcello Battilana

Marcello Battilana, MCIP
Manager, Community Planning



 Subject Property / Propriété visée

Rezoning from Residential Zone 1 (R-1) to Residential Zone 3 (R-3) and 0.56m and a 0.29m lot frontage variance, to permit the subdivision of an existing semi-detached dwelling along the common party wall.

Rezonnement de la zone résidentielle 1 (R-1) à la zone résidentielle 3 (R-3) et dérogation de 0,85 mètre et à 0,29 mètre la largeur minimale de façade du lot, afin de permettre le lotissement du bâtiment jumelé existant le long du mur mitoyen.

Fredericton

Community Planning
Planification urbaine

Map \ carte # I

File \ fiche: PR-71-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: rue 214 McKnight Street

Sonia Wilson & Cindy Dempsey
(c/o Sonia Wilson)

**0.29 metre lot frontage variance required /
Une dérogation de 0,29 mètre à la
largeur minimale de façade est requise.**

SUBDIVISION PLAN DEMPSEY AND WILSON SUBDIVISION 25-1

CREATING

LOT 25-122 & LOT 25-123

214/218 MCKNIGHT STREET
CITY OF FREDERICTON
COUNTY OF YORK
PROVINCE OF NEW BRUNSWICK

NOTES
1. AZIMUTHS ROUNDED TO NEAREST 10 SECONDS.
2. ALL DIMENSIONS ARE EXPRESSED IN METRES AND DECIMALS THEREOF AND MAY BE CONVERTED TO FEET BY DIVIDING BY 0.3048.
3. ALL DISTANCES AND DIRECTIONS ARE CALCULATED UNLESS NOTED OTHERWISE.
4. AZIMUTHS ARE N.B. GRID NORTH DERIVED FROM GPS OBSERVATIONS.
5. COMBINED SCALE FACTOR OF 1.0000 APPLIED.
THIS PLAN ALSO SERVES AS THE SURVEYOR'S REAL PROPERTY REPORT.
6. BUILDING LINES SHOWN HEREON ARE TO THE EXTERIOR LIMITS OF THE SIDING, FOUNDATION AND PERPENDICULAR TO PROPERTY SIDELINES.

LEGEND
STANDARD SURVEY MARKER FOUND
STANDARD SURVEY MARKER PLACED
IRON PIPE FOUND
ROUND IRON BAR FOUND
SQUARE IRON BAR FOUND
WOODEN SURVEYOR'S POST
CALCULATED POINT
UTILITY POLE
SURVEY SYSTEM COORDINATE MONUMENT
TABULATED COORDINATE VALUE
BOUNDARIES DEALT WITH BY THIS PLAN
EASEMENT
N.B.S. SURVEYOR NUMBER
DEED TRACT LINE
PUBLIC UTILITY EASEMENT
LOCAL GOVERNMENT SERVICES EASEMENT
SQUARE METRES
HECTARE
SMB PARCEL IDENTIFIER
DOCUMENTS ARE FILED IN THE COUNTY REGISTRY OF PROPERTY LOCATION

#356
L.S.S.E.
46 m
Hc
PID 12345678
DOCUMENTS ARE FILED IN THE COUNTY REGISTRY OF PROPERTY LOCATION

SURVEYOR'S CERTIFICATE
I, H. MURDOCK MACALLISTER, DO HEREBY CERTIFY THAT THE INFORMATION SHOWN FOR THE BOUNDARIES BEING DEALT WITH ON THIS PLAN IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.
JULY 15, 2025
DATE SURVEYED

SCALE 1:250
3.75 0 2.5 5 7.5 10 12.5 metres

CONSULTING ENGINEERS & SURVEYORS
SURTEK GROUP LTD.
101-411 St. Mary's Street
Fredericton, NB E3A 8H4
Phone: 454-7044
Fax: 454-1147
Email: Office@SurTek.ca
DRAWING: 25353SUB FILE No.: 25353

DRAINAGE EASEMENTS RESTRICTIONS
1. THE LOTS ON THIS SUBDIVISION PLAN THAT ARE BENEFITTED OR BURDENED BY A DRAINAGE EASEMENT ARE SUBJECT TO THE DRAINAGE EASEMENT RESTRICTIVE COVENANT FILED WITH SERVICE NEW BRUNSWICK AS C-961.
2. THE CITY OF FREDERICTON IS NOT RESPONSIBLE FOR THE OPERATION OR MAINTENANCE OF THE DRAINAGE SWALES LOCATED ON THE DRAINAGE EASEMENTS.
3. THE CITY OF FREDERICTON ASSUMES NO OBLIGATION TO ENFORCE COMPLIANCE WITH THE DRAINAGE EASEMENT RESTRICTIVE COVENANT.

ASHLEY CRESCENT
PID 75332791 PLAN 200452
20.07m WIDE (PUBLIC)

ALEXANDER PHILLIP GREEN
LOT 94-12
PLAN 200452
REG. 1995-06-13
PID 75332650

TABLE OF N.B. CO-ORDINATE VALUES

Point	Easting	Northing	Description
85	2487638.995	7443485.841	LOT CORNER
100	2487637.191	7443522.372	LOT CORNER
101	2487645.996	7443522.807	LOT CORNER
103	2487657.611	7443486.416	LOT CORNER
104	2487661.174	7443522.904	LOT CORNER
106	2487649.722	7443486.172	LOT CORNER
107	2487648.750	7443522.923	LOT CORNER
28149	2487601.199	7437047.558	NBHPN MONUMENT

THE COORDINATE VALUES LISTED ARE NAD83 (CSRS)

SURVEYORS SEAL
ASSOC. OF NEW BRUNSWICK LAND SURVEYORS PLAN VALIDATION No.

KEY PLAN SCALE 1:500

MCKNIGHT STREET
PID 75332791 - PLAN 200452
20.17m WIDE (PUBLIC)

Plan of Subdivision / Plan de lotissement



Front - McKnight Street (North) / Façade – rue McKnight (nord)



Rear (South) / Arrière (sud)



Left side (East) / Côté gauche (est)

Elevations / Élévations

To: Rodney Blanchard and Members of the Planning Advisory Committee

From: Fredrick Van Rooyen, Senior Planner

Date: August 15, 2025

Subject: South Core Secondary Municipal Plan Adoption, Municipal Plan Amendments, Zoning By-law Amendments, and Subdivision By-law Amendments

I. PROPOSAL

The proposal for the adoption of the South Core Plan includes the following components:

1. Repeal of the Residential Town Plat Secondary Municipal Plan;
2. Adoption of the South Core Secondary Municipal Plan and associated Urban Design Guidelines;
3. Municipal Plan Amendments:
 - Boundary and legend adjustments to Schedule 1 (Urban Structure Map);
 - Boundary updates to Schedule 2 (Land Use Map);
 - Updating the background description for the South Core;
 - Addition of policy reference to South Core Secondary Municipal Plan and New Brunswick Exhibition Grounds (NBEX) Secondary Municipal Plan;
 - Update to mid-rise and high-rise terms in the glossary;
4. Zoning By-law Amendments:
 - Updating references to South Core and new Schedules for Neighbourhood, Corridor, and Mid-Town Areas;
 - Addition of Multiplex and POPS definitions;
 - Parking adjustments within the Corridor Area;
 - Adjustment to lot standards in the R-4, TP-4, and TP-6 zones;
 - New landscaped area standards for “sponge area”;
 - Clarification to the 45 degree angle height limit;
 - Increased density within the Corridor and Mid-Town Area;
 - Implement building height and transition policies for the Corridor and Mid-Town Areas; and,
 - Adds all multi-residential building forms in the Mid-Town Area.
5. Subdivision By-law:
 - New exemption for subdivisions from 8% LPP where POPS are provided as an incentive.

II. BACKGROUND

For the last few decades, the Residential Town Plat Secondary Municipal Plan has guided the development of the South Core neighbourhood. It set out a vision of conservation and stability in the established neighbourhood area, while targeting several key brownfield industrial sites with mixed-use and high-density residential development. The Plan's success can largely be seen today in thriving businesses, historic residential character, and new multi-residential buildings.

However, as the City of Fredericton continues to experience unprecedented population growth, an update to the vision for the neighbourhood is critical to properly plan and guide its future. Approximately 6,000 new residents (or 3,200 new housing units) will need to be accommodated in the South Core neighbourhood and New Brunswick Exhibition Grounds site. This is nearly double the current population in this area.

Through extensive public and stakeholder collaboration, the new South Core Plan will enhance the historic character that has defined this area for over a century and will lay the foundation for high-quality new development in the right places. This development also brings with it the services and amenities that will enhance the quality of life and overall experience of living in the South Core neighbourhood.

IV. ENGAGEMENT

Engagement

The South Core Plan process was structured to be transparent and accessible from project kick-off. It is critical that residents and stakeholders feel they are connected to the development of the planning document to both understand and support the policies and proposals therein. A series of public engagement events were conducted from May to October 2024 to gather feedback as the framework of the South Core Plan first started to take shape. These events were clustered in three separate multi-day Design Studios along with pop-up engagement over the summer. Members of the consultant team gathered in one location in the Plan Area to hold stakeholder meetings and public open houses with presentations and workbook exercises.

Two 'What We Heard Report' documents, Summer Engagement Report, and a Survey Results Report summarize the comments received during the three Design Studios, other pop-up engagement sessions, and the survey conducted. These reports are attached for further information. A final Public Open House was then held in April 2025 to present the draft Plan and provide residents and stakeholders the opportunity to review the documents before proceeding to the adoption process.

In conjunction with the public engagement process, there was robust consultation with other internal City Departments covering aspects from parks and trees to transit and municipal servicing. This collaboration with other internal City Departments has helped shape the policies and proposals contained within the South Core Plan. The plan process has also provided other departments the opportunity to examine the current servicing systems and transportation network as well as explore how future growth will be accommodated. This included looking at potential trail connections, transit routes, and the overall municipal servicing systems, including modelling for the sanitary and water systems. Background details on transportation and municipal services can be found within the attached Technical Background Report. These considerations have then shaped the proposals contained within the South Core Plan, ensuring that upgrades to municipal systems are prioritized to support infill growth, updates to the transit network serve the densest population areas, and supporting active transportation connections among other improvements.

Recognizing that the South Core Plan is a long range planning document that shapes the development of the area over the next 20 years, additional review and upgrades to the overall system would take place to ensure that our infrastructure is keeping pace with and supporting growth.

IV. ANALYSIS

Amendments to Municipal Plan Z-6

The following amendments are proposed to the Municipal Plan Z-6:

#	Section	Amendment	Rationale
1.	Schedule 1 (Urban Structure Map)	Repeal and replace Schedule 1 (Urban Structure Map) to make boundary and legend adjustments for the South Core Municipal Plan Area and the New Brunswick Exhibition Grounds (NBEX) Secondary Municipal Plan Area.	Administrative amendments to align with new boundaries and plan titles.
2.	Schedule 2 (Land Use Map)	Repeal and replace Schedule 2 (Land Use Map) to make boundary adjustments for the South Core Secondary Plan Area	Administrative amendments to align with new boundaries.
3.	2.2.1 South Core	Background The South Core, largely comprised of the Town Plat, includes large sites for potential redevelopments, such as the Railway Lands and the New Brunswick Exhibition Grounds (NBEX). Arterial and collector roads will accommodate greater intensification, while more established residential areas can absorb more moderate intensification and appropriate infill development. Capital Exhibit Centre (formerly known as the Fredericton Exhibition Centre or "FREX") site, as well as small sites for modest intensification along the principal corridors in the area. [Note: At the time of adoption of this Plan a committee is in place to consider a future redevelopment concept for the Capital Exhibit Centre site]. Given the constrained boundaries of the City Centre, and therefore its limited capacity for residential growth, achieving significant intensification in the South Core will be critical to maximizing downtown vitality. The South Core Plan Area is intended to accommodate over 6,000 new residents by 2051. 2.2.1(12) Prior to the adoption of a new South Core Secondary Municipal Plan, new development or redevelopment within the Town Plat area of the South Core shall occur in accordance with the Town Plat Secondary Municipal Plan. 2.2.1(12) New development or redevelopment within the South Core shall occur in accordance with the South Core Secondary Municipal Plan. 2.2.1(13) Prior to adoption of a new South Core Secondary Municipal Plan, mid- or high-rise building design should comply with the City Centre Built Form Design Guidelines. 2.2.1(13) New development or redevelopment on the New Brunswick Exhibition Grounds (NBEX) shall occur in accordance with the New Brunswick Exhibition Grounds (NBEX) Secondary Municipal Plan.	Update to South Core background to reflect more current policy direction. Policy to ensure development is in accordance with South Core Municipal Plan and NBEX Secondary

		2.2.1(15) Small-scale commercial uses and professional offices will be encouraged along Smythe Street, York Street, and Regent Street in low-rise mixed-use buildings to achieve a more complete neighbourhood and provide a transition to the downtown. 2.2.1(16) Compatible residential infill will be encouraged along Smythe Street, York Street, and Regent Street, in low- to mid-rise buildings to achieve residential intensification.	Municipal Plan.
4.	2.2.2 Proposals	2.2.2(3) Initiate a new South Core Secondary Municipal Plan which will replace the Town-Plat Secondary Municipal Plan, to provide a detailed framework for new development, including public realm and built form design guidelines, access and circulation, land use distribution, heights and densities, and servicing requirements, that support positive urban development, including: i. The redevelopment of the Capital Exhibit Centre as a short-term goal; and, ii. The redevelopment of the Railway Lands as a short-term goal.	The South Core Secondary Plan implements this proposal.
5.	Glossary	Mid-Rise A building between 3-5 4 and 6 7 storeys in height High-Rise A building greater than 6 7 storeys in height	Adjustments to mid-rise and high-rise terms to align with South Core policies.

Amendments to Zoning By-law Z-5

The following text amendments are proposed to Zoning By-law Z-5:

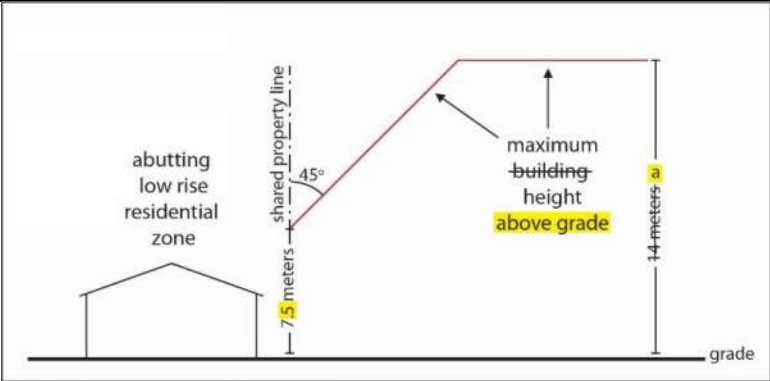
#	Section	Amendment	Rationale
1.	Table of Contents	7.1(4) Lot Consolidation in the Town-Plat South Core	Update Town Plat references with applicable South Core references throughout.
2.	Section 3 Definitions	Add: Multiplex means a <i>building</i> containing more than two <i>dwelling units</i> where only individual outside access is provided to all or a portion of the <i>dwelling units</i>. (<i>multiplex</i>) POPS means a privately-owned public space provided as part of a <i>development</i> that is made accessible to the public through legal easements and/or agreements.	Replaces Three or Four Unit Dwelling with Multiplex in the R-4, TP-4, and TP-6 zones and add POPS as a defined term.

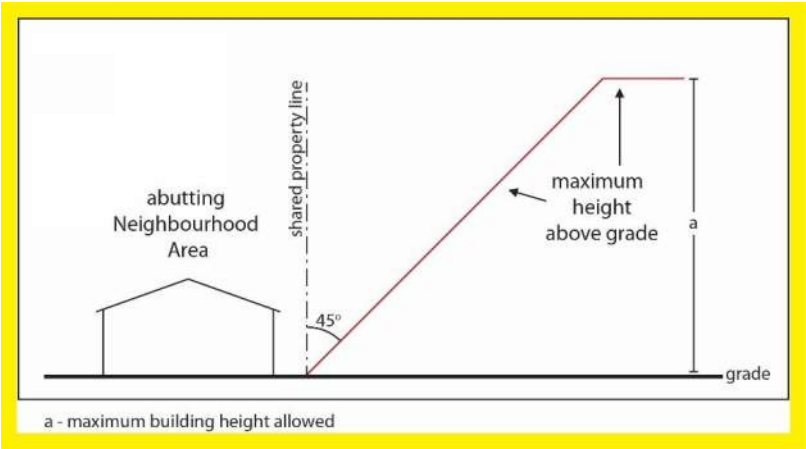
	Section 8.1 Comparative Use Chart	Delete: Three or Four Unit Dwelling means a building containing either 3 or 4 dwelling units. (habitation à trois ou quatre logements) Replace Three or Four Unit Dwelling with Multiplex in the Comparative Use Chart				
3.	Section 5.2(10) Design of Parking Lots	(vii) parking lots shall be screened and separated as follows: (E) along the rear property line of parking lots in the Residential Town-Plat Planning Neighbourhood Area (Schedule 6 13): by a minimum 4 metre wide landscaped strip consisting of coniferous and deciduous trees, shrubs, hedges, 2 metre high opaque fence or a combination thereof; and, (F) along the side property line adjacent to a parking lot in the Residential Town-Plat Planning Neighbourhood Area (Schedule 6 13): by a 2 metre high opaque fence.	Alignment to area within South Core Plan			
4.	Section 5.2(12)(e) Required Parking	Table (i) Residential Parking Requirements Row: (A) Apartment Building or Stacked Townhouse and (B) Dwelling Unit Column: Area 1 Inside the Town-Plat and lots fronting on the north side of Beaverbrook Street Corridor Area (Schedule 14): • Per 2 or less BR unit: 0.5 sp • Per 3 or more BR unit: 1 sp Inside the Neighbourhood Area (Schedule 13): • Per bachelor or 1-BR unit: 0.5 sp • Per 2 or more BR unit: 1 sp Outside the Town-Plat above areas: • Per bachelor or 1-BR unit: 0.75 sp • Per 2 or more BR unit: 1.25 sp Row: (L) Three or Four Unit Dwelling Multiplex Table (ii) Commercial Parking Requirements <table><tr><td>Development</td><td>CBD and Corridor Area</td><td>Outside CBD and Corridor Area</td></tr></table>	Development	CBD and Corridor Area	Outside CBD and Corridor Area	Reduced parking requirement for residential and commercial uses within Corridor Area.
Development	CBD and Corridor Area	Outside CBD and Corridor Area				
5.	Section 7 Regulations Applying to Residential Uses	7.1(4) Lot Consolidation in the Town-Plat South Core (a) Increasing lot area and lot frontage to obtain additional density rights through consolidation of land is not permitted within the Residential Town-Plat Planning Neighbourhood Area (Schedule 6-13) except for lots fronting on Smythe Street, York Street or Regent Street. 7.1(5) Converted Dwellings (b) Converted Dwellings in the Town-Plat South Core and Central Business District (c) Converted Dwellings with Additions in the Town-Plat South Core	Update to South Core Plan reference and associated areas			

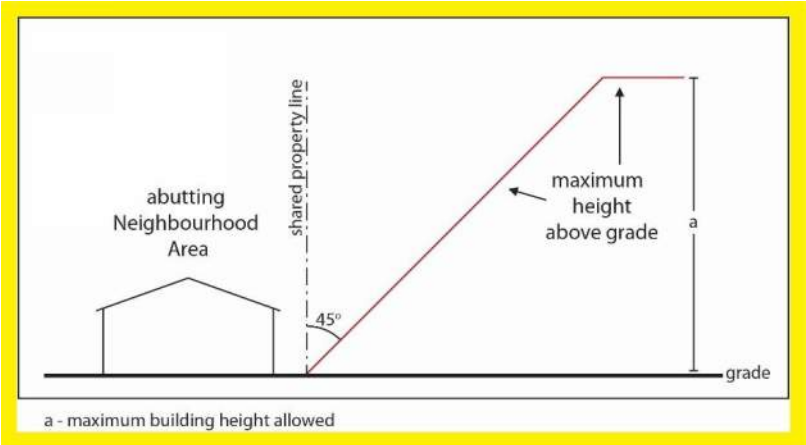
6.	Section 8.7(1) Purpose Section 8.8(1) Purpose Sections 8.4(2)(a), 8.7(2)(a), 8.8(2)(a) Permitted Uses Section 8.4(3) Use Rules Section 8.7(3) Use Rules Section 8.8(3) Use Rules Section 8.4(4) Standards Sections 8.7(4) and 8.8(4) Standards	The TP-4 Zone: • accommodates residential <i>development</i> primarily in the form of new or converted dwellings containing up to 4 6 dwelling units and new buildings containing up to 4 dwelling units; The TP-6 Zone: • accommodates residential <i>development</i> primarily in the form of converted dwellings containing up to 6 8 dwelling units and new buildings containing up to 4 6 dwelling units; Delete: Three or Four Unit Dwelling Add: Multiplex (in alphabetical order) Add; (e) multiplex buildings shall contain a maximum of 4 dwelling units. (a) converted dwellings shall contain a maximum of 4 6 dwelling units; (b) multiplex and townhouse buildings shall contain a maximum of 4 dwelling units; (a) converted dwellings shall contain a maximum of 6 8 dwelling units; (b) multiplex and townhouse buildings shall contain a maximum of 4 6 dwelling units; (a) Lot Area (MIN) (i) Three-unit Dwelling Multiplex 630 m² (3 unit) (ii) Four-unit Dwelling 690 m² (4 unit) (b) Lot Frontage (MIN) (i) Three-unit Dwelling Multiplex 21 m (3 unit) (ii) Four-unit Dwelling 23 m (4 unit) (a) Density (Lot Area per Dwelling Unit) & Lot Area (i) Townhouse: MAX 55 dwelling units per hectare (MIN 180 m² per dwelling unit) (ii) Three- or Four-Unit Dwelling MIN 630 m² (3 unit) MIN 690 m² (4 Unit) (ii) Multiplex: MAX 62 dwelling units per hectare (MIN 161 m² per dwelling unit)	Allows up to 6 units in converted dwellings in the TP-4 zone. Allows up to 8 units in converted dwellings and up to 6 units in new buildings in the TP-6 zone.
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8.	Section 8.6(4) Standards	(f) Landscaped Area (i) In accordance with Section 4.2(2) (ii) Despite Section 5.2(10)(vii)(E), a contiguous <i>soft landscaped area</i> comprising at least 10% of the <i>lot area</i> shall be provided abutting the <i>rear property line</i> or portion thereof.	Requires a contiguous soft landscaped area to be provided as a “sponge area” along the rear property line of TP-2 zoned properties.																																																
9.	Sections 8.7(4) and 8.8(4) Standards	(h) Landscaped Area (i) 45 m² per dwelling unit required at grade. The minimum <i>landscaped area</i> required at grade is 35% of the <i>lot area</i>. (ii) In accordance with Section 4.2(2) (iii) Despite Section 5.2(10)(vii)(E), a contiguous <i>soft landscaped area</i> comprising at least 10% of the <i>lot area</i> shall be provided abutting the <i>rear property line</i> or portion thereof.	Adopts multi-residential zone landscape standard for the TP-4 and TP-6 zones for more flexibility. Adds the contiguous soft landscaped area (sponge) rule as above for TP-4 and TP-6 zones.																																																
10.	Sections: 9.3(4)(e)(ii), 9.5(4)(g)(ii), 9.6(4)(g)(ii),	(ii) Where the <i>lot</i> abuts a <i>low rise residential zone</i>, <i>building height</i> is measured beginning at a point 7 7.5 metres above <i>grade</i> at the shared <i>property line</i> and increases proportionally at a 45 degree angle to the maximum <i>building height</i> allowed.	Clarification and slight increase to the 45 degree angle height																																																

	11.6(4)(h)(ii), 11.13(4)(h)(ii) 12.2(4)(e)(ii), 12.3(4)(e)(ii), 12.4(4)(e)(ii) Standards	 a - maximum building height allowed	limit for more flexibility in all applicable zones.
11.	Sections: 9.5(4)(g)(iii), 9.6(4)(g)(iii), 11.6(4)(h)(iii), 11.13(4)(h)(iii) 12.2(4)(e)(iii), 12.3(4)(e)(iii), 12.4(4)(e)(iii) Standards	Delete entirety of Sections: 9.5(4)(g)(iii), 9.6(4)(g)(iii), 11.6(4)(h)(iii), 11.13(4)(h)(iii), 12.2(4)(e)(iii), 12.3(4)(e)(iii), 12.4(4)(e)(iii) (sections outlining the 45 degree height limit diagram adjacent to MR-1, MR-2, MX-1 or MX-2 zone).	Deletes the 45 degree angle height limit adjacent to zones permitting mid-rise building forms as it is only needed adjacent to low rise zones.
12.	Section 9.5(4) Standards	(a) Density (Lot Area per Dwelling Unit) (i) MAX 100 <i>dwelling units</i> per hectare (MIN 100 m ² per <i>dwelling unit</i>) except , (ii) Buildings located within the Corridor Area as shown on Schedule 14: MAX 123 <i>dwelling units</i> per hectare (MIN 81m ² per <i>dwelling unit</i>) (iii) Despite Section (ii) above, buildings located within the Corridor Area as shown on Schedule 14 that provide at least 50% of the required <i>parking</i> underground: MAX 150 <i>dwelling units</i> per hectare (MIN 66m ² per <i>dwelling unit</i>)	Increased density allowance within Corridor Area (MR-4 zone).
13.	Section 9.5(4) Standards	(e) Building Location and Design (i) Within the Residential Town-Plan South Core (Schedule 6) the <i>building</i> shall be located on the <i>lot</i> such that a minimum of 60% of the <i>façade</i> length facing a public <i>street</i> is located within the minimum and maximum <i>building setback</i> from a <i>front property line</i> . (vii) Buildings located within the Corridor Area as shown on Schedule 14 that are more than four (4) <i>storeys in height</i> shall incorporate a continuous step back at the 3 rd , 4 th or 5 th storey level to a minimum depth of 2 metres on any <i>façade</i> that faces a public <i>street</i> .	Implements the 2m building step back in Corridor Area policy (MR-4 zone).
14.	Section 9.5(4) Standards	(f) Building Setbacks (MIN) (i) From a <i>front property line</i> : 3 metres, except : (A) within the Residential Town-Plan South Core (Schedule 6):	Update to South Core Plan reference
15.	Section 9.5(4) Standards	(g) Building Height (MAX) (i) 18 metres, except :	Implements Corridor Area building height

		(A) Buildings located within the Corridor Area as shown on Schedule 14 shall comply with the following: (I) Minimum <i>building height</i>: 8 metres; (II) Maximum <i>building height</i>: 22.5 metres, except: (III) Maximum <i>building height</i> on a <i>corner lot</i>: 25.5 metres (iii) Despite Section (ii) above, where the <i>lot</i> is located within the Corridor Area as shown on Schedule 14, the 45 degree angle <i>height</i> limit does not apply where the abutting <i>lot</i> is also located within the Corridor Area. (iv) Where the <i>lot</i> is located within the Corridor Area as shown on Schedule 14 and abuts a <i>lot</i> located within the Neighbourhood Area as shown on Schedule 13 with its <i>rear property line</i>, <i>building height</i> is measured beginning at a point at <i>grade</i> at the shared <i>property line</i> and increases proportionally at a 45 degree angle to the maximum <i>building height</i> allowed. 	and transition policies (MR-4 zone).
16.	Section 9.6(4) Standards	(e) Building Location and Design (i) Within the Residential Town-Plat South Core (Schedule 6) the <i>building</i> shall be located on the <i>lot</i> such that a minimum of 60% of the <i>façade</i> length facing a public <i>street</i> is located within the minimum and maximum <i>building setback</i> from a <i>front property line</i>. (viii) Despite Section (vii) above, <i>buildings</i> located within the Mid-Town Area as shown on Schedule 15 that are more than four (4) <i>storeys in height</i> shall incorporate a continuous step back at the 3rd, 4th or 5th storey level to a minimum depth of 2 metres on any <i>façade</i> that faces a public <i>street</i>. (ix) <i>Buildings</i> located within the Mid-Town Area as shown on Schedule 15 between Hanwell Road and Rookwood Avenue, shall provide ground floor entrances that face the Cross Town Trail where the <i>building</i> or portion thereof is located within 15 metres of the <i>property line</i> abutting the Cross Town Trail. (f) Building Setbacks (MIN) (i) From a <i>front property line</i>: 3 metres, except: (A) within the Residential Town-Plat South Core (Schedule 6):	Implements the 2m building step back in Mid-Town Area policy (MR-5 zone).

		(g) Building Height (MAX) (i) 24 metres, except: (A) Buildings located within the Mid-Town Area as shown on Schedule 15 shall comply with the following: (I) Minimum <i>building height</i>: 9 metres (II) Maximum <i>building height</i>: 37.5 metres, except: (III) Maximum <i>building height</i> on a <i>corner lot</i>: 46.5 metres (B) Despite (A) above, <i>buildings</i> located within the Mid-Town Area as shown on Schedule 15 between Hanwell Road and Rookwood Avenue, shall be limited to a maximum <i>building height</i> of 14 metres where the <i>building</i> or portion thereof is located within 15 metres of the <i>property line</i> abutting the Cross Town Trail. (iii) Where the <i>lot</i> is located within the Mid-Town Area as shown on Schedule 15 and abuts a <i>lot</i> located within the Neighbourhood Area as shown on Schedule 13 with its <i>rear property line</i>, <i>building height</i> is measured beginning at a point at <i>grade</i> at the shared <i>property line</i> and increases proportionally at a 45 degree angle to the maximum <i>building height</i> allowed. 	Implements Mid-Town Area building height and transition policies (MR-5 zone).
17.	Section 11.11 Mixed Use Zone One	11.11(1) Purpose The MX-1 Zone: • accommodates <i>buildings</i> containing limited commercial uses and at least 1 <i>dwelling unit</i>; • allows for the <i>conversion</i> and adaptive reuse of existing <i>buildings</i> in order to help maintain the character of the area; • allows up to a maximum of 3 <i>secondary dwelling units</i> on a <i>lot</i>; and, • provides a limited mix of <i>uses</i> primarily to facilitate a transition between the City Centre and the Town-Plan South Core.	Update to South Core Plan reference
18.	Section 11.12(4) Standards	(d) Density (Lot Area per Dwelling Unit) (i) MAX 62 <i>dwelling units</i> per hectare (MIN 161 m² per <i>dwelling unit</i>) (ii) Buildings located within the Corridor Area as shown on Schedule 14: MAX 123 <i>dwelling units</i> per hectare (MIN 81 m² per <i>dwelling unit</i>) (iii) For <i>affordable housing units</i>: MAX 224 <i>dwelling units</i> per hectare (MIN 45 m² per <i>dwelling unit</i>)	Increased density allowance within Corridor Area (MX-2 zone).

		(e) Building Height (MAX) (i) 13 metres, except: (A) Buildings located within the Corridor Area as shown on Schedule 14 shall comply with Section 9.5(4)(g) and Section 9.5(4)(e)(vii).	Implements the building height, transition and step back policies in Corridor Area (MX-2 zone).
19.	Section 11.13(2) Uses Section 11.13(3) Use Rules Section 11.13(4) Standards	(a) Permitted Uses Add: Apartment Building, Stacked Townhouse and Townhouse (in alphabetical order) 11.13(3) Use Rules (d) Apartment building, stacked townhouse and townhouse shall only be permitted within the Mid-Town Area as shown on Schedule 15. (d) Density (Lot Area per Dwelling Unit) (i) MAX 100 <i>dwelling units</i> per hectare (MIN 100 m² per <i>dwelling unit</i>) (ii) Buildings located within the Mid-Town Area as shown on Schedule 15: MAX 160 <i>dwelling units</i> per hectare (MIN 62 m² per <i>dwelling unit</i>) (iii) For <i>affordable housing units</i>: MAX 224 <i>dwelling units</i> per hectare (MIN 45 m² per <i>dwelling unit</i>) (e) Building Setbacks (MIN) (i) From a <i>property line</i> that abuts a public <i>street</i>: 3 metres (ii) From any other <i>property line</i>: 3 metres, except: (A) where the <i>property line</i> abuts a <i>low rise residential zone</i> or P-zone: 10 metres (h) Building Height (MAX) (i) MIN 9 metres AND MAX 24 metres, except: (A) Buildings located within the Mid-Town Area as shown on Schedule 15 shall comply with Section 9.6(4)(g). (i) Building Design (iii) Despite Section (ii) above, buildings located within the Mid-Town Area as shown on Schedule 15 that are more than four (4) <i>storeys in height</i> shall incorporate a continuous step back at the 3rd, 4th or 5th storey level to a minimum depth of 2 metres on any <i>façade</i> that faces a public <i>street</i>. (iv) Apartment building, stacked townhouse and townhouse buildings located within the Mid-Town Area as shown on Schedule 15 between Hanwell Road and Rookwood Avenue, shall provide ground floor entrances that face the Cross Town Trail where the <i>building</i> or portion thereof is located within 15 metres of the <i>property line</i> abutting the Cross Town Trail.	Adds stand-alone multi-residential uses limited to the Mid-Town Area (MX-3 zone). Increased density allowance within Mid-Town Area (MX-3 zone). Implements the building height, transition and step back policies in Corridor Area (MX-3 zone).

		(j) Landscaped Area (i) The minimum <i>landscaped area</i> required at <i>grade</i> is 35 % of the <i>lot area</i>. (ii) Despite Section (i) above, <i>development</i> located within the Mid-Town Area as shown on Schedule 15: (A) may provide the required <i>landscaped area</i> above <i>grade</i>, and (B) shall provide a minimum <i>landscaped area</i> of 8% of the <i>lot area</i> where privately-owned public space (<i>POPS</i>) is provided as part of the <i>development</i>. (iii) In accordance with Section 4.2(2)	
20.	Section 11.16(1) Purpose	Purpose ... facilitates a transition in land use between the City Centre's east end and the Town-Plat South Core.	
21.	Section 20 Schedules	LIST OF SCHEDULES 1 City Centre Plan Area & Central Business District 2 Maximum Building Height 3 Group Homes 4 Special Sign Districts A, B, C 5 Parking Districts 6 Residential Town-Plat South Core Planning Area ... 13 Neighbourhood Area 14 Corridor Area 15 Mid-Town Area Delete Schedule 6 and replace with new Schedule 6 South Core Planning Area (attached as Map IV) Add new Schedule 13 Neighbourhood Area (attached as Map V) Add new Schedule 14 Corridor Area (attached as Map VI) Add new Schedule 15 Mid-Town Area (attached as Map VII)	Adds new Schedules to reference South Core land use areas in the zoning by-law.

Amendment to Zoning By-law Z-5 Maps (Schedule 11)

An amendment to the Zoning Maps (Schedule 11) is required to rezone land within the Neighbourhood Area (see attached Map I) from R-3 and R-4 to TP-4 and land within the Mid-Town Area from NC and DC to MX-3 (see attached Maps II and III) to better align with the policy direction of the South Core Secondary Municipal Plan.

Amendments to Subdivision By-law Z-4

The following text amendments are proposed to Subdivision By-law Z-4:

#	Section	Amendment	Rationale
1.	By-Law Z-4, A Subdivision By-Law: Section 3 Land For Public Purposes	Add and renumber accordingly: 3.03 Where approval of a plan of subdivision is sought on land located within the Mid-Town Area pursuant to the South Core Secondary Municipal Plan, the condition of approval of the plan of subdivision prescribed by Sections 3.01 and 3.02 above shall not apply where privately-owned public space (POPS) is provided on the subdivided land in lieu of the 8% public land or cash payment requirement.	Exempts subdivisions from the 8% LPP requirement where POPS are provided as an incentive.

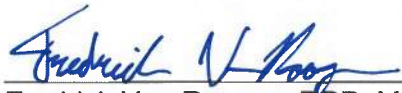
V. RECOMMENDATION

- A. That By-law No. Z-8, A By-law to Adopt the Residential Town Plat Secondary Municipal Plan, be repealed and replaced with By-law No. Z-8, A By-law to Adopt the South Core Secondary Municipal Plan, the contents of which are enclosed as Schedule A attached to P.R. 59/25.
- B. That Council adopt by resolution the associated South Core Urban Design Guidelines.
- C. That By-law No. Z-6, A By-law to Adopt a Municipal Plan for the City of Fredericton, be amended by repealing and replacing Schedule 1 (Urban Structure Map) to make boundary and legend adjustments for the South Core Secondary Municipal Plan Area and the New Brunswick Exhibition Grounds (NBEX) Secondary Municipal Plan Area as shown on Schedule B attached to P.R. 59/25.
- D. That By-law No. Z-6, A By-law to Adopt a Municipal Plan for the City of Fredericton, be amended by repealing and replacing Schedule 2 (Land Use Map) to make boundary adjustments for the South Core Secondary Municipal Plan Area as shown on Schedule C attached to P.R. 59/25.
- E. That By-law No. Z-6, A By-law to Adopt a Municipal Plan for the City of Fredericton, be amended by repealing the Background statement of the South Core Designation contained in Section 2.2.1, subparagraphs 2.2.1(12), 2.2.1(13), and the mid-rise and high-rise terms in the Glossary and replacing with a new Background statement, subparagraphs, and Glossary terms as outlined in the attached Schedule D.
- F. That By-law No. Z-6, A By-law to Adopt a Municipal Plan for the City of Fredericton, be amended by repealing subparagraphs 2.2.1(15), 2.2.1(16), and 2.2.2(3) as outlined in the attached Schedule D and renumbering the remaining sections as required accordingly.
- G. That the text amendments to By-law No. Z-5, A Zoning By-law for the City of Fredericton, as outlined in the attached Schedule E be approved.
- H. That the following amendments to the Zoning Maps (Schedule 11) of By-law No. Z-5, A Zoning By-law for the City of Fredericton be approved:

- i. Rezone land from Residential Zone Three (R-3) to Town Plat Zone Four (TP-4) as shown on Map I attached to P.R. 59/25;
- ii. Rezone land from Neighbourhood Commercial Zone (NC) and District Commercial Zone (DC) as shown on Map II attached to P.R. 59/25; and,
- iii. Rezone land from District Commercial Zone (DC) to Mixed-Use Zone Three (MX-3) as shown on Map III attached to P.R. 59/25.

- I. That Schedule 6 Residential Town Plat Planning Area within Section 20 of By-law No. Z-5, A Zoning By-law for the City of Fredericton, be repealed and replaced with a new Schedule 6 South Core Planning Area, as shown on Map IV attached to P.R. 59/25.
- J. That Schedules 13, 14, and 15 be added to Section 20 of By-law No. Z-5, A Zoning By-law for the City of Fredericton, as shown on Maps V, VI, and VII attached to P.R. 59/25.
- K. That the text amendments to By-law Z-4, A Subdivision By-law, as outlined in the attached Schedule F be approved.

Prepared by:



Fredrick Van Rooyen, RPP, MCIP
Senior Planner, Community Planning

Approved by:

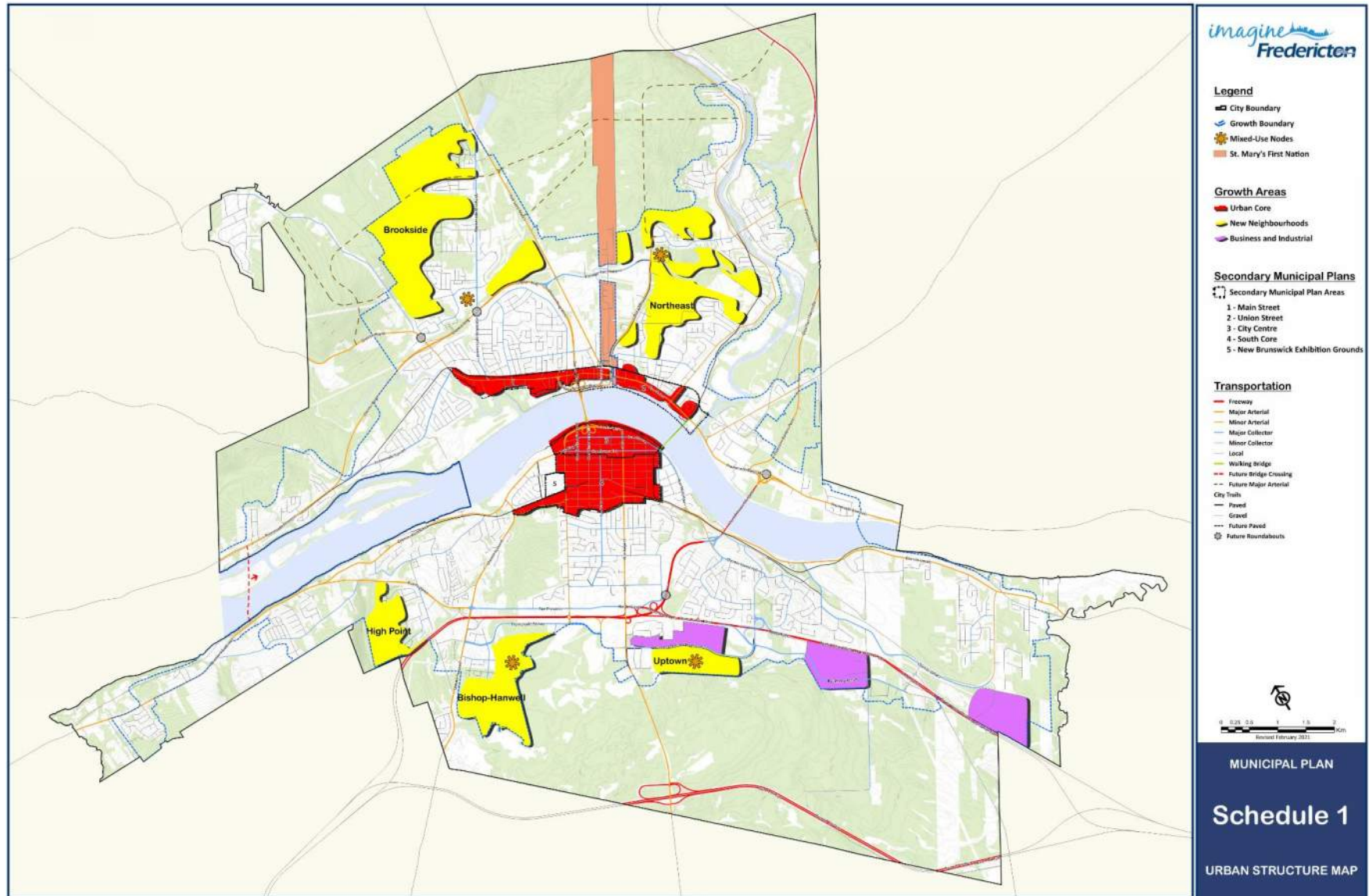


Marcello Battilana, RPP, MCIP
Assistant Director, Planning & Development

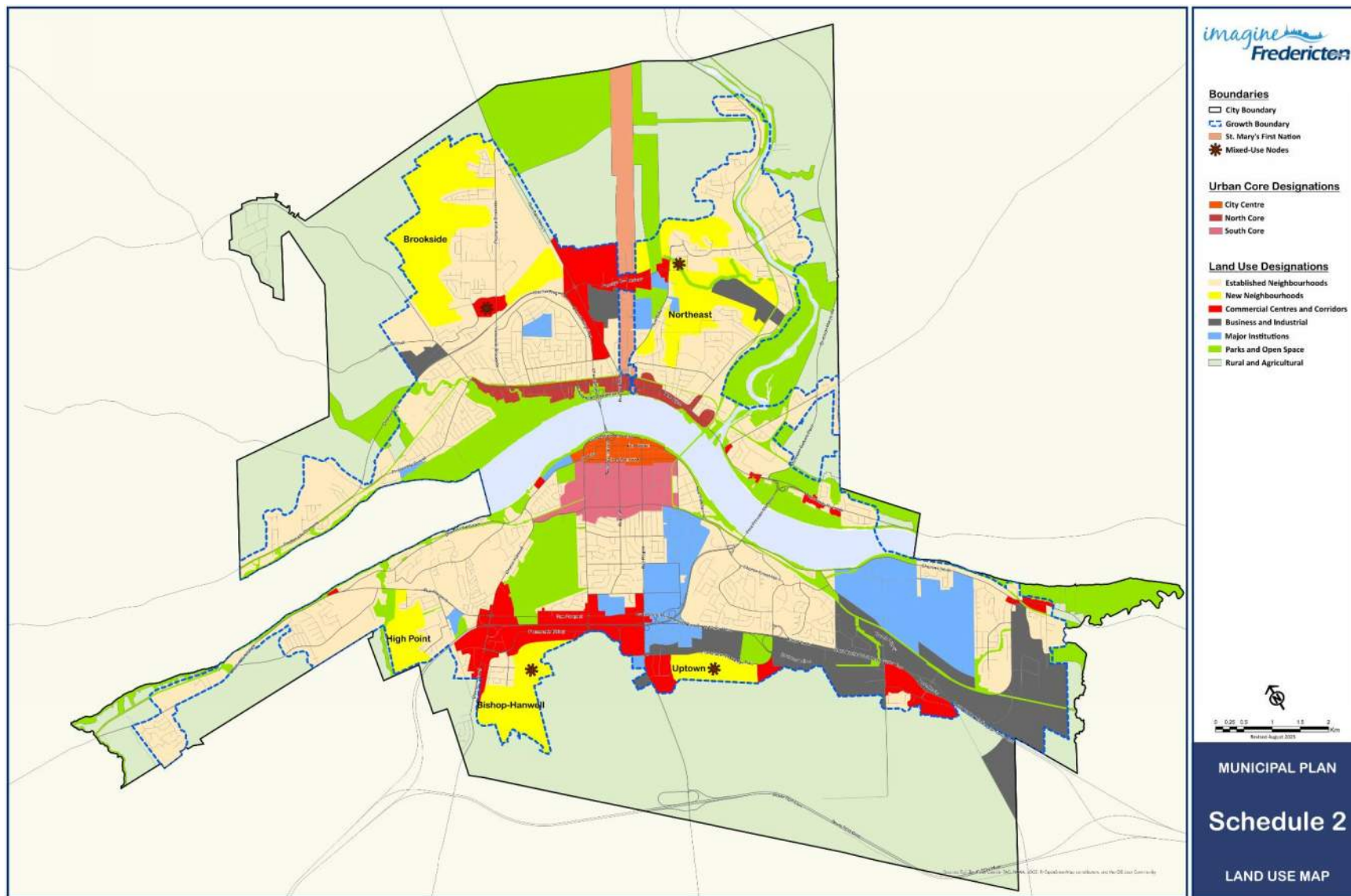
Schedule A
South Core Secondary Municipal Plan & Urban Design Guidelines

The South Core Secondary Municipal Plan and Urban Design Guidelines are attached herein.

Schedule B Municipal Plan – Schedule 1 (Urban Structure Map)



Schedule C Municipal Plan – Schedule 2 (Land Use Map)



Schedule D

Municipal Plan – Text Amendments

Repealing background statement, subparagraphs, and glossary terms and replacing as follows:

2.2.1 South Core

Background

The South Core, largely comprised of the Town Plat, includes large sites for potential redevelopments, such as the Railway Lands and the New Brunswick Exhibition Grounds (NBEX). Arterial and collector roads will accommodate greater intensification, while more established residential areas can absorb more moderate intensification and appropriate infill development.

Given the constrained boundaries of the City Centre, and therefore its limited capacity for residential growth, achieving significant intensification in the South Core will be critical to maximizing downtown vitality. The South Core Plan Area is intended to accommodate over 6,000 new residents by 2051.

2.2.1(12) New development or redevelopment within the South Core shall occur in accordance with the South Core Secondary Municipal Plan.

2.2.1(13) New development or redevelopment on the New Brunswick Exhibition Grounds (NBEX) shall occur in accordance with the New Brunswick Exhibition Grounds (NBEX) Secondary Municipal Plan.

Glossary

Mid-Rise

A building between 4 and 7 storeys in height

High-Rise

A building greater than 7 storeys in height

Repealing subparagraphs as follows:

~~2.2.1(15) Small-scale commercial uses and professional offices will be encouraged along Smythe Street, York Street, and Regent Street in low-rise mixed-use buildings to achieve a more complete neighbourhood and provide a transition to the downtown.~~

~~2.2.1(16) Compatible residential infill will be encouraged along Smythe Street, York Street, and Regent Street, in low- to mid-rise buildings to achieve residential intensification.~~

~~2.2.2(3) Initiate a new South Core Secondary Municipal Plan which will replace the Town Plat Secondary Municipal Plan, to provide a detailed framework for new development, including public realm and built form design guidelines, access and circulation, land use distribution,~~

~~heights and densities, and servicing requirements, that support positive urban development, including:~~

- ~~iii. — The redevelopment of the Capital Exhibit Centre as a short-term goal; and,~~
- ~~iv. — The redevelopment of the Railway Lands as a short-term goal.~~

Schedule E

Zoning By-law – Text Amendments

1. **Table of Contents** 7.1(4) Lot Consolidation in the **South Core**

2. **Section 3 Definitions**

Add:

Multiplex means a *building* containing more than two *dwelling units* where only individual outside access is provided to all or a portion of the *dwelling units*. (*multiplex*)

POPS means a privately-owned public space provided as part of a *development* that is made accessible to the public through legal easements and/or agreements.

Section 8.1 **Comparative Use Chart** Replace Three or Four Unit Dwelling with Multiplex in the Comparative Use Chart

3. **Section 5.2(10) Design of Parking Lots**

(vii) parking lots shall be screened and separated as follows:

(E) along the *rear property line* of parking lots in the **Neighbourhood Area** (Schedule **13**): by a minimum 4 metre wide landscaped strip consisting of coniferous and deciduous trees, shrubs, hedges, 2 metre high opaque fence or a combination thereof; and,

(F) along the *side property line* adjacent to a *parking lot* in the **Neighbourhood Area** (Schedule **13**): by a 2 metre high opaque fence.

4. **Section 5.2(12)(e) Required Parking**

Table (i) Residential Parking Requirements

Row: (A) *Apartment Building or Stacked Townhouse* and (B) *Dwelling Unit*

Column: **Area 1**

Inside the **Corridor Area** (Schedule **14**):

 - Per 2 or less BR unit: 0.5 sp
 - Per 3 or more BR unit: 1 sp

Inside the Neighbourhood Area (Schedule **13**):

 - Per bachelor or 1-BR unit: 0.5 sp
 - Per 2 or more BR unit: 1 sp

Outside the **above areas**:

 - Per bachelor or 1-BR unit: 0.75 sp
 - Per 2 or more BR unit: 1.25 sp

Row: (L) **Multiplex**

Table (ii) Commercial Parking Requirements

Development	CBD and Corridor Area	Outside CBD and Corridor Area
-------------	------------------------------	--------------------------------------

- | | |
|----|--|
| 5. | <p>Section 7
Regulations Applying to Residential Uses</p> <p>7.1(4) Lot Consolidation in the South Core</p> <p>(a) Increasing <i>lot area</i> and <i>lot frontage</i> to obtain additional <i>density</i> rights through consolidation of land is not permitted within the Neighbourhood Area (Schedule 13)</p> <p>7.1(5) Converted Dwellings</p> <p>(b) Converted Dwellings in the South Core and Central Business District</p> <p>(c) Converted Dwellings with Additions in the South Core</p> |
| 6. | <p>Section 8.7(1)
Purpose</p> <p>The TP-4 Zone:</p> <ul style="list-style-type: none"> • accommodates residential <i>development</i> primarily in the form of <i>converted dwellings</i> containing up to 6 dwelling units and new buildings containing up to 4 dwelling units; <p>Section 8.8(1)
Purpose</p> <p>The TP-6 Zone:</p> <ul style="list-style-type: none"> • accommodates residential <i>development</i> primarily in the form of <i>converted dwellings</i> containing up to 8 dwelling units and new buildings containing up to 6 dwelling units; <p>Sections 8.4(2)(a), 8.7(2)(a), 8.8(2)(a)
Permitted Uses</p> <p>Add:
Multiplex (in alphabetical order)</p> <p>Section 8.4(3)
Use Rules</p> <p>Add;
(e) multiplex buildings shall contain a maximum of 4 dwelling units.</p> <p>Section 8.7(3)
Use Rules</p> <p>(a) <i>converted dwellings</i> shall contain a maximum of 6 dwelling units;</p> <p>(b) multiplex and <i>townhouse buildings</i> shall contain a maximum of 4 <i>dwelling units</i>;</p> <p>Section 8.8(3)
Use Rules</p> <p>(a) <i>converted dwellings</i> shall contain a maximum of 8 dwelling units;</p> <p>(b) multiplex and <i>townhouse buildings</i> shall contain a maximum of 6 dwelling units;</p> <p>Section 8.4(4)
Standards</p> <p>(a) Lot Area (MIN)</p> <p>(i) Multiplex 630 m² (3 unit)
 690 m² (4 unit)</p> <p>(b) Lot Frontage (MIN)</p> <p>(i) Multiplex 21 m (3 unit)
 23 m (4 unit)</p> <p>Sections 8.7(4) and 8.8(4)
Standards</p> <p>(a) Density (Lot Area per Dwelling Unit) & Lot Area</p> <p>(i) <i>Townhouse</i>: MAX 55 <i>dwelling units</i> per hectare</p> |

(MIN 180 m² per dwelling unit)

(ii) Multiplex: MAX 62 dwelling units per hectare
(MIN 161 m² per dwelling unit)

Section 8.7(4)
Standards

(b) Lot Frontage (MIN)

(i) Multiplex 21m

Section 8.8(4)
Standards

(b) Lot Frontage (MIN)

Multiplex 21m (3 or 4 units)
23m (5 or 6 units)

7. Section 8.4(4)
Standards

(i) Converted Dwellings

Number of Units	(A) Lot Area (MIN)	(B) Lot Frontage (MIN)
2 units	420m ²	14 metres
3 units	480m ²	16 metres
4 units	540m ²	18 metres
5 units	600m ²	20 metres
6 units	660m ²	22 metres

Section 8.6(4)
Standards

(h) Converted Dwellings

Number of Units	(A) Lot Area (MIN)	(B) Lot Frontage (MIN)
2 units	400m ²	8 metres
3 units	500m ²	10 metres

(i) Converted Dwellings with Additions

Number of Units	(A) Lot Area (MIN)	(B) Lot Frontage (MIN)
2 units	500m ²	10 metres
3 units	600m ²	12 metres

Section 8.7(4)
Standards

(j) Converted Dwellings

Number of Units	(A) Lot Area (MIN)	(B) Lot Frontage (MIN)
2 units	400m ²	8 metres
3 units	500m ²	10 metres
4 units	600m ²	12 metres
5 units	700m ²	14 metres
6 units	800m ²	16 metres

(k) Converted Dwellings with Additions

Number of Units	(A) Lot Area (MIN)	(B) Lot Frontage (MIN)
2 units	500m ²	10 metres
3 units	600m ²	12 metres
4 units	700m ²	14 metres
5 units	800m ²	16 metres
6 units	900m ²	18 metres

Section 8.8(4)
Standards

(j) Converted Dwellings

Number of Units	(A) Lot Area (MIN)	(B) Lot Frontage (MIN)
2 units	400m ²	8 metres
3 units	500m ²	10 metres
4 units	600m ²	12 metres
5 units	700m ²	14 metres
6 units	800m ²	16 metres
7 units	900m ²	18 metres
8 units	1000m ²	20 metres

(k) Converted Dwellings with Additions

Number of Units	(A) Lot Area (MIN)	(B) Lot Frontage (MIN)
2 units	500m ²	10 metres
3 units	600m ²	12 metres
4 units	700m ²	14 metres
5 units	800m ²	16 metres
6 units	900m ²	18 metres
7 units	1000m ²	20 metres
8 units	1100m ²	22 metres

8. Section 8.6(4)
Standards

(f) Landscaped Area

(i) In accordance with Section 4.2(2)

(ii) Despite Section 5.2(10)(vii)(E), a contiguous *soft landscaped area* comprising at least 10% of the *lot area* shall be provided abutting the *rear property line* or portion thereof.

9. Sections
8.7(4) and
8.8(4)
Standards

(h) Landscaped Area

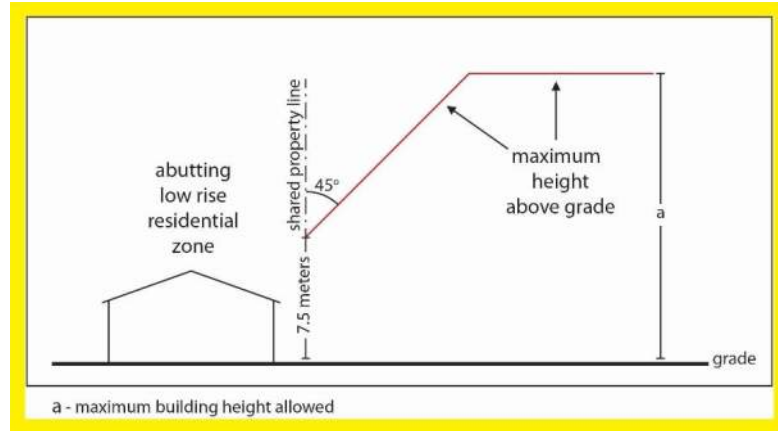
(i) The minimum *landscaped area* required at *grade* is 35% of the *lot area*.

(ii) In accordance with Section 4.2(2)

(iii) Despite Section 5.2(10)(vii)(E), a contiguous *soft landscaped area* comprising at least 10% of the *lot area* shall be provided abutting the *rear property line* or portion thereof.

10. Sections:
9.3(4)(e)(ii),
9.5(4)(g)(ii),
9.6(4)(g)(ii),
11.6(4)(h)(ii),
11.13(4)(h)(ii),
12.2(4)(e)(ii),
12.3(4)(e)(ii),
12.4(4)(e)(ii)
Standards

(ii) Where the *lot* abuts a *low rise residential zone*, *building height* is measured beginning at a point 7.5 metres above *grade* at the shared *property line* and increases proportionally at a 45 degree angle to the maximum *building height* allowed.



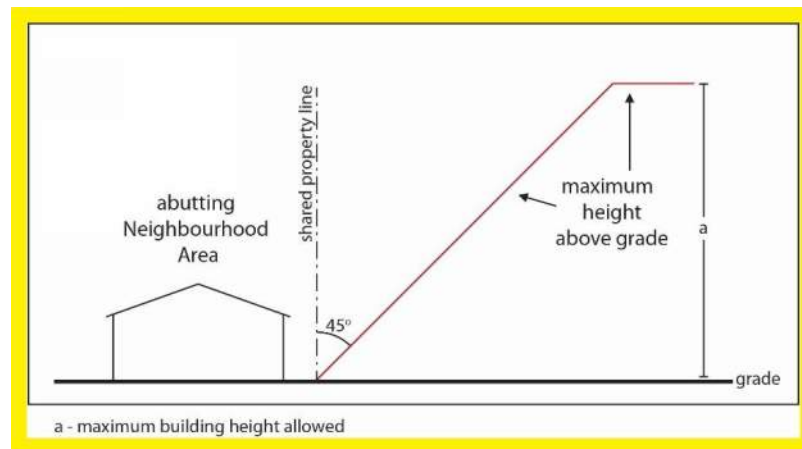
11. Sections:
 9.5(4)(g)(iii), Delete entirety of Sections: 9.5(4)(g)(iii), 9.6(4)(g)(iii), 11.6(4)(h)(iii),
 9.6(4)(g)(iii), 11.13(4)(h)(iii), 12.2(4)(e)(iii), 12.3(4)(e)(iii), 12.4(4)(e)(iii)
 11.6(4)(h)(iii),
 11.13(4)(h)(iii)
 12.2(4)(e)(iii),
 12.3(4)(e)(iii),
 12.4(4)(e)(iii)
Standards
 (sections outlining the 45 degree height limit diagram adjacent to MR-1, MR-2, MX-1 or MX-2 zone).
12. Section 9.5(4) **(a) Density (Lot Area per Dwelling Unit)**
Standards
 (i) MAX 100 *dwelling units* per hectare (MIN 100 m² per *dwelling unit*) **except,**
 (ii) *Buildings* located within the Corridor Area as shown on Schedule 14:
 MAX 123 *dwelling units* per hectare (MIN 81m² per *dwelling unit*)
 (iii) Despite Section (ii) above, *buildings* located within the Corridor Area as shown on Schedule 14 that provide at least 50% of the required *parking* underground:
 MAX 150 *dwelling units* per hectare (MIN 66m² per *dwelling unit*)
13. Section 9.5(4) **(e) Building Location and Design**
Standards
 (i) Within the **South Core** (Schedule 6) the *building* shall be located on the *lot* such that a minimum of 60% of the *façade* length facing a public *street* is located within the minimum and maximum *building setback* from a *front property line*.
 (vii) *Buildings* located within the Corridor Area as shown on Schedule 14 that are more than four (4) *storeys* in *height* shall incorporate a continuous step back at the 3rd, 4th or 5th storey level to a minimum depth of 2 metres on any *façade* that faces a public *street*.
14. Section 9.5(4) **(f) Building Setbacks (MIN)**
Standards
 (i) From a *front property line*: 3 metres, except:
 (A) within the **South Core** (Schedule 6):
15. Section 9.5(4) **(g) Building Height (MAX)**
Standards
 (i) 18 metres, **except:**

(A) Buildings located within the Corridor Area as shown on Schedule 14 shall comply with the following:

- (III) Minimum *building height*: 8 metres;
- (IV) Maximum *building height*: 22.5 metres, except:
- (III) Maximum *building height* on a *corner lot*: 25.5 metres

(iii) Despite Section (ii) above, where the *lot* is located within the Corridor Area as shown on Schedule 14, the 45 degree angle *height* limit does not apply where the abutting *lot* is also located within the Corridor Area.

(iv) Where the *lot* is located within the Corridor Area as shown on Schedule 14 and abuts a *lot* located within the Neighbourhood Area as shown on Schedule 13 with its *rear property line*, *building height* is measured beginning at a point at *grade* at the shared *property line* and increases proportionally at a 45 degree angle to the maximum *building height* allowed.



16. Section 9.6(4) **(e) Building Location and Design Standards**

(i) Within the **South Core** (Schedule 6) the *building* shall be located on the *lot* such that a minimum of 60% of the *façade* length facing a public *street* is located within the minimum and maximum *building setback* from a *front property line*.

(viii) Despite Section (vii) above, *buildings* located within the Mid-Town Area as shown on Schedule 15 that are more than four (4) *storeys* in *height* shall incorporate a continuous step back at the 3rd, 4th or 5th storey level to a minimum depth of 2 metres on any *façade* that faces a public *street*.

(ix) *Buildings* located within the Mid-Town Area as shown on Schedule 15 between Hanwell Road and Rookwood Avenue, shall provide ground floor entrances that face the Cross Town Trail where the *building* or portion thereof is located within 15 metres of the *property line* abutting the Cross Town Trail.

(f) Building Setbacks (MIN)

- (i) From a *front property line*: 3 metres, except:
 - (A) within the **South Core** (Schedule 6):

(g) Building Height (MAX)

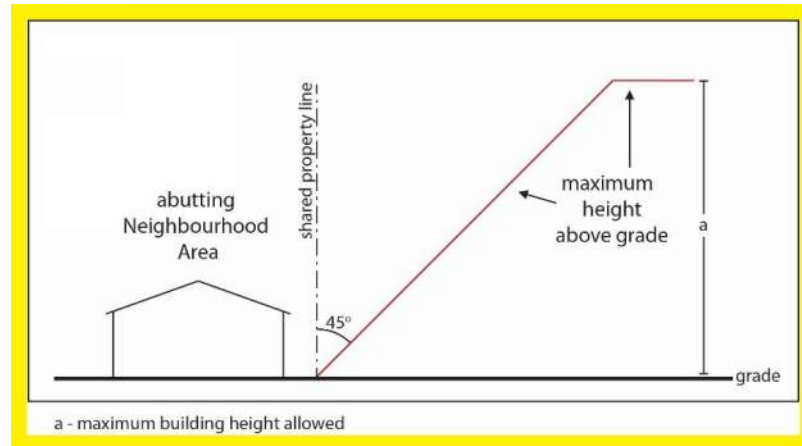
- (i) 24 metres, **except:**

(A) *Buildings* located within the Mid-Town Area as shown on Schedule 15 shall comply with the following:

- (I) Minimum *building height*: 9 metres
- (II) Maximum *building height*: 37.5 metres, except:
- (III) Maximum *building height* on a *corner lot*: 46.5 metres

(B) Despite (A) above, *buildings* located within the Mid-Town Area as shown on Schedule 15 between Hanwell Road and Rookwood Avenue, shall be limited to a maximum *building height* of 14 metres where the *building* or portion thereof is located within 15 metres of the *property line* abutting the Cross Town Trail.

(iii) Where the *lot* is located within the Mid-Town Area as shown on Schedule 15 and abuts a *lot* located within the Neighbourhood Area as shown on Schedule 13 with its *rear property line*, *building height* is measured beginning at a point at *grade* at the shared *property line* and increases proportionally at a 45 degree angle to the maximum *building height* allowed.



17.

Section 11.11
Mixed Use
Zone One

11.11(1) Purpose

The MX-1 Zone:

- accommodates *buildings* containing limited commercial *uses* and at least 1 *dwelling unit*;
- allows for the *conversion* and adaptive reuse of existing *buildings* in order to help maintain the character of the area;
- allows up to a maximum of 3 *secondary dwelling units* on a *lot*; and,
- provides a limited mix of *uses* primarily to facilitate a transition between the City Centre and the **South Core**.

18.

Section
11.12(4)
Standards

(d) Density (Lot Area per Dwelling Unit)

- (i) MAX 62 *dwelling units* per hectare (MIN 161 m² per *dwelling unit*)
- (ii) *Buildings* located within the Corridor Area as shown on Schedule 14:
MAX 123 *dwelling units* per hectare (MIN 81 m² per *dwelling unit*)
- (iii) For *affordable housing units*: MAX 224 *dwelling units* per hectare (MIN 45 m² per *dwelling unit*)

(e) Building Height (MAX)

(i) 13 metres, **except:**

(A) Buildings located within the Corridor Area as shown on Schedule 14 shall comply with Section 9.5(4)(g) and Section 9.5(4)(e)(vii).

19. Section
11.13(2)
Uses

(a) Permitted Uses

Apartment Building, Stacked Townhouse and **Townhouse** (in alphabetical order)

Section
11.13(3) **Use
Rules**

11.13(3) Use Rules

(d) Apartment building, stacked townhouse and townhouse shall only be permitted within the Mid-Town Area as shown on Schedule 15.

Section
11.13(4)
Standards

(d) Density (Lot Area per Dwelling Unit)

- (i) MAX 100 *dwelling units* per hectare (MIN 100 m² per *dwelling unit*)
- (ii) **Buildings** located within the Mid-Town Area as shown on Schedule 15: MAX 160 *dwelling units* per hectare (MIN 62 m² per *dwelling unit*)
- (iii) For *affordable housing units*: MAX 224 *dwelling units* per hectare (MIN 45 m² per *dwelling unit*)

(e) Building Setbacks (MIN)

- (i) From a *property line* that abuts a public *street*: 3 metres
- (ii) From any other *property line*: 3 metres, **except:**
 - (A) where the *property line* abuts a *low rise residential zone*: 10 metres

(h) Building Height (MAX)

- (i) MIN 9 metres **AND** MAX 24 metres, **except:**
 - (A) **Buildings** located within the Mid-Town Area as shown on Schedule 15 shall comply with Section 9.6(4)(g).

(i) Building Design

(iii) Despite Section (ii) above, **buildings** located within the Mid-Town Area as shown on Schedule 15 that are more than four (4) *storeys in height* shall incorporate a continuous step back at the 3rd, 4th or 5th storey level to a minimum depth of 2 metres on any *façade* that faces a public *street*.

(iv) **Apartment building, stacked townhouse and townhouse** buildings located within the Mid-Town Area as shown on Schedule 15 between Hanwell Road and Rookwood Avenue, shall provide ground floor entrances that face the Cross Town Trail where the *building* or portion thereof is located within 15 metres of the *property line* abutting the Cross Town Trail.

(j) Landscaped Area

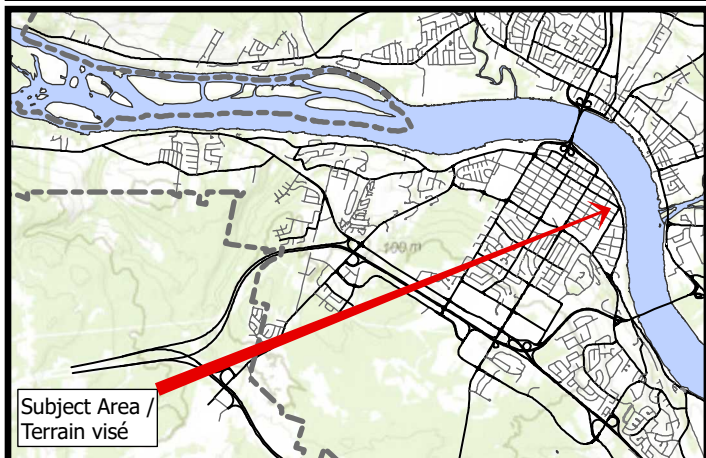
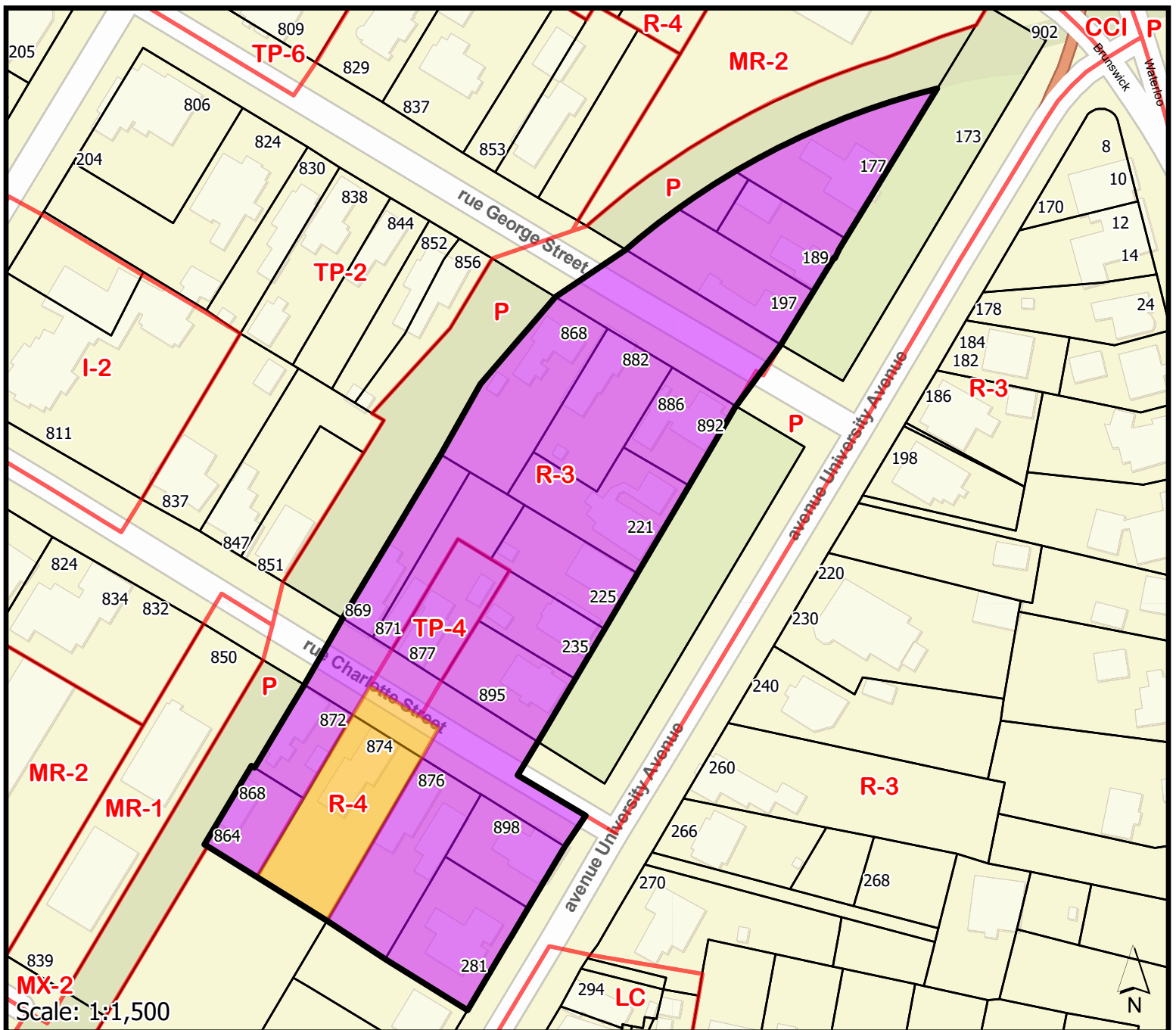
- (i) The minimum *landscaped area* required at *grade* is 35 % of the *lot area*.
(ii) Despite Section (i) above, *development* located within the Mid-Town Area as shown on Schedule 15:
 (A) may provide the required *landscaped area* above *grade*, and
 (B) shall provide a minimum *landscaped area* of 8% of the *lot area* where privately-owned public space (*POPS*) is provided as part of the *development*.
(iii) In accordance with Section 4.2(2)

20. Section 11.16(1)
Purpose
- Purpose
...
• facilitates a transition in land use between the City Centre's east end and the **South Core**.
21. Section 20
Schedules
- LIST OF SCHEDULES**
- 1 City Centre Plan Area & Central Business District**
2 Maximum Building Height
3 Group Homes
4 Special Sign Districts A, B, C
5 Parking Districts
6 South Core Planning Area
...
13 Neighbourhood Area
14 Corridor Area
15 Mid-Town Area
- Delete Schedule 6 and replace with new **Schedule 6 South Core Planning Area** (attached as Map IV)
- Add new **Schedule 13 Neighbourhood Area** (attached as Map V)
Add new **Schedule 14 Corridor Area** (attached as Map VI)
Add new **Schedule 15 Mid-Town Area** (attached as Map VII)

Schedule F

Subdivision By-law – Text Amendments

1. By-Law Z-4,
A Subdivision
By-Law:
Section 3
**Land For
Public
Purposes**
 - 3.03 Where approval of a plan of subdivision is sought on land located within the Mid-Town Area pursuant to the South Core Secondary Municipal Plan, the condition of approval of the plan of subdivision prescribed by Sections 3.01 and 3.02 above shall not apply where privately-owned public space (POPS) is provided on the subdivided land in lieu of the 8% public land or cash payment requirement.



 Subject Area / Point d'intérêt

Rezone / Rezoner

 R-3 to/à TP-4

 R-4 to/à TP-4

Fredericton

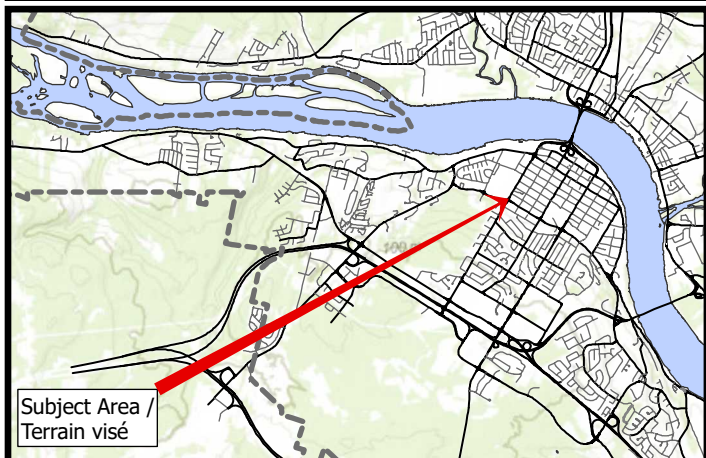
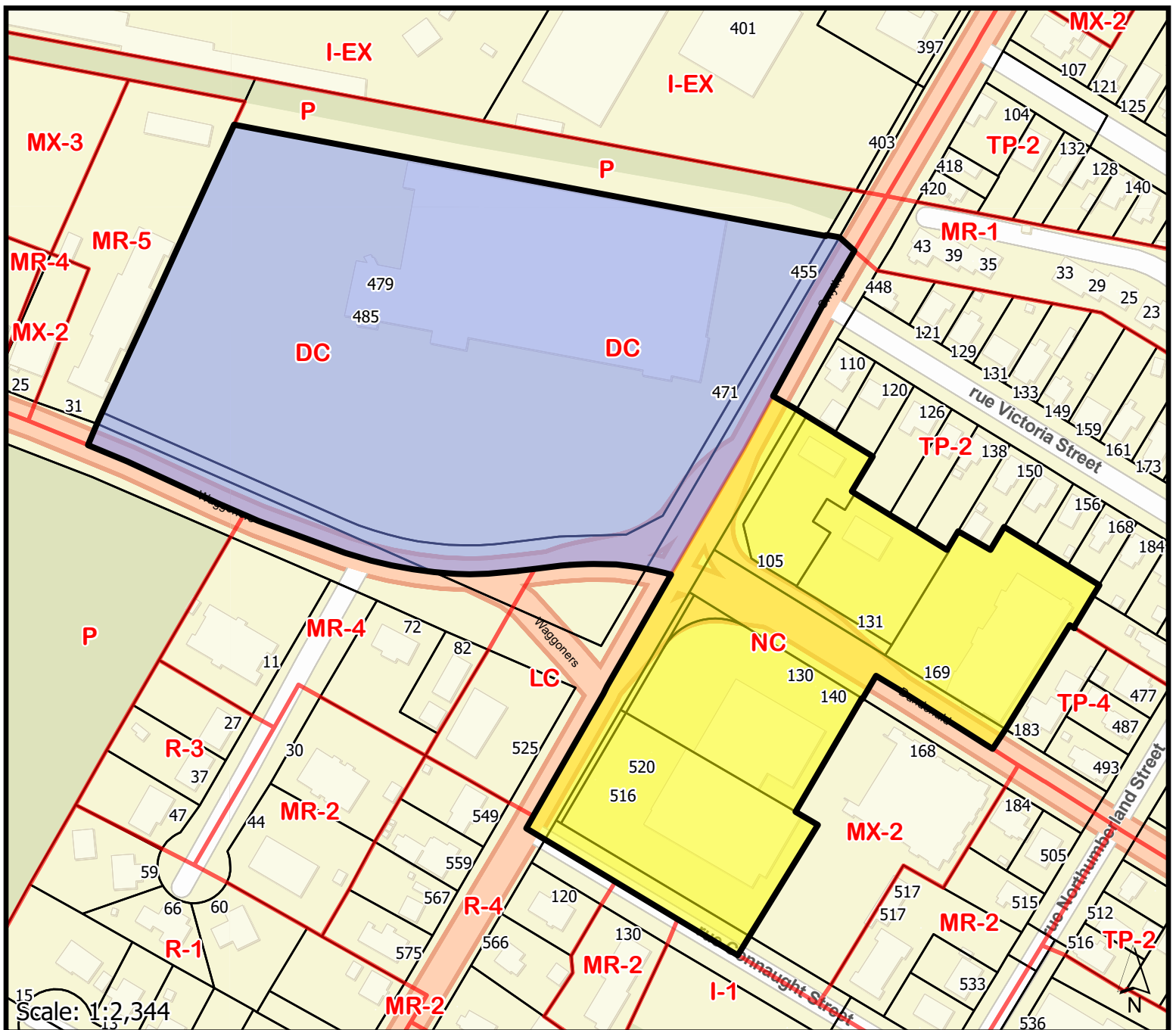
Community Planning
Planification urbaine

Map \ carte # I

File \ fiche: PR-59-25

Date \ date: août \ August 20, 2025

Subject \ sujet: South Core / Noyau Sud
City of Fredericton



 Subject Area / Point d'intérêt

Rezone / Rezoner

 NC to/à MX-3

 DC to/à MX-3

Fredericton

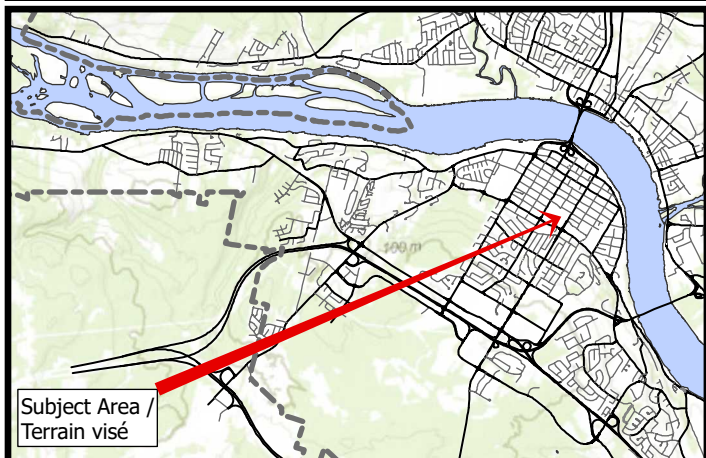
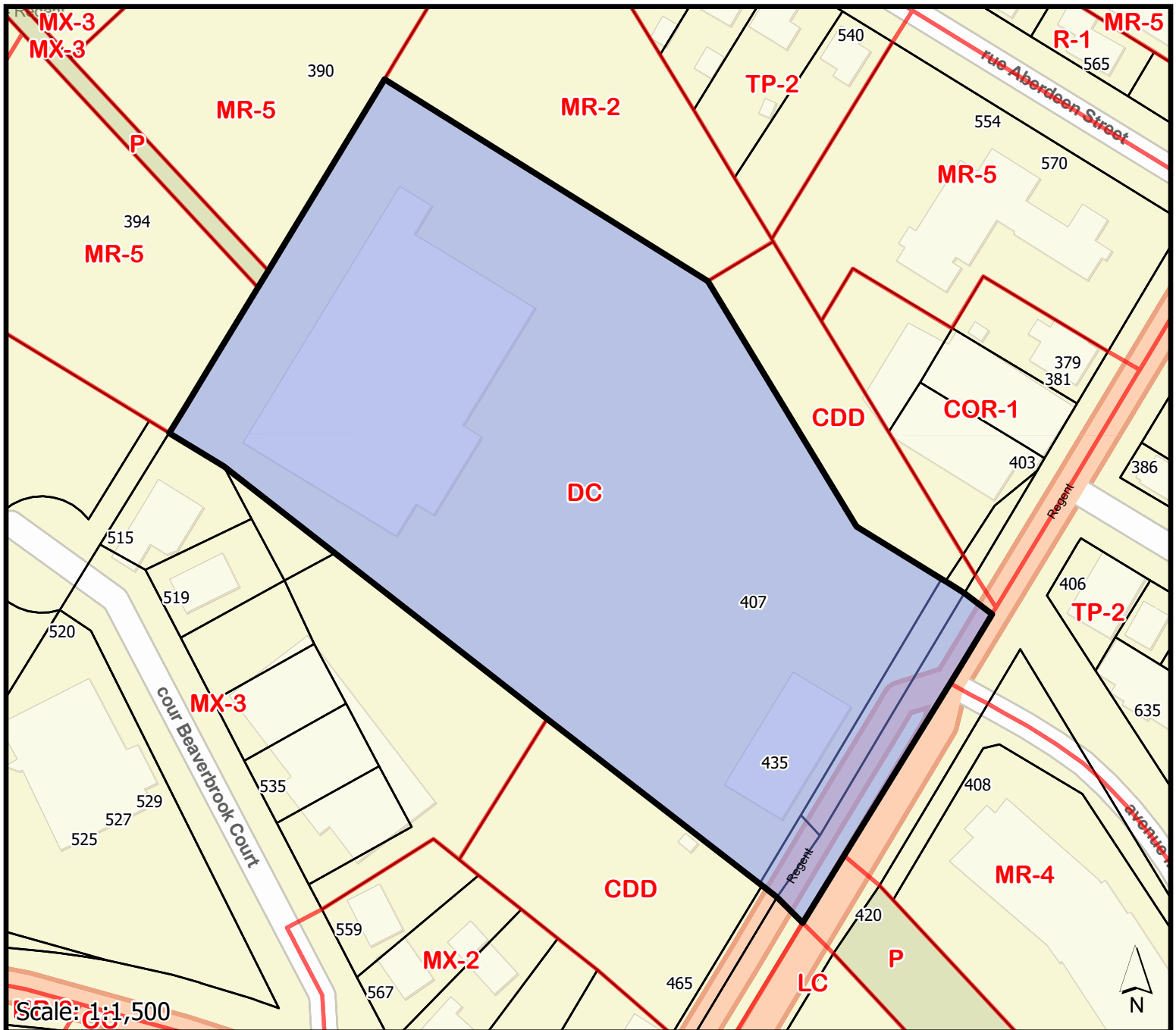
Community Planning
Planification urbaine

Map \ carte # II

File \ fiche: PR-59-25

Date \ date: août \ August 20, 2025

Subject \ sujet: South Core / Noyau Sud
City of Fredericton



 Subject Area / Point d'intérêt

Rezone / Rezoner

 DC to/à MX-3

Fredericton

Community Planning
Planification urbaine

Map \ carte # III

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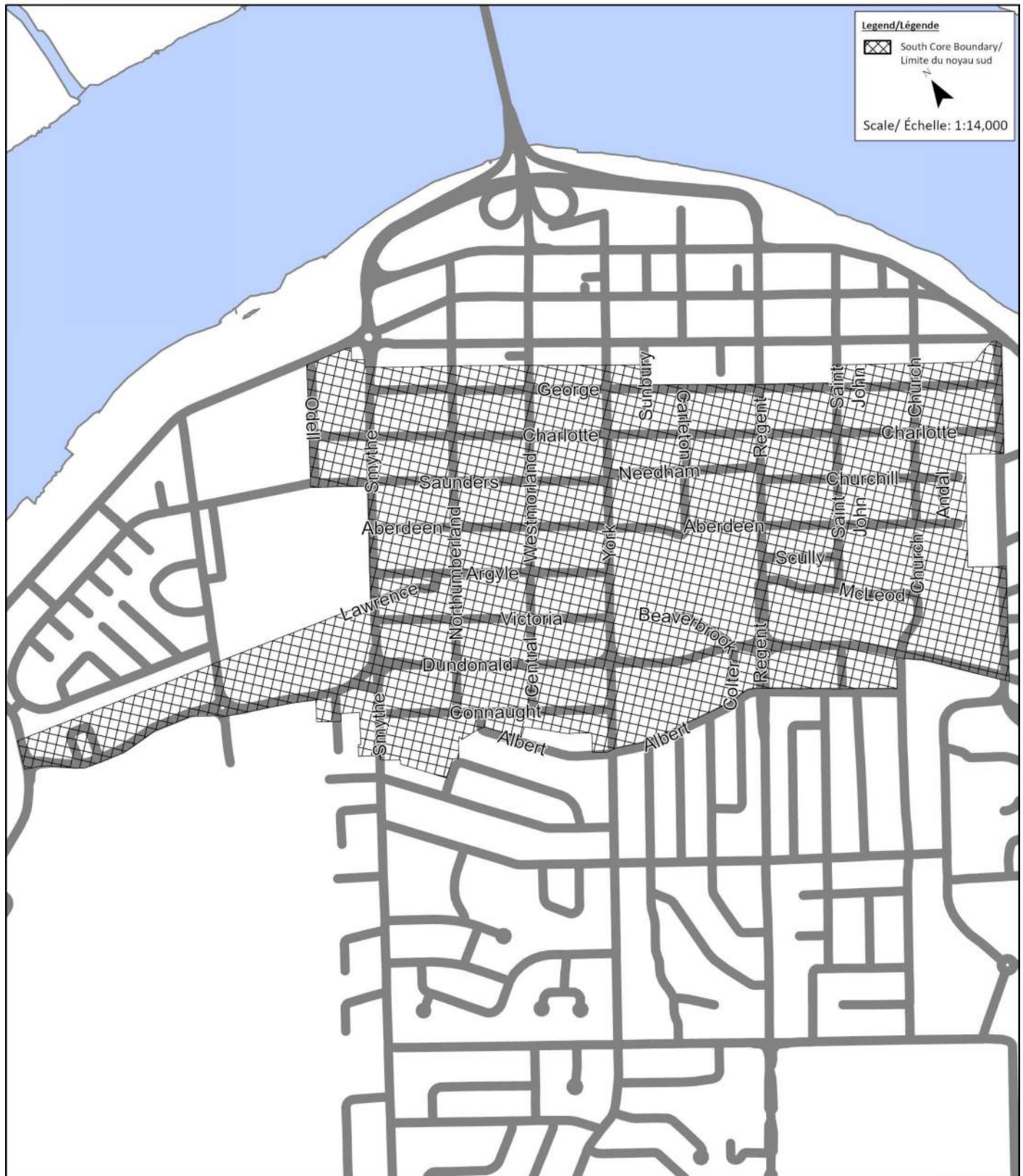
Date \ date: août \ August 20, 2025

Subject \ sujet: South Core / Noyau Sud
City of Fredericton

SOUTH CORE PLANNING AREA / LIMITE DU NOYAU SUD

Zoning By-Law/Arrêté n° Z-5

Schedule/Annexe 6



Community Planning
Planification urbaine

Map \ carte # IV

File \ fiche: PR-59-25

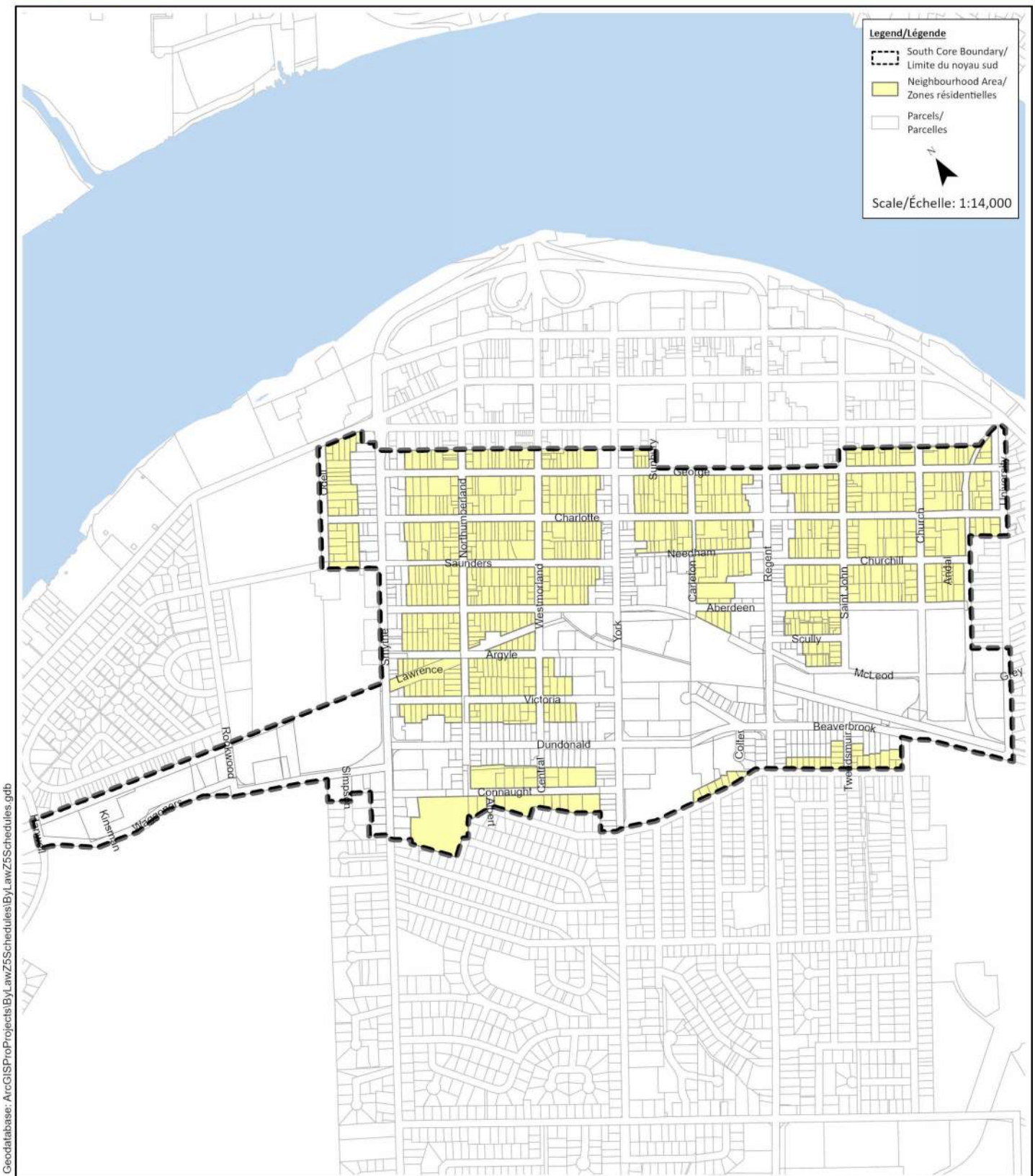
Date \ date: août \ August 20, 2025

Subject \ sujet: South Core / Noyau Sud
City of Fredericton

NEIGHBOURHOOD AREA/ ZONES RÉSIDENTIELLES

Zoning By-Law/Arrêté n° Z-5

Schedule/Annexe 13



Community Planning
Planification urbaine

Map \ carte # V

File \ fiche: PR-59-25

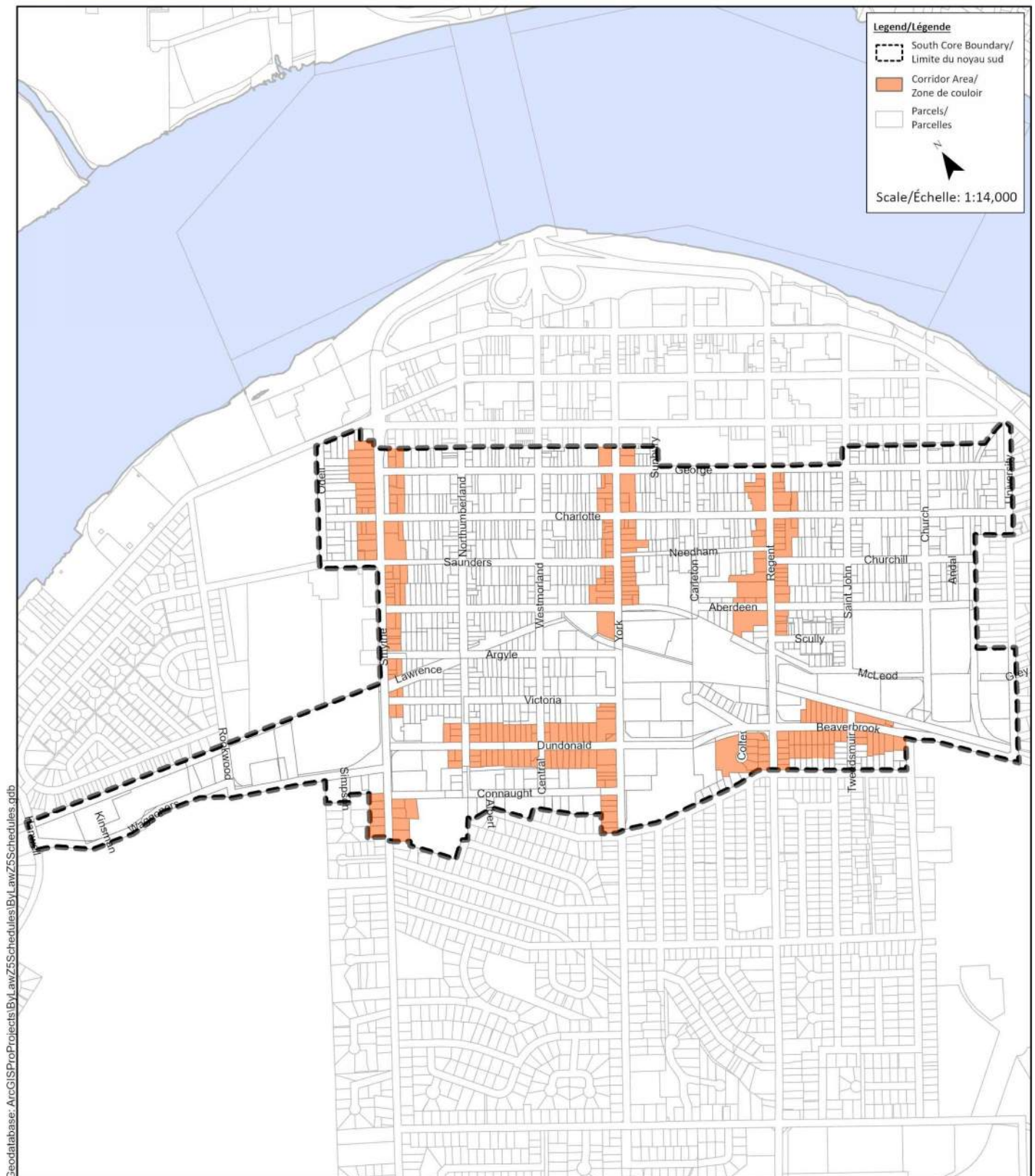
Date \ date: août \ August 20, 2025

Subject \ sujet: South Core / Noyau Sud
City of Fredericton

CORRIDOR AREA/ ZONE DE COULOIR

Zoning By-Law/Arrêté n° Z-5

Schedule/Annexe 14



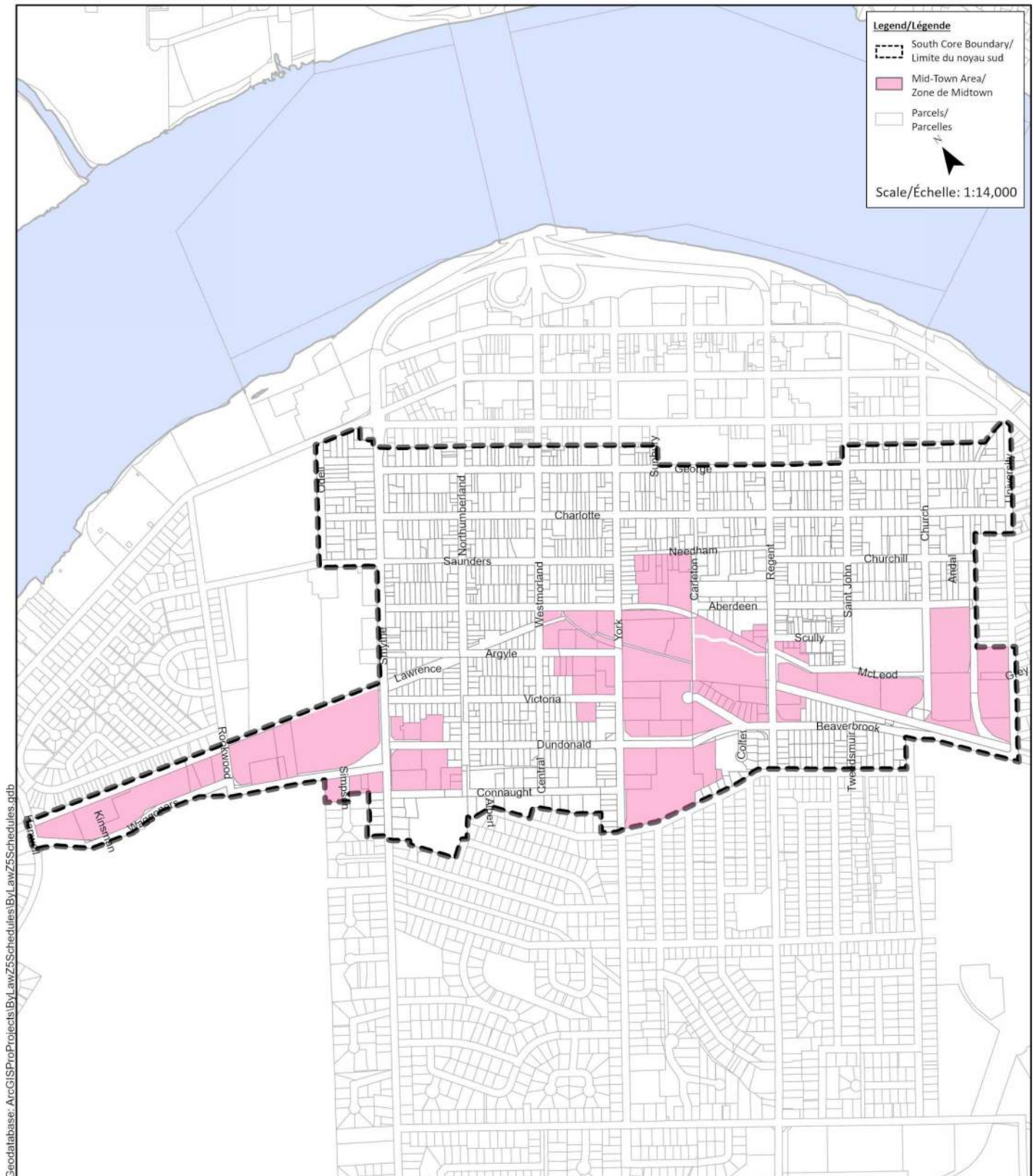
Community Planning
Planification urbaine

Map \ carte # VI
File \ fiche: PR-59-25
Date \ date: août \ August 20, 2025
Subject \ sujet: South Core / Noyau Sud
City of Fredericton

MID-TOWN AREA/ ZONE DE MDTOWN

Zoning By-Law/Arrêté n° Z-5

Schedule/Annexe 15



Geodatabase: ArcGISProProjects\ByLawZ5\Schedules\ByLawZ5\Schedules.gdb



Community Planning
Planification urbaine

Map \ carte # VII

File \ fiche: PR-59-25

Date \ date: août \ August 20, 2025

Subject \ sujet: South Core / Noyau Sud
City of Fredericton

FREDERICTON SOUTH CORE PLAN

Secondary Municipal Plan

2025

Fredericton

 The Planning
Partnership

Trace Planning & Design

CBCL

ERA Architects

Fredericton

Land Acknowledgment

We acknowledge that the City of Fredericton is situated on traditional Wolastoqey territory. The territory of the Wolastoqiyik people is recognized in the Peace and Friendship Treaties to establish an ongoing relationship of peace, friendship, and mutual respect between equal nations. The river that runs through our city is known as the Wolastoq, along which live the Wolastoqiyik, “the people of the beautiful and bountiful river”.



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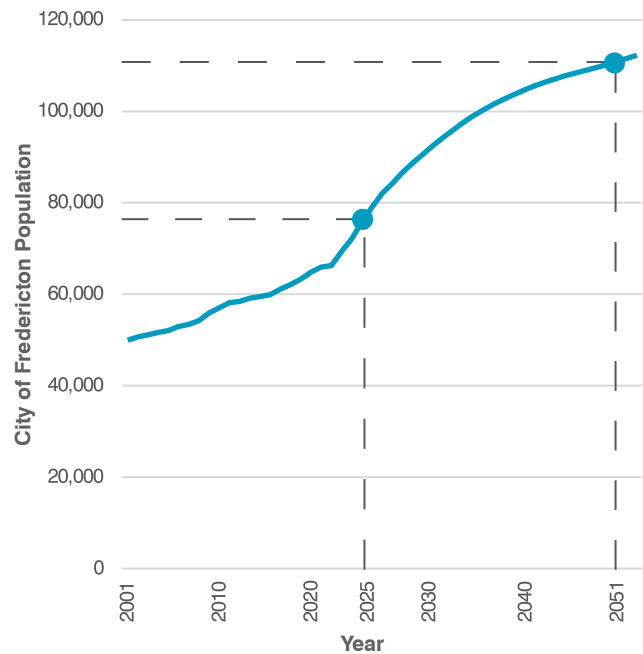
York Street Development Aerial Photo

1.1 Introduction

The City of Fredericton is growing at an unprecedented pace, attracting new residents from across the province, the country, and beyond. In recent years, the annual population growth was measured between two and three times what was initially anticipated pre-COVID in the Fredericton Growth Strategy (2017) and Imagine Fredericton Municipal Plan (2020). By 2051, Fredericton's population is projected to increase from 77,000 today to approximately 114,000. While this trend does require a shift in how the city develops in the coming years, the framework created by the Imagine Fredericton planning process has set us in the right direction.

The Growth Strategy was based on the principles of creating the best environment for managing growth wisely and designing the city to be attractive and sustainable well into the future. An Urban Core containing three core areas (City Centre, North Core, and South Core) was defined and expected to accommodate 25% of the population growth, while four new growth areas were identified for the remaining 75%. Secondary Municipal Plans for the City Centre, and North Core and Main Street were adopted in 2015 and 2016 respectively, leaving the South Core as the last remaining Urban Core area without a relatively up to date plan.

Based on the anticipated city-wide population growth, approximately 6,000 new residents (or 3,200 new housing units) will need to be accommodated in the South Core neighbourhood. It is also vital that new commercial uses and supporting amenities and services are included in this growth. The South Core Plan will be an important guiding document to ensure the Urban Core is able to meet the principles of the Growth Strategy and accommodate both new and existing residents in the heart of Fredericton.



City of Fredericton Population Growth Chart

Given the constrained Plan boundaries of the City Centre, and therefore, its limited capacity for residential growth, achieving significant intensification in the South Core, a short walk away, will be critical to maximizing downtown vitality.

– Fredericton Growth Strategy

1.2 Defining the South Core

The South Core is an area defined by the Municipal Plan and Growth Strategy, which specifies large redevelopment sites such as the 'Railway Lands' and the New Brunswick/Fredericton Exhibition Centre (NBEX lands) as well as the primary corridors: Smythe, York, Regent, and Waggoners/Dundonald/Beaverbrook (Mid-Town Corridor). Beyond the policies in the Municipal Plan, the South Core has been guided primarily by the Residential Town Plat Secondary Municipal Plan for ~25 years. This previous plan was successful in accommodating a mix of new residential and commercial development without compromising the existing neighbourhood character. However, given the changes in the city and the community itself, a new plan is important to better guide development over the next ~25 years as proposed in the Municipal Plan.

The South Core is positioned as a critical neighbourhood for supporting significant residential growth in Fredericton's Urban Core. There is already a base of commercial, employment and institutional uses, such as schools and places of worship, that help to further define the area as a complete community. Building on the foundation of historic character, walkable streets, and accessible amenities will only further establish the area as a wonderful place to live, work, and play.

NBEX Lands

While the Municipal Plan and Growth Strategy includes the NBEX site as a component of the South Core with a higher level of priority for redevelopment, it was not directly included in the development of this plan. This is due to the NBEX Secondary Municipal Plan adopted by City Council in 2021, which provides three different scenarios for a future site build-out.

Given the ability to accommodate a significant amount of residential development on the NBEX, as stated in the Site Redevelopment Plan, this site will play a role in meeting the growth needs for the South Core and the city. The inclusion of a new middle school would also provide major institutional support to existing and future residents. Any future development on the NBEX site, as well as on the lots immediately adjacent to the site, will need to consider connectivity and complementary land uses.



1.3 Plan Area Boundary

The updated South Core Plan Area boundary is based off the original Residential Town Plat Secondary Municipal Plan and the land use designations from the Municipal Plan. Thoroughly examining these boundaries was an important step in the planning process to ensure the appropriate areas were being considered for this Plan. The map below provides the boundary for the South Core Plan, as well as all Secondary Municipal Plans in this area.



Secondary Plan Map - South Core



Urban Mid-Town/Corridor Rendering

1.4 Community Vision

The South Core is a vital mixed-use neighbourhood in the heart of Fredericton that will continue to evolve as a complete community, providing a high quality of life for its residents through beautiful public spaces and a wide range of amenities. Even though this area will play a major role in supporting the residential growth of the Urban Core through mid- and high-rise infill development, the South Core will retain its historic character and create an engaging, welcoming, and accessible pedestrian experience through excellent urban design. As a primary gateway to the City Centre, all modes of transportation will be well-supported and connected.

1.5 Community Goals

In the Municipal Plan, there are 9 Community Goals developed through extensive public consultation to guide future growth and land use planning decisions. The South Core Plan strives to meet each of these goals as follows:

1. Welcoming and Supportive

- a) Support a range of employment opportunities through mixed-use development.
- b) Prioritize schools in the neighbourhood.
- c) Provide a diverse range of housing types and unit sizes for all people.



2. Strong and Diverse Economy

- a) Provide increased opportunities for small and medium-sized businesses in mixed-use buildings.
- b) Support flexible live-work spaces.
- c) Allow a range of appropriate commercial uses.



3. Culturally Rich and Diverse

- a) Provide opportunities for public art and cultural heritage within open spaces and the public realm.
- b) Allow spaces for gathering throughout the area in both private and public spaces.



4. Complete Neighbourhoods and Distinctive Places

- a) Provide access to all basic amenities in close proximity to where people live.
- b) Integrate new development with the existing area character.
- c) Ensure a high standard of urban design and architecture through Urban Design Guidelines.





5. Vibrant Downtown and Riverfront

- a) Create conditions that support the goals of the City Centre through new residential development and well-connected north-south transportation networks.
- b) Transition character and public realm design to City Centre.



6. Complete Transportation System

- a) Align transit routes and stops with high-rise residential nodes and employment areas.
- b) Connect trails and cycling facilities with the surrounding network.
- c) Improve pedestrian safety at intersections.



7. Safe and Inviting Public Realm

- a) Support safety improvements to parks and trail system, such as lighting and CPTED.
- b) Design public realm and open spaces for all seasons, all ages, and all abilities.



8. Green and Healthy

- a) Protect greenspace and trees, ensuring proper stormwater retention and tree canopy retention.
- b) Support park spaces and facilities with a diversity of recreational activities.
- c) Look for opportunities to increase public greenspace (parklets, boulevard gardens, rain gardens, etc.) or use nature-based solutions for stormwater management.



9. Sustainable and Efficient

- a) Support compact forms of development to minimize the carbon footprint.
- b) Support densification and mixed-use infill for the efficient use of space, municipal infrastructure and services (such as water and sewer, garbage collection, etc.) leading to savings in taxpayers monies as well as energy and emissions.

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Charlotte Street

Cities shape growth through policies that encourage and promote different forms of development in the areas where they are best suited. Together with the Urban Design Guidelines, the following policies will provide the necessary framework to ensure new infill development in the South Core will be of high-quality and will thoughtfully integrate into the community.

2.1 General Policies

The following policies apply to all development in the Plan Area:

Sustainable Development

1. Encourage net-zero, passive, LEED, and/or other environmental certifications for new development projects.
2. Encourage water re-use systems for buildings and/or landscaping.
3. Encourage planting of native vegetation species that are drought-resistant and pollinator friendly, where possible, to reduce water consumption and contribute to the ecological value of the area.
4. Support the planting of trees to increase tree canopy coverage.
5. Encourage the use of high-quality, durable, and sustainable building materials.

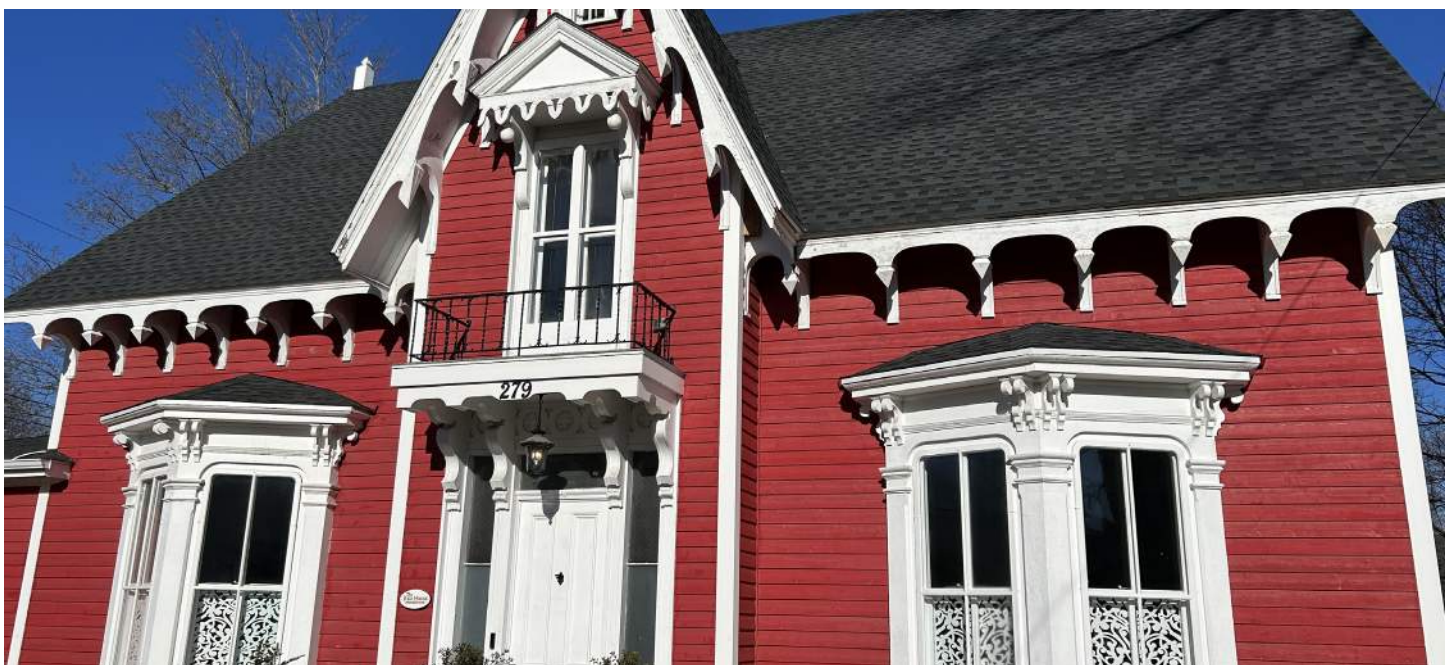
Accessibility

6. Encourage development within the Plan Area that incorporates universally accessible, age-friendly design and is mindful of the different needs that people with various disabilities have in accessing the built environment.
7. Encourage development to provide for passive surveillance of all open spaces and flex spaces, such as through significant first-floor windows/glazing and frequent entrances, and/or ground floor residential units with their principal entrances on the street.

Housing

The South Core is a critical residential community in the Urban Core and will need to accommodate over 6,000 new residents in the next 25 years. It is a priority for the South Core Plan to create supportive conditions for housing in all forms. Given the mature housing stock already in place, new development will primarily be multi-residential dwellings (e.g. apartments and stacked townhouses) while retention of low-rise residential forms (e.g. single-detached dwellings and converted dwellings) will be most appropriate in areas of historic character.

8. Support affordable housing development by:
 - a) Requiring a minimum of 20% of units on publicly-owned land to be considered affordable housing;
 - b) Providing density and/or building height bonuses to support the feasibility of providing affordable units;
 - c) Encouraging innovative housing forms;
 - d) Allowing single-room occupancy residential use in Corridors and Mid-Town land use areas, as permitted in the Zoning By-law; and,
 - e) Encourage supportive housing forms in suitable areas. Notwithstanding subsection 3.1.1(10)(iii) of the Municipal Plan, emergency/homeless shelters may only be considered within the Corridor and Mid-Town land use areas of the South Core.
9. Encourage the redevelopment of commercial properties as mixed-use residential buildings.
10. Encourage larger units (i.e. 3 or more bedrooms) in multi-residential buildings to support intergenerational families and larger non-family households.
11. Encouraging the development of forms of housing units targeted to the needs of senior citizens.
12. Require safe bicycle storage facilities for multi-residential development.
13. Development with a transit stop along its street frontage shall incorporate a pedestrian connection from building entrances, where feasible.
14. Encourage accessible amenities, such as seating and shelter, in the landscaped area of development, adjacent to transit stops, when warranted and as outlined in the Fredericton Transit Service Standards.
15. Reduce the impact of river flooding on new residential dwellings (e.g. ensure habitable spaces are built above a 9-metre geodetic elevation).
16. Ensure that all mechanical equipment, including roof mechanical units, are screened or incorporated within the building's design.



Regent Street House

Historic Character

As one of the City of Fredericton's historic mature neighborhoods, there are many different building types in the South Core that make up the built environment, such as large civic and institutional buildings (e.g. schools and places of worship), brick-and-beam factories, and original modest houses dating back to the late 19th century. Historic character policies prioritize the maintenance of architecturally significant buildings and ensure that new development complements the neighbouring structures that have defined the South Core for over a century.

17. Encourage the conservation, rehabilitation, and adaptive re-use of historic buildings and structures.
18. Development adjacent to historic or architecturally significant buildings to be designed in a manner that is complementary to existing structures and does not detract from the overall character.
19. Support and restore the street grid pattern including multi-modal connections, where possible.

Archaeological Resources

20. Encourage developers to follow all appropriate predetermined procedures if or when archaeological resources are encountered, including monitoring by assigned members of the Wolastoqey Nation.

Economic Development

The South Core is home to many different businesses spread throughout the neighbourhood, supporting residents both inside and outside the community. Most are located within the Mid-Town land use area, which is where the focal point for commercial opportunities will be as the neighbourhood grows. Currently, the Corridor land use areas are primarily residential with some small businesses located close to larger intersections. The evolution of these corridors into a mixed-use environment with modest increases in building height and flexible ground floor uses (e.g. live-work units, cafes, etc.) will create a more engaging street environment. It will also reinforce north-south transitions into the City Centre and provide additional commercial uses to support the anticipated new residents.

21. Encourage commercial development that serves primarily local resident needs, including but not limited to retail, restaurants, entertainment, and professional services.
22. Encourage commercial uses on the ground floor of multi-residential buildings with a building height of 4 storeys or greater.
23. Encourage the development of urban format grocery stores, with a smaller overall building footprint and residential uses above.
24. Encourage outdoor common amenity spaces, such as pedestrian plazas and outdoor seating areas as a component of new commercial or mixed-use development, to include:
 - a) landscaping and tree planting, particularly shading of parking lots, patio spaces, and plazas; and
 - b) planting of native vegetation.
25. Encourage flexible ground floor uses such as amenity spaces, internal at-grade parking, and live-work units in mixed-use developments.
26. Encourage front yard setback areas for patio space serving adjacent commercial businesses.
27. Ensure new commercial development adjacent to a residential use minimizes negative impacts related to parking, loading, waste collection, and vehicle access as per the Urban Design Guidelines.
28. Prohibit any uses that manage and/or store hazardous materials in the Plan Area.
29. Prohibit new vehicle-oriented uses, such as gas stations and drive-thrus.
 - a) Electric vehicle charging stations may be considered an exception.

Institutional

The South Core is home to a diverse range of institutional uses that include places of worship, assisted living centres, educational facilities, and a fire station. These are all important amenities for a healthy and complete community that should be supported. As the community grows, there will likely be a need for more of these services throughout the Plan Area and thus, it is important to understand how to integrate these uses into the urban fabric.

30. Prioritize the lots between Smythe Street and the Connaught Street School for the future school expansion if/when they become available.
31. Encourage the integration of community uses (arts centres, libraries, seniors centres etc.) within the ground floor of in mixed-use developments and around parks and open spaces.
32. Support new institutional uses in all land use areas, subject to Section 2.2.1 (43) of the Municipal Plan.
33. Encourage the conversion of institutional uses as multi-residential or mixed-use development, which may permit commercial or employment uses that serve the community.
34. Encourage the redevelopment of former institutional uses that match the grid pattern and character of the surrounding area.

Recreation, Parks, and Open Space

Queen's Square represents the largest park space in the South Core, but there are many smaller publicly-owned open spaces that need to be supported as an amenity for residents. This includes the Cross Town Trail and linear green spaces along Smythe Street and University Avenue. Furthermore, infill development will require the integration of new open spaces that could be transferred to public ownership.

35. Prioritize new open spaces located within the Mid-Town land use areas, designed to serve the local community as either publicly owned or privately operated public space.
36. Support the creation of new parks, open spaces, and recreational facilities as the opportunity arises within the Plan Area, subject to Section 2.2.1 (47) of the Municipal Plan.



St. Paul's United Church

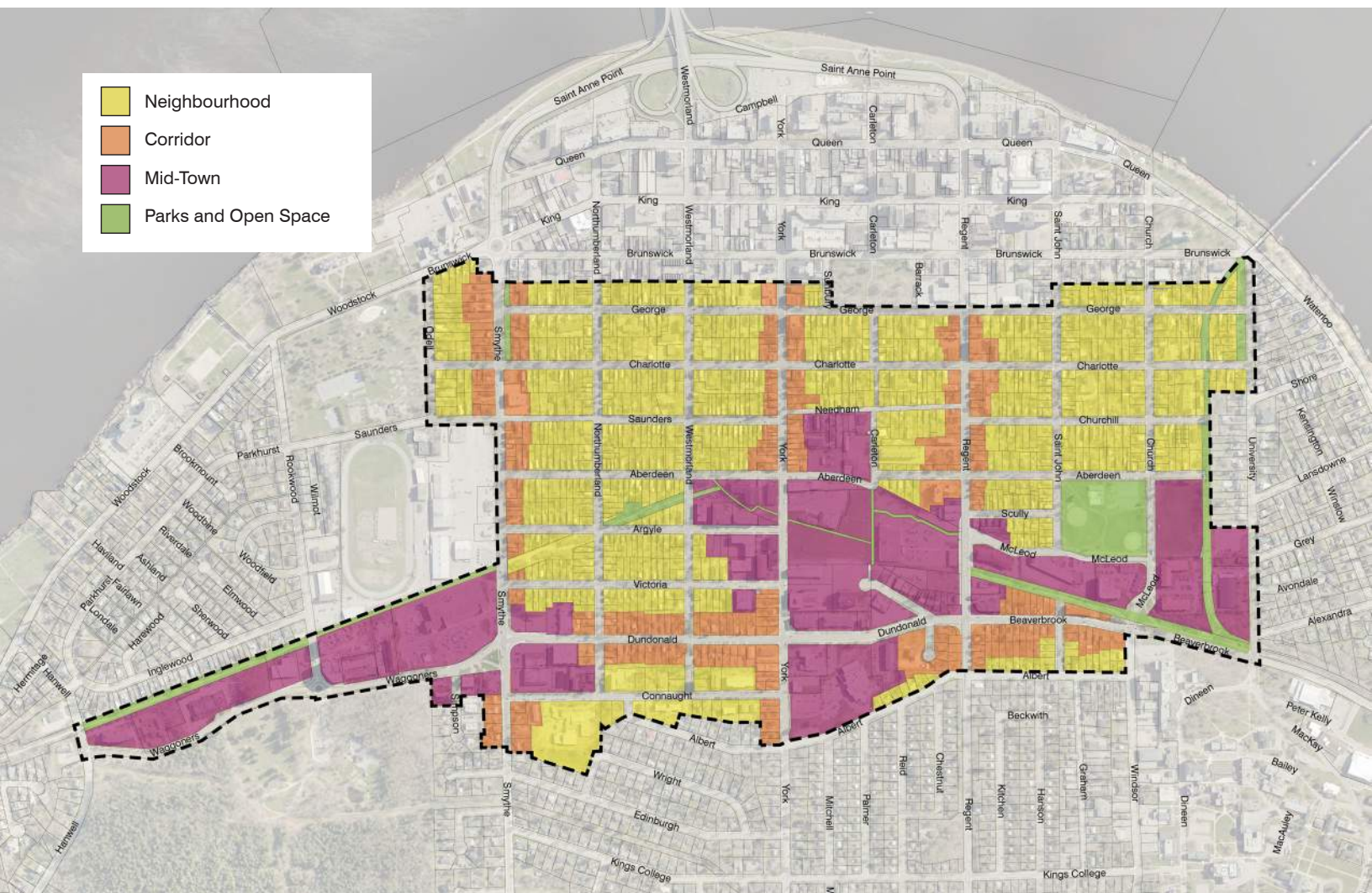


Queen's Square Park Aerial Photo

2.2 Land Use Area Policies

Through a comprehensive analysis of the urban structure and built form in the South Core, four distinct character areas were defined: Neighbourhood, Corridor, Mid-Town, and Parks and Open Space. Each area contains strong traits that create unique design and land use parameters to be further enhanced as the South Core evolves. The following policies enhance and support these distinctions while providing a sensitive transition along their boundaries.

1. Map 1 – Land Use Map establishes the location of the following land use designations:
 - a) Neighbourhood
 - b) Corridor
 - c) Mid-Town
 - d) Parks and Open Space (refer to policy 3.2)



Map 1 - Land Use Map

Neighbourhood

Intent

The Neighbourhood area is defined by a consistent grid pattern with long narrow lots. Over time, many of the modest single-detached houses in this area have been converted to multi-unit dwellings through attached secondary dwelling units extending back into their rear yards. This evolution was intended to accommodate larger families and multiple tenancies, while still reflecting the loyalist architecture of the community. Policies for this land use area will continue to protect the lot pattern, while allowing for appropriate infill development (e.g. accessory apartments) and conversions. Overall, the least amount of built form change is expected in this area and development is generally restricted to low-rise built forms.

2. Map 2 – Neighbourhood Character Area Map further defines the Neighbourhood area based on the historical context and built form characteristics to account for the unique properties across the community.
3. Development shall be compatible with the scale and massing of adjacent residential buildings.
4. Development shall respect and maintain the average front yard setback of adjacent properties, to be measured from the front wall of the main residential dwelling, allowing porches, stairs, canopies and other entrance features to encroach into the front yard setback.



Map 2 - Neighbourhood Character Map

5. Development shall preserve a contiguous area of rear yard landscaping, referred to as sponge area, for the support of stormwater management and maintenance of the urban tree canopy.
 - a) Where no contiguous area of rear yard landscaping is present at the time of development, the sponge area shall be directed to a location on site that can be well-maintained and allow for a future contiguous area to be created;
 - b) All structures on-site shall minimize the impact on the sponge area, as indicated in the Urban Design Guidelines.
6. Support additional dwelling units as follows:
 - a) Modest conversions of existing low-rise residential buildings that maintain the street-facing character are encouraged;
 - b) Garden and accessory apartments located in the rear yard are encouraged provided they minimize the impact on the sponge area, as indicated in the Urban Design Guidelines; and,
 - c) Exterior façade design shall be complementary in overall appearance to the main residential dwelling.
7. Neighbourhood Character Area specific policies are as follows:
 - a) Lot consolidation in Character Areas 1, 2, and 3 as defined in Map 2 – Neighbourhood Character Area Map is not permitted.
 - b) Residential development in Character Areas 1, 2, and 3 containing a maximum of 6 units and up to 3 storeys in building height may be permitted, except:
 - i. Character Area 3 may permit up to 8 units provided the sponge area is maintained, subject to the requirements of the Zoning By-law; and
 - ii. Conversion of institutional uses may exceed 6 units, subject to the requirements of the Zoning By-law.
 - c) Development in Character Area 1 is subject to the St. Anne's Pointe Heritage conservation By-law.
 - d) Converted dwellings in Character Area 2 should be permitted only when maintaining or enhancing the historic character.
 - e) Development in Character Area 4 may:
 - i. be permitted in multi-residential building forms;
 - ii. be built up to 4 storeys in height; and,
 - iii. include lot consolidation.
8. Commercial uses on mid-block lots should be limited to home occupations.
9. Commercial uses intended to serve the local neighbourhood may be permitted on corner lots in a mixed-use building form or in a building that does not contain a residential use, and does not exceed 2 storeys in height.
10. Institutional uses including educational facilities, places of worship, and assisted-living facilities in low-rise built forms are considered an appropriate use in the area and shall also conform to the Institutional land use policies in the Municipal Plan.

Corridor

Intent

Corridor areas are distinguished from the Neighbourhood areas by their greater intensity and variety of uses, as well as frontage along streets with higher traffic volumes (Smythe, York, Regent and Mid-Town area). Current forms of development and uses range from single-detached houses to commercial and multi-residential uses. Policies for the Corridor area promote new development with active building frontages, including entrances along the street, and a flexible mix of residential and commercial uses. It is anticipated that there will be a moderate amount of change over the lifecycle of the Plan, with a built form that transitions respectfully to the adjacent Neighbourhood area. New development may include both low-rise and mid-rise built forms.

11. Development shall be a minimum of 2 storeys and a maximum of 7 storeys in building height, except on prominent corner lots, where up to 8 storeys maximum building height may be considered, provided all policies and guidelines relating to building height transitions are met.
12. Development shall include design considerations for privacy using screening and landscaping buffer-s, where appropriate.
13. Development shall require a street-facing building setback at the 3rd, or 4th, or 5th storey level.
14. Buildings on corners should provide active frontages and a similar design treatment on both sides fronting streets or at the corner, which can include secondary entrances or single-unit entrances.
15. Encourage the development of multi-residential and mixed-use built forms.
16. Encourage the consolidation of smaller residential lots to create larger developable blocks.
17. Permit low rise housing forms such as single-detached, semi-detached and townhouse forms that are in keeping with the intent and policies of the Corridor Designation and Urban Design Guidelines.
18. Encourage residential uses on the ground floor to integrate with the public realm through landscaping and design features (e.g. front stoop, semi-private space), live-work units, or allowing for the conversion to commercial use when feasible.
19. Encourage high-quality landscaping in the front yard that is coordinated with streetscaping and accommodates spill out of active at-grade uses, as per the Urban Design Guidelines.
20. Residential development shall incorporate accessible pedestrian connections from the nearest building entrance to all abutting trails or sidewalks.
21. Prohibit parking in front of buildings.
22. Support underground and/or screened at-grade structured parking.
23. Encourage the preservation of existing mature trees and natural areas to support the sponge area.
24. Development should incorporate underground stormwater management systems, or other innovative stormwater systems, and connect to existing municipal storm water infrastructure, where necessary.

Mid-Town

Intent

The Mid-Town area is intended to accommodate the majority of population growth in the South Core through all types of built form (low-rise, mid-rise, and high-rise). Lots are typically larger in total area, containing a majority of the South Core's commercial uses, such as the neighbourhood's two primary grocery stores. To achieve the concept of a Fredericton-based Mid-Town community and accommodate the necessary growth, bold steps should be taken to support high-density residential and mixed-use development. Doing so also requires strong guidelines for designing spaces where people want to gather and experience the city. This area is fortunate to have an existing foundation of pedestrian infrastructure that can be further leveraged to create a walkable and connected environment.

25. Development shall be a minimum of 2 storeys and a maximum of 12 storeys in building height, except on corner lots within gateways (as defined in the Urban Design Guidelines), which may permit up to 15 storeys where all other policies and guidelines relating to building height transitions are met.
26. Development shall require a street-facing building stepback at the 3rd, or 4th, or 5th storey level.
27. Encourage the consolidation of smaller lots to enable the comprehensive and coordinated development of larger areas.
28. Where lots directly abut Neighbourhood areas, transition in building height and massing is required as defined in Section 2.3 of the Urban Design Guidelines.
29. Lots adjacent to the NBEX site shall consider land use compatibility and connections that support all modes of transportation.
30. Development shall ensure well-designed pedestrian access from all building entrances to the nearest sidewalk or trail.
31. Require buildings on corners to provide active frontages and a similar design treatment on both sides fronting streets or at the corner, which can include secondary entrances or single-unit entrances.
32. Encourage a mix of residential forms on large development blocks including mixed-use buildings, apartments buildings, and stacked townhouses.
33. Development abutting new or existing trails should have active frontages, entrances, windows facing and accessible pedestrian connections to the trail.
34. Residential development abutting the Valley Trail from Hanwell Road to Rookwood Avenue shall not exceed a maximum building height of 4 storeys.
35. Existing mature trees shall be retained as landscaped areas where feasible.
36. Development shall provide either landscaped areas, common amenity spaces, privately operated public space (POPS), or land for public purposes to support the creation of urban plazas.
37. Institutional uses may be permitted on the ground floor of all buildings.



New Brunswick Community College Building on Smythe Street

The physical components that serve and support growing communities are found in the public realm. It is where people move, gather, and experience the natural environment. In the effort to create beautiful and livable cities, the public realm needs to acknowledge the importance of balancing form and function. For example, when the operational needs of a street are harmonious with the design of the streetscape and the buildings that face it, the public realm becomes a seamless and integral part of the community. This ideal is even more important when considering the need to accommodate a greater density of residential development. Streets and parks are the front yards for a community and therefore should be treated as such.

The following section provides policy direction for the public realm, which includes streets, sidewalks, trails, and parks. Urban Design Guidelines, appended to this Plan, further support these policies and provide the City with additional tools to enhance both the streetscape and open spaces.

3.1 General Policies

The following policies apply to all public realm spaces in the Plan Area:

1. Use CPTED principles in the design of the public realm to ensure that spaces feel safe, have adequate lighting, clear sightlines, and intuitive wayfinding.
2. Encourage the preservation of existing mature trees within the public right-of-way.
3. Encourage planting of tree species that supports the City of Fredericton Urban Forest Strategy.
4. Encourage development patterns that enhance the street grid pattern.
 - a) Support a future street extension that links Victoria Street to McLeod Avenue.
5. Ensure all future right-of-way improvements encourage multi-modal forms of transportation, balancing efficient vehicular movement with safe and comfortable pedestrian and bicycle circulation.
6. Encourage universal accessibility (e.g., age-friendly, dementia-inclusive, etc.) in future right-of-way improvements.
7. Design public spaces and streets to accommodate snow removal and storage, while ensuring the prioritization of pedestrian movement.

3.2 Parks and Open Spaces

The South Core is a mature neighbourhood with established green and open space areas containing a variety of amenities. These areas are important in providing ‘third place’ options for residents and visitors, places to gather outside of work and home. Four important parks anchor the Plan Area, Odell Park, Wilmot Park, Queen’s Square, and the Riverfront Trail and Green. While Queen’s Square is the only major public park located within the Plan Area, the proximity of the other parks provides accessible green space opportunities across the community. It is important to note that these areas support public art installations, which is a key component of the City of Fredericton Culture Plan.

1. Encourage the development and inclusion of POPS as a component of the Mid-Town public realm.
2. Engage with a diversity of community members in the design of new open spaces.
3. Encourage integrating public art projects into visible and accessible areas in the public realm, as defined in the Urban Design Guidelines.
4. Where possible, ensure all new parks and open spaces provide interconnected internal pathways that connect to existing parks and trails.
5. Ensure pathway alignment with mid-block crossings, where beneficial.
6. Support the inclusion of community gardens as a component of new open spaces.
7. Support the provision of barrier-free seating along trails, paths, and other activity areas in parks and open spaces.
8. Support the provision of amenities for nearby residents such as playgrounds, dog-runs, fitness equipment and BBQ/picnic areas.
9. Encourage and support innovative, low-maintenance native species, drought-tolerant and pollinator friendly planting on boulevards and other public open spaces (e.g. traffic calming circles). This may include community groups or private individuals adopting spaces for the purposes of gardening and maintenance.

Linear Green Spaces

The Plan Area is home to two linear green spaces along Smythe Street and University Avenue that were allegedly reserved for farmers’ markets and cattle enclosures at the time of the original survey conducted in 1786. While they likely never served those purposes, they remain part of the public realm today.

10. Maintain the existing publicly-owned green space along Smythe Street and University Avenue as important extensions of the public realm.
11. Support new development along linear green spaces that enhances the public use and visibility of these areas.
12. Support the installation of public art as a gateway feature along the edge of the South Core and City Centre boundaries.

3.3 Mobility

The South Core supports the movement of all transportation modes, given its position as the southern gateway to the City Centre. While vehicle traffic flow remains a critical component to a well-functioning transportation system, it should not detract from the pedestrian experience. As stated in the Municipal Plan, a 'pedestrian first' principle shall apply to all design and capital works when implementing City plans and guidelines. Benefits to active transportation and transit users should be given more weight than vehicle traffic.

Neighbourhood Streets

Neighbourhood streets are defined as the public right-of-ways located within the Neighbourhood land use area, comprising most of the road infrastructure in the Plan Area. Generally, residents feel there is a positive pedestrian experience while travelling along these streets, which should be maintained. These streets are typically classified as residential or minor collectors within Schedule 1 (Urban Structure Map) of the Municipal Plan.

1. Ensure mid-block curb drops, such as paved driveways, are available to allow for accessible vehicle pick-up and drop-off at reasonable distances.
2. Discourage through-traffic use of neighbourhood streets with traffic calming measures such as, on-street parking, bump-outs and traffic calming circles, where appropriate.
3. Ensure safety of pedestrian crossings along Westmorland Street through bump outs and other traffic-calming measures that slow traffic flow.

Corridor Streets

Corridor streets are defined as the primary public right-of-ways for vehicular traffic in the Corridor and Mid-Town areas. This would also include any new public streets established within the Corridor of Mid-Town areas. Pedestrian experience in the corridors can vary and, in some cases, is viewed as unfriendly compared to local streets. Supporting active commercial and residential street frontages that are enjoyable, safe, and accessible is important to the growth of the community. These streets are typically classified as arterials or major collectors within Schedule 1 (Urban Structure Map) of the Municipal Plan.

4. Support pedestrian-friendly environments along all Corridor streets through appropriate buffering between the sidewalk and the street, which may include street furniture, vegetation, and bike racks.
5. Ensure safe and convenient pedestrian crossing opportunities at appropriate intervals along the street.
6. Consider incorporating on-street parking to support retail and residential uses on York Street, north of Aberdeen Street.
7. Support the spillover of commercial uses, such as patios and cafes, into the public right-of-way provided it does not hinder accessible pedestrian traffic.
8. Driveway entrances are not encouraged when there is site access available from an adjacent local street, with no more than one driveway entrance generally permitted unless the lot frontage width is greater than 40 metres.

Boulevards

9. Consider the integration of Low Impact Development strategies, such as rain gardens, that help improve stormwater management in the public realm.

Transit

10. Transit routes in the South Core, both existing and new, to be provided in accordance with the Fredericton Transit Service Standards.
11. Ensure accessible public seating and other pedestrian amenities, such as shelter, are provided at trip generating transit stops in the South Core, in accordance with the Fredericton Transit Service Standards.
12. Ensure any street furniture and signage is not within the 'sweep path' of transit vehicles along transit routes.

Trails

13. Prioritize trails as a major open space amenity and year-round active transportation link.
14. Ensure trails are well-lit year-round, where feasible, with seating areas spaced at frequent intervals.
15. Ensure trail crossings at public streets include traffic calming measures and infrastructure that prioritize pedestrian safety.

Bike Lanes

16. Support on-street bike infrastructure along Northumberland Street, Carleton Street, and Church Street to enhance primary north/south bike routes.
17. Prioritize Cross Town Trail infrastructure and safe crossing for cyclists travelling east-west through the Plan Area.
18. Encourage well-marked and safe bicycle paths, with clear wayfinding and year-round maintenance.

Sidewalks

19. Where feasible, public sidewalks should be widened to accommodate wheelchair users.
20. Sidewalks should be located on both sides of all streets.

3.4 Municipal Services and Utilities



Regent Street

Water and Sanitary Sewer Services

1. Ensure water and sanitary infrastructure is maintained in a state of good repair and required new infrastructure is developed concurrent with growth.
2. Developers shall contribute to the realignment and increased capacity of water and sanitary sewer services, where required.

Stormwater Management

Responsible and efficient management of stormwater runoff is critical as the South Core continues to develop alongside the realities of climate change, such as increased rainfall events. The ‘sponge area’ is one of the concepts included in this Plan to ensure the private realm is preserving soft landscaped area for natural water infiltration. A network of underground infrastructure in the public realm ensures collected runoff is released back into the watershed in a sustainable manner.

3. Promote stormwater retention in the rear yards (sponge area) of the Neighbourhood areas through the retention or replanting of landscaped areas.
4. Restrict post-development peak runoff, in alignment with current regulations, to that of the pre-development condition.
5. Require Stormwater Management Plans and underground stormwater retention systems, if required, for development in the Corridor and Mid-Town land use areas.
6. Public parks, open spaces, and local street boulevards should be retained as sponge area where feasible for snow storage and stormwater infiltration.

Other Utilities

NB Power operates a substation at 437 Aberdeen Street, in the heart of the South Core within the Mid-Town area. As an important piece of infrastructure to the entire Urban Core that has recently been upgraded, this site will need to integrate better into the surrounding development. Currently, all power lines in the Plan Area are above ground, which plays a role in the overall aesthetic of the community. Due to the significant costs of transitioning to underground services, existing electrical infrastructure will remain above ground in the public right-of-way for the life cycle of this Plan.

7. Require new public right-of-ways in the Mid-Town land use area to incorporate underground electrical infrastructure, where possible.
8. Support the use of alternative energy (except geothermal heating systems) and emission reduction in all land use areas through solar panels and energy efficiency retrofits.
9. Prioritize the screening of utility uses, such as substations and mechanical equipment, from the public right-of-way.
10. Promote and encourage the conversion of oil energy to alternative energy in order to eliminate the use of oil tanks within environmentally sensitive areas of the Plan Area to protect the aquifer.

Wellfield Protection

The South Core is within the City’s Wellfield Protection Area, primarily in Zone A and B, which presents significant pollution risks from petroleum products, chlorinated solvents, and other persistent chemicals. Eleven drinking water wells are located near Wilmot Park and Queen’s Square park, meaning it is critical to prohibit contamination from hazardous land uses.

11. Hazardous materials are not permitted to be stored in the Plan Area.
12. Ensure that developments are serviced in a manner that meets the Wellfield Protection Designation order – *Clean Water Act* and City By-laws to protect the city’s drinking water supply, or receives exemptions as appropriate.



Part 4 Achieving Growth

This Plan is meant to convey how long-term development and public realm improvements will occur over the next ~25 years, as well as directly guide the short-term opportunities and aspirations of residents and developers. That doesn't mean this is a rigid document. The way Fredericton grows and evolves may shift from what we envision today and revisiting the policies of the South Core Plan regularly will ensure we're moving in the right direction.

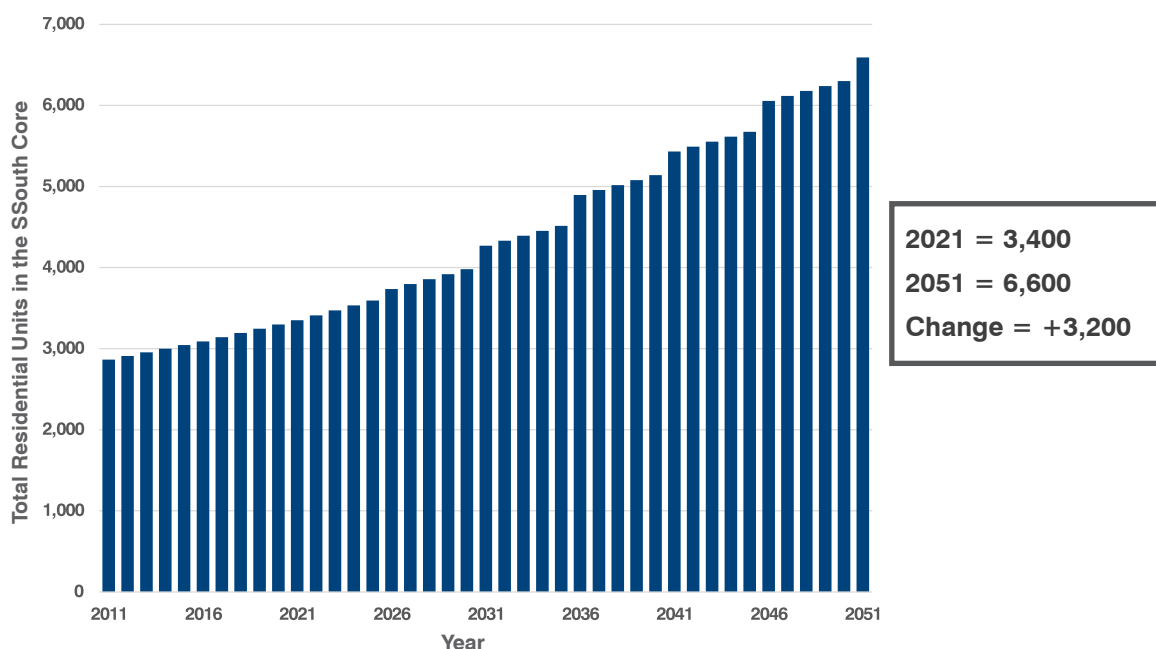
4.1 Major Themes

While developing the South Core Plan, there were several 'Major Themes' that emerged that will impact the evolution of the neighbourhood.

Major Development in the Mid-Town

For the South Core Plan to be successful in the goal of providing housing at the projected rate of growth, it will be important that new major developments are brought onto the market periodically and consistently. Assuming a baseline of 3,400 existing residential units (as of 2021) in the South Core and an approximate goal of 6,600 units by 2051, more than 100 units per year must be constructed. Year-over-year, this number may be challenging to maintain but given the potential for major developments in the Mid-Town, new multi-residential buildings above 10 storeys will help infuse these units into the area as necessary. Looking at the graph below, large increases in units every ~5 years from these potential infill projects ease the development pressures over the long-term.

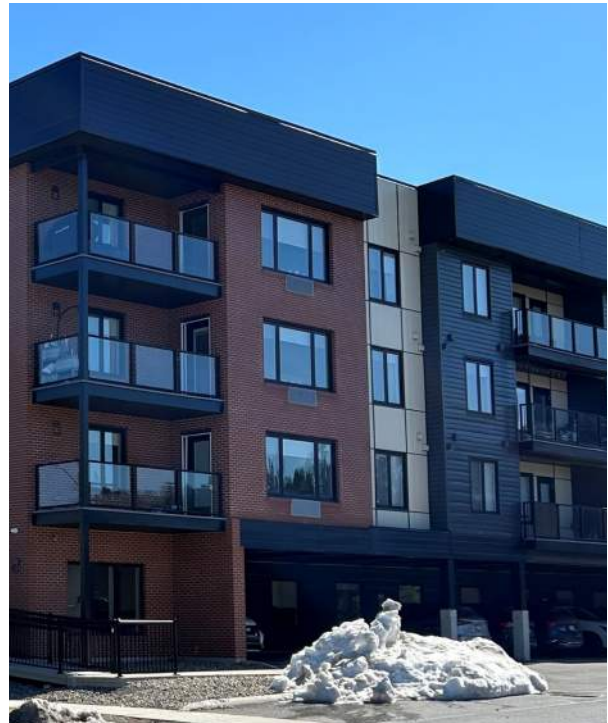
While there are no plans for specific developments or locations to be constructed at predefined times, City Staff should ensure that infrastructure upgrades that support increased capacity align with major development growth years.



New Urban Corridor Identities

The current identities of the four major corridors in the Plan Area (Smythe Street, York Street, Regent Street, and Mid-Town) are varied in both their uses and their form. Through infill development opportunities that incorporate higher density residential with a mix of commercial uses, these primary gateways to the City Centre can evolve to become true urban corridors, with active building fronts along the sidewalk, appropriately screened and landscaped townhouse entryways, and attractive apartment entrances.

While it is beneficial to establish shared goals and policies across these corridors, it is also important to recognize their unique qualities that come with their public realm environments (i.e., street width, sidewalks, cycling and transit infrastructure), lot pattern (i.e., width and depth), and historic properties. It is not a one-size-fits-all approach and as new development is proposed, the surrounding context will play a key role in what is appropriate for the area.



Enhancing Character with Compatible Development

Diversity of built form is one of the South Core's strengths. The area has a strong historic residential character that reinforces the identity of the core community that is a great place to live in the centre of the city. It is critical that new development is compatible and integrates thoughtfully with the existing built form. This will be accomplished through limiting the intensity of development in the Neighbourhood land use areas, ensuring building design is complementary with the existing built form, and maintaining appropriate transitions of taller buildings adjacent to low-rise housing.

Furthermore, in the Mid-Town areas with less sensitivity to high-rise infill development, it is a priority to establish a modern urban character through high-quality design and engaging, accessible open spaces. These infill areas are also primary gateways to the City Centre and should reflect the city's identity as a growing urban centre with a rich history, highlighted by art and cultural features in the public realm.



4.2 Proposals

Where policies provide guidance primarily to development on private property, proposals are the priority of Council that include both the public realm and publicly-owned properties. Using the four lenses established over the course of the public engagement process, here are the proposals for the South Core Plan.

Building

1. Council shall explore a long-term redevelopment plan for the Lady Beaverbrook Rink and adjacent parking lot.
2. Council shall work with the University of New Brunswick to establish a long-term plan for College Field that integrates with the surrounding community and the goals of the South Core Plan.
3. Council shall continue to pursue dialogue with various levels of government in order to protect the vitality of the two existing educational institutions and support a new school on the NBEX site.
4. Council shall ensure that upgrades to municipal systems are prioritized to support infill growth in the South Core.
5. Council shall explore reductions to parking requirements for new development in the South Core to support the goals of this Plan.

Historic Character

6. Council shall maintain an inventory of historic buildings in the South Core Plan Area.
7. Council shall explore models to incentivize the rehabilitation of historic buildings in the South Core.
8. Council shall explore the creation of an external Design Review process, similar to the City Centre Plan.
9. Council shall collaborate with NB Power to screen the Aberdeen Street substation on all sides with opaque material that is compatible with the character of the surrounding area.

Greening

10. Council shall initiate a Queen's Square Park Plan that supports the needs of the surrounding community as a neighbourhood park, as defined in the Municipal Plan and as per the Recreation and Leisure Master Plan.
11. Council shall explore formalizing a development agreement process for POPS that includes standard signage.
12. Council shall support the development of a new urban plaza in the Mid-Town area, located between York Street and Regent Street.
13. Council shall ensure maintenance procedures are included with the installation of public art.



Queen Street

Mobility

14. Council shall explore the viability of an overnight on-street parking model when there is sufficient pressure on parking in Neighbourhood areas.
15. Council shall explore alterations to transit routes and stops in the South Core that provide appropriate service levels in accordance with the Fredericton Transit Service Standards to residents living in the Mid-Town area.
16. Council shall update the transit network in the South Core to serve the densest population areas while aligning with the Fredericton Transit Service Standards, objectives, and policies as density increases.
17. Council shall explore the reduction of traffic speeds to 30 km/h on local streets.
18. Council shall support the connection of north-south active transportation infrastructure along Carleton Street to connect to the City Centre.
19. Council shall support the completion of the connected Cross Town Trail network, specifically focusing on a short-term solution from York Street to Regent Street.
20. Council shall support a future street connection from Victoria Street to McLeod Avenue.
21. Council shall explore opportunities to upgrade the Dundonald Street and Smythe Street intersection in a way that balances the needs of mobility with the development opportunities of the adjacent sites.
22. Council shall prioritize light installation along the Cross Town Trail and McLeod Avenue trail.

4.3 Interpretation

For the purposes of interpreting this Plan, the definitions of applicable Provincial legislation, the Municipal Plan, and the City's Zoning By-law shall apply.

1. Recognize that the Land Use Designations, as shown on Map 1 – Land Use Map, are intended to be approximate, except where they coincide with roads or other clearly defined physical features. Where general compliance with the South Core Secondary Municipal Plan policies is maintained, minor boundary adjustments to the land use designations shall not require an amendment to this Plan.
2. Notwithstanding any approval or site specific amendment made or issued under the former secondary municipal plan or any other by-law made pursuant to the Community Planning Act, where a conflict exists between such prior approval or site specific amendment with this secondary municipal plan, then the provisions of this secondary municipal plan shall prevail.

Glossary

Affordable Housing

For the purposes of the South Core Plan, Affordable Housing is defined as dwelling units constructed under a municipal, provincial, or federal affordable housing program.

Building Heights

For the purposes of this Plan, building heights are categorized as follows:

- Low-Rise = 1-3 storeys
- Mid-Rise = 4-7 storeys
- High-Rise = 8+ storeys

Compatible Development

Development that, although it is not necessarily the same or similar to existing buildings in the vicinity, nonetheless enhances an established community and coexists with existing development without causing undue adverse impact on surrounding properties.

Crime Prevention through Environmental Design (CPTED)

CPTED is founded on the belief that the proper design and effective use of the built environment can lead to a reduction in the incidence and fear of crime, and an improvement in the quality of life. Some of the key concepts include natural surveillance through creating clear sight lines and well-lit environments, natural control access through clear borders and barriers, and territorial reinforcement through creating spaces with clear purpose and timely maintenance.

Low Impact Development

Low Impact Development (LID) is an approach to land development that minimizes the environmental impact of development, particularly on water resources, by mimicking natural processes to manage stormwater runoff

Mid-Town Corridor

The public street right-of-way consisting of Waggoners Lane, Dundonald Street, and Beaverbrook Street, between Hanwell Road and University Avenue.

Passive Surveillance

Passive surveillance in Crime Prevention Through Environmental Design (CPTED) refers to the concept of designing spaces to allow for natural observation of activities, making it easier for people to see what's happening around them and potentially deterring criminal behaviour.

Sponge Area

The 'Sponge Area' is based off the 'Sponge City' design concept, where nature-based approaches are used for managing excess rainfall. Minimizing the amount of permeable surface in an area creates the conditions for uncontaminated storm water to drain into the soil and reduce the risk of flooding. In this Plan, the concept of retaining green areas in the back of residential lots also retains and supports the growth of the urban tree canopy, as well as reinforcing the existing character of the neighbourhood.

This concept also extends to larger development blocks, which are often significant producers of stormwater runoff with extensive swathes of concrete. Using permeable green spaces that break up the monotony of urban landscapes and providing guidance for good landscape design will help meet the goals of community and ensure resilience in the long-term.

Sweep Path

A transit 'sweep path' refers to the area covered by a vehicle, or any part of it, as it moves through a path, particularly during turns or maneuvers.

Third Place

The concept of a 'third place' was created in the 1980s but has continued to hold relevance today. It is a space where you don't need an invitation to enter, you can come and go as you please, there are minimal financial barriers, it is nearby, and interaction is highly encouraged. Beyond these characteristics, there are no set definitions on what a 'third place' looks like. It could be indoors or outdoors, public or private, quiet or loud, etc.

Based on the demonstrated desire from the young adult age demographic (18-35, which represent the highest population in the South Core) for these types of spaces, it is critical that there is consideration for how they are integrated into policy. The three ways in which they are included in this Plan are as follows:

- Urban squares that are activated with seating areas and interactive structures, centralized within higher density areas;
- Support for new commercial development that promotes gathering and socializing; and,
- Common amenity space within new high density residential development.

The success of a 'third place' requires both community buy-in and the conditions to support the ongoing use and maintenance by property owners, either public or private. This Plan will ensure that there is the necessary policy in place to support these spaces as the community continues to grow.

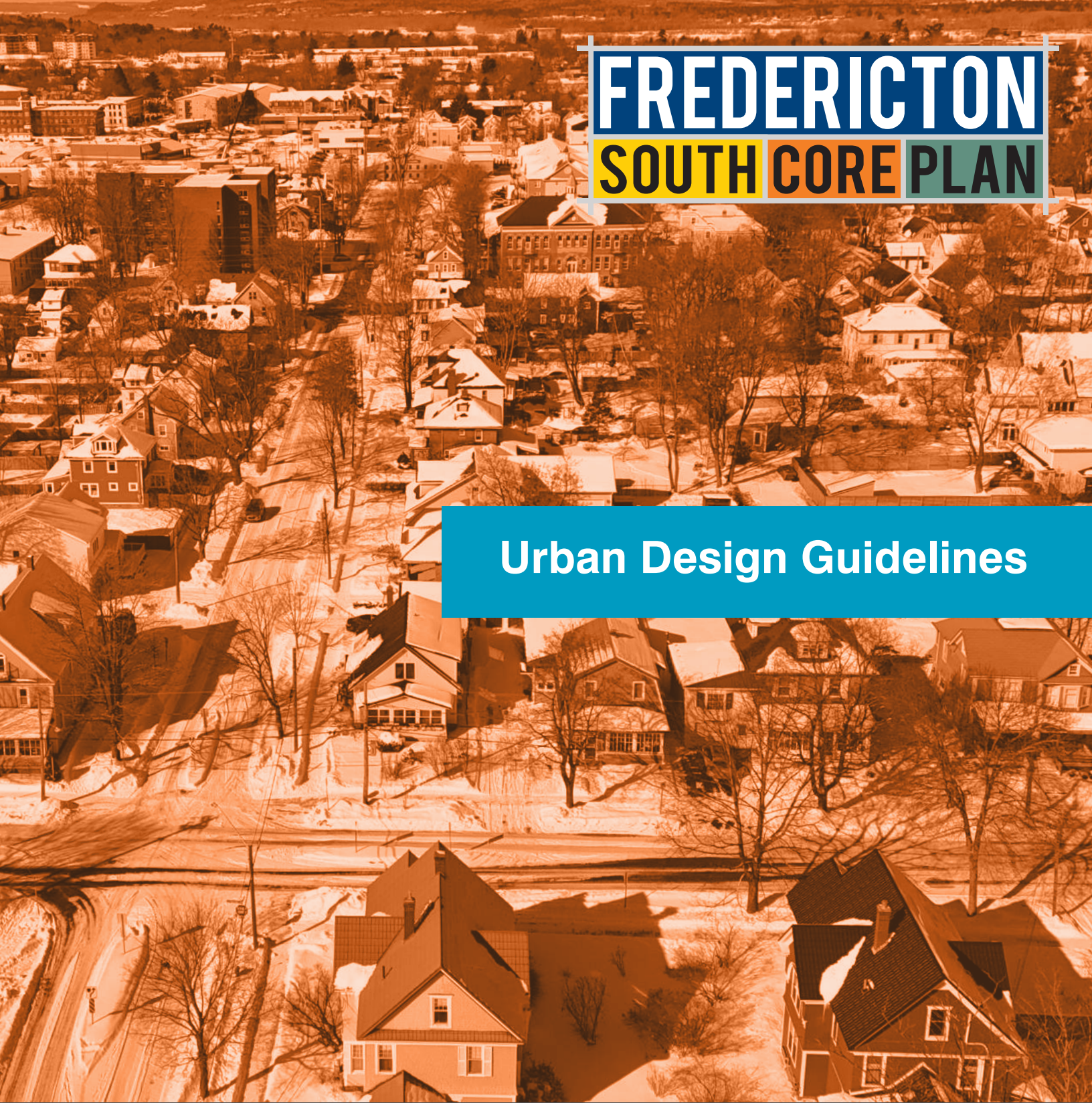
FREDERICTON

SOUTH CORE PLAN



Trace Planning & Design
CBCL
ERA Architects

Fredericton



FREDERICTON SOUTH CORE PLAN

Urban Design Guidelines

2025

Fredericton

 The Planning
Partnership

Trace Planning & Design

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Fredericton

Land Acknowledgment

We acknowledge that the City of Fredericton is situated on traditional Wolastoqey territory. The territory of the Wolastoqiyik people is recognized in the Peace and Friendship Treaties to establish an ongoing relationship of peace, friendship, and mutual respect between equal nations. The river that runs through our city is known as the Wolastoq, along which live the Wolastoqiyik, “the people of the beautiful and bountiful river”.



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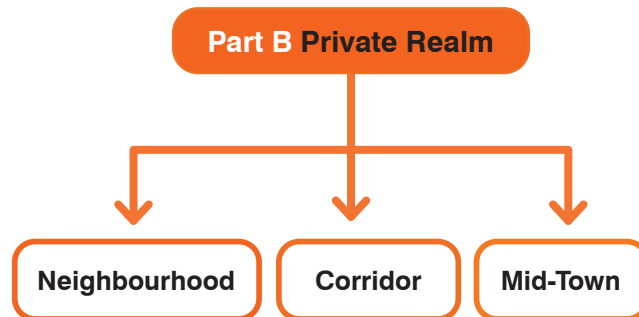
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1 Organization of the Document



1.1 Private Realm

What is the Private Realm?

The private realm is comprised of built form (buildings) and their associated parking, landscape, and open space within privately-owned parcels.

Built form refers to the design, shape, size, massing and exterior appearance of an individual building, or buildings in the collective. How buildings relate to each other and to adjacent spaces in terms of height, scale and character determines the extent to which they will define the public realm – the outdoor rooms that are the streets and gathering places of the city. Buildings, through their individual beauty and unique characteristics, collectively define a sense of place.

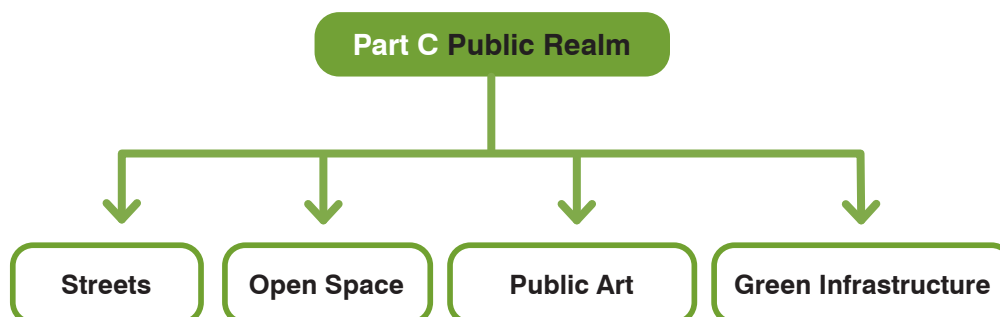
Fredericton's South Core is a complex mix of historic neighbourhoods, infill buildings, and large sites suitable for significant redevelopment. The lifespan of buildings is measured in decades – and for good ones, centuries. High-quality buildings that age well, are adaptable over time for different uses, and relate well to their context, have an importance well beyond the immediate functional or profit motive.

Purpose of the Private Realm Design Guidelines

The Private Realm Design Guidelines are intended to shape individual buildings, one by one, to create the collective whole for Fredericton's South Core. They are primarily concerned with creating a comfortable, safe and interesting pedestrian environment as perceived from sidewalks and public spaces, by focusing on creating a fine-grained, human-scaled building fabric. The Private Realm Design Guidelines respond to, and enhance, the unique character and circumstances of the South Core.

The primary purpose of the Private Realm Design Guidelines is to direct and shape development in the South Core in a balanced manner and according to good urban design principles. In particular, they seek to protect and reinforce the area's distinct history and built characteristics, while enabling investment, revitalization, and intensification opportunities as envisioned in the Municipal Plan. The intent of this document is to provide the City with a sound and rational framework for consistently assessing proposals.

The Private Realm Design Guidelines relate to the scale, character, and design of new developments, both public and private. While they are primarily concerned with buildings, they also provide guidance on other aspects of development on a lot such as access, parking, and landscaping.



1.2 Public Realm

What is the Public Realm?

The Public Realm generally refers to the publicly-owned land that constitutes the streets (the right-of-way), parks, and trails within the South Core. In the context of the South Core Plan, public realm also refers to privately-owned public space (POPS) that is made accessible to the public by way of legal easements and/or agreements. This new type of feature in Fredericton creates greater opportunities for green space and gathering space (also referred to as 'Third Places') in a mature urban neighbourhood.

Purpose of the Public Realm Design Guidelines

The Public Realm guidelines identify the various elements that make up the public realm - the streets, parks, open space, trails, and also POPS. They speak to the organization and composition of these elements towards achieving the vision and principles established for the South Core.

The guidelines are intended to be used by the City to direct capital projects and to coordinate with development applications.

It should be noted that the concepts and diagrams shown in the public realm guidelines are intended to illustrate principles and concepts only; they are not detailed drawings.



Church Street

A 2 Structure Plan

2.1 Structure Plan

The Structure Plan outlines the key elements of the South Core Secondary Municipal Plan. It defines the major land use areas, identifies the street network, highlights key intersections, and shows where trails and open spaces are or could be located.

Land Use Areas

The Structure Plan divides the South Core into three main land use areas:

- **Neighbourhood:** Established residential blocks with a mix of building types and lot patterns.
- **Corridor:** Areas along key arterial and collector streets that support a mix of land uses and greater development intensity.
- **Mid-Town:** Larger land parcels generally located near the former rail corridor, offering potential for redevelopment and mixed-use activity.

Street Network and Connections

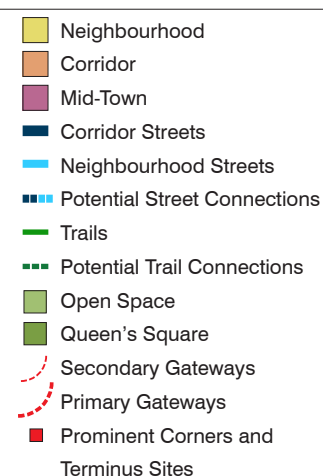
All streets in the South Core are classified as either Corridor Streets or Neighbourhood Streets, based on their function and surrounding context. The Plan also identifies potential new street connections to improve access and support future development.

Gateways and Intersections

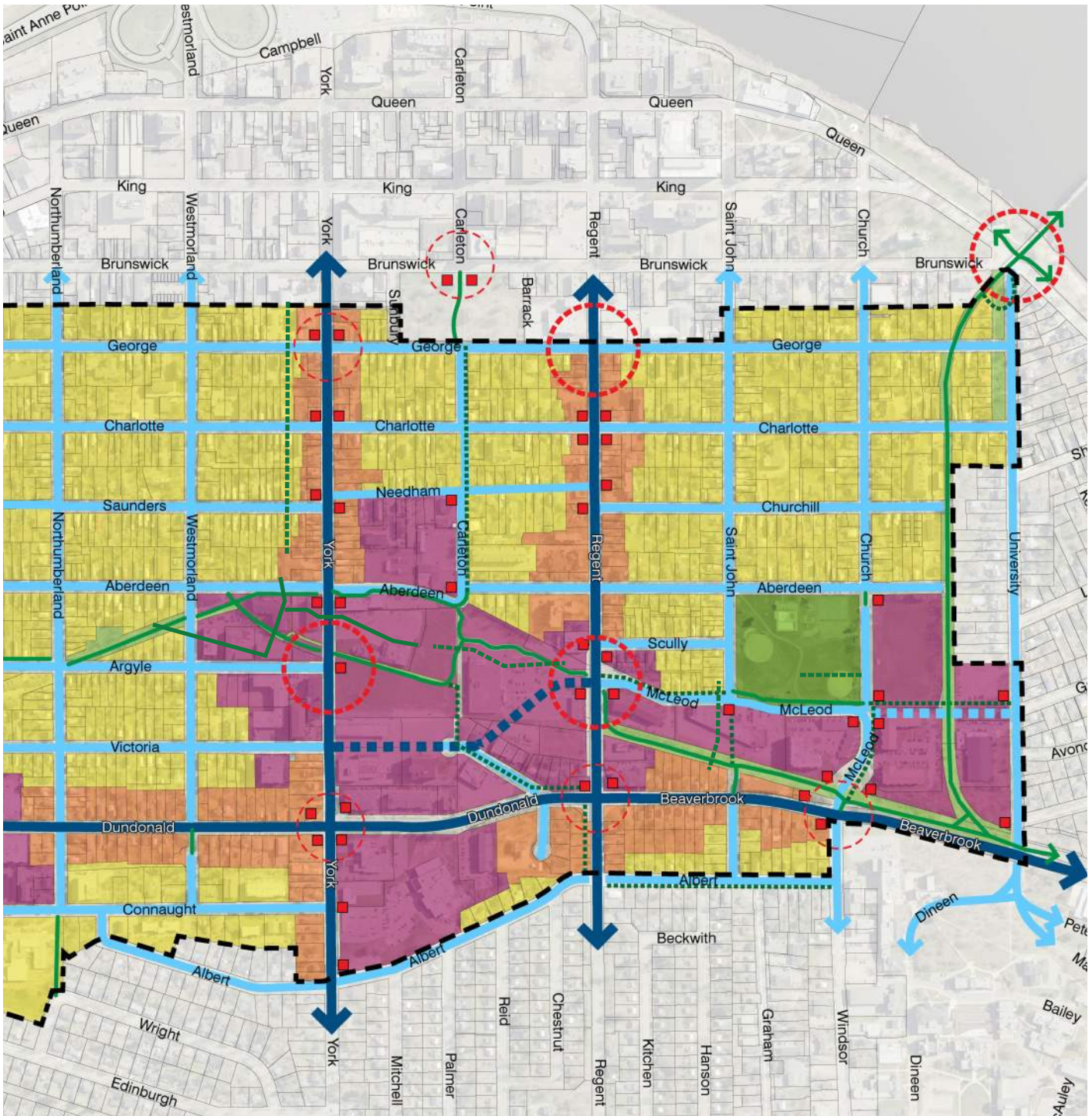
Key intersections are identified as Gateways, which include both primary and secondary nodes. These are locations where urban design and placemaking are prioritized to support a strong sense of arrival and local identity.

Trails and Open Space Network

The Plan includes existing and potential trail connections, such as the Cross Town Trail, and highlights prominent green spaces like Queen's Square Park. These features help strengthen pedestrian and cycling routes while supporting community gathering and recreation.



Structure Plan for the South Core

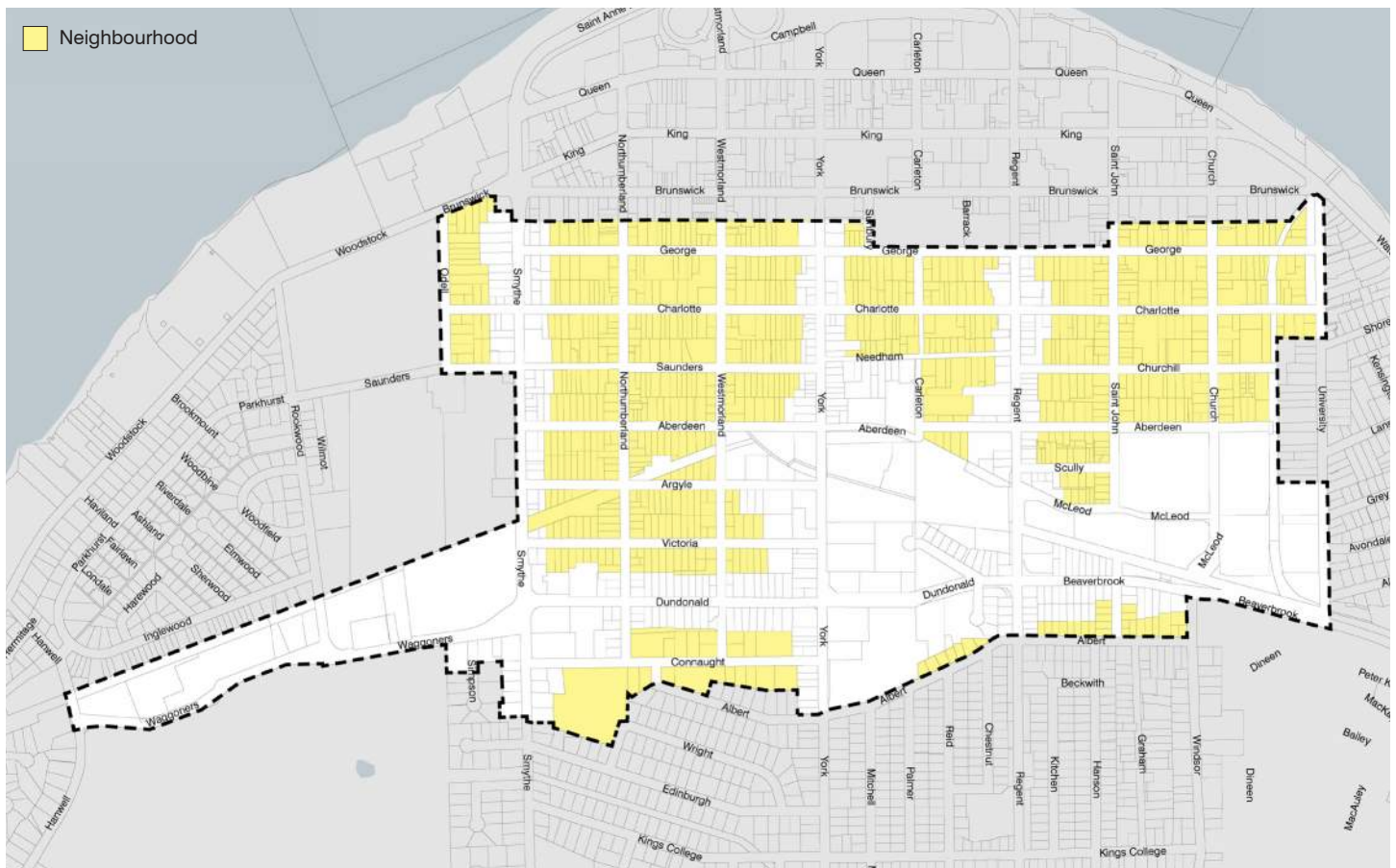


2.2 Neighbourhood

The Neighbourhood Areas are made up of the established residential blocks within the South Core, framed by the surrounding Corridor Streets. These areas have evolved over time, beginning in the 19th century and continuing through the 20th, resulting in a diverse and layered urban character.

Key Features

- **Street Pattern:** A generally consistent (though slightly irregular) grid layout with narrow streets that support walkability.
- **Lot Configuration:** Long, narrow lots typically oriented to face east–west streets.
- **Building Types:** Primarily detached houses, with modest variation. Many have been converted into multi-unit dwellings over time.
- **Architectural Styles:** A variety of traditional styles including Classical Revival, Queen Anne Revival, American Foursquare, Second Empire, Craftsman, and other vernacular styles.
- **Queen’s Square District:** The area around George and Church Streets contains a notable concentration of grand homes from the late 19th and early 20th centuries.



Map of Neighbourhood Areas

Evolving Residential Patterns

- **20th Century Development:** Smaller-scale, post-war housing is concentrated further east and south, particularly near Connaught Street, and differs from the older housing stock in style and lot pattern.
- **Infill Housing:** Walk-up apartments and townhouses have been added throughout the neighbourhood, reflecting changing housing needs and architectural trends such as Modernist and Post-Modern design.
- **Conversions:** Many original single-family homes have been converted into multi-unit buildings often referred to locally as *accordion houses* or *telescope houses*.

Green Character

- The neighbourhood retains a mostly intact **mature tree canopy**, supported by tree-lined boulevards and rear-yard vegetation that soften the urban environment.
- Due to small front setbacks, **front yard trees** are less common but appear in several locations, contributing to street-level greenery.



Typical building in the Neighbourhood Area

2.3 Corridor

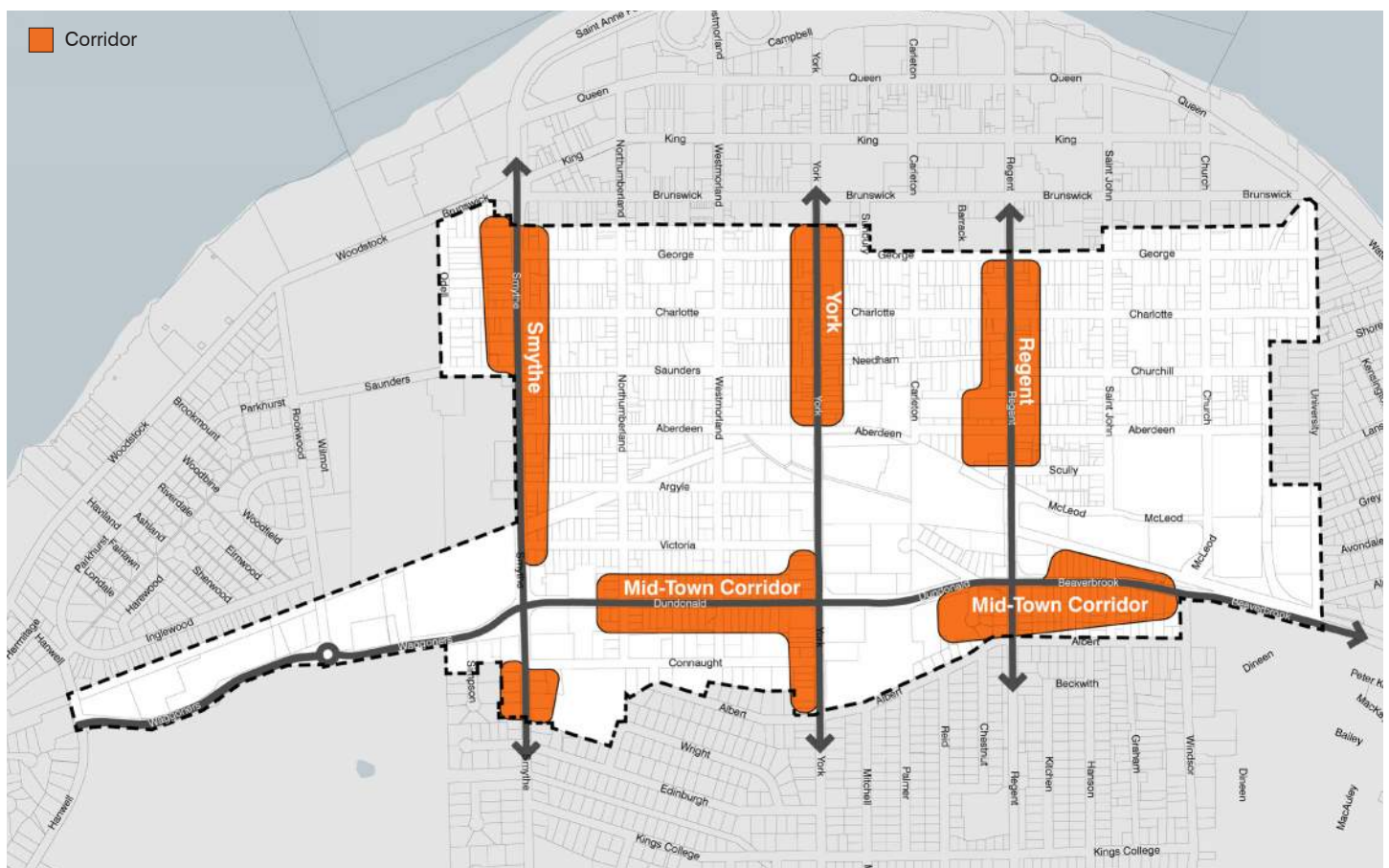
The South Core includes three major Corridor Streets: Smythe Street, York Street, and Regent Street. It also includes part of the Mid-Town Corridor, which consists of Waggoners Lane, Dundonald Street, and Beaverbrook Street. These corridors connect the South Core with the rest of the city, including the City Centre. Each corridor has a distinct built form, but all are more intensely used than the Neighbourhood Areas and support a greater mix of land uses.

Key Features

- **Street Design:** Corridor Streets are generally wider and are designed primarily for car traffic.
- **Lot Pattern:** Shallow building lots of various sizes typically front the north-south streets.



Typical existing conditions in the Corridor Areas

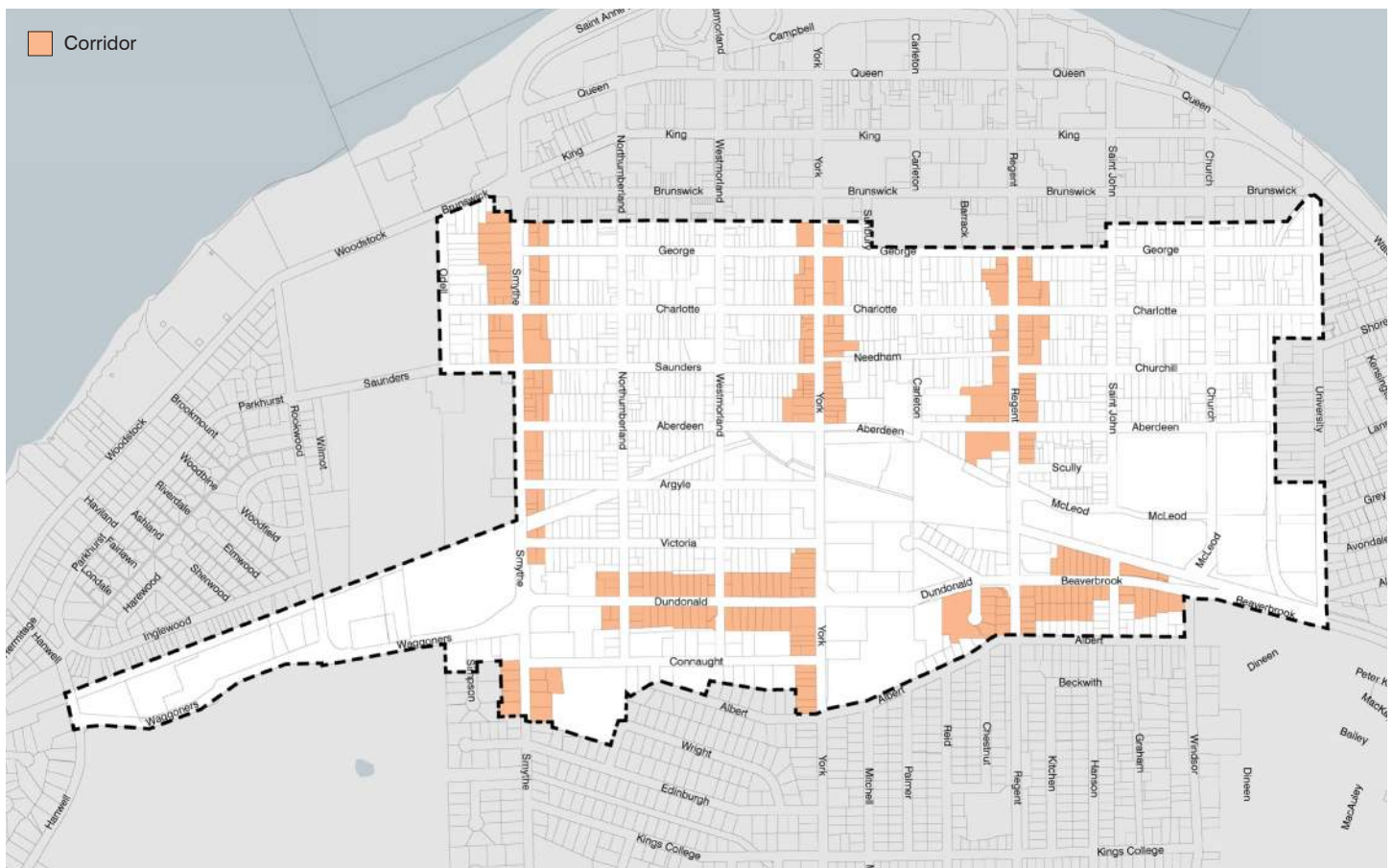


Corridor identification map

- **Building Types:** The Corridors include a range of building types, including detached houses, infill housing, and civic and institutional buildings.
- **Architectural Styles:** A mix of traditional and contemporary architectural styles appears throughout the Corridor Areas.
- **Green Character:** The tree canopy is less regular along Smythe, York, and Regent Street, and along the Mid-Town Corridor, due to the higher intensity of land use.



Smythe Street, adjacent to the NBEX site



Map of Corridor Areas

2.4 Mid-Town

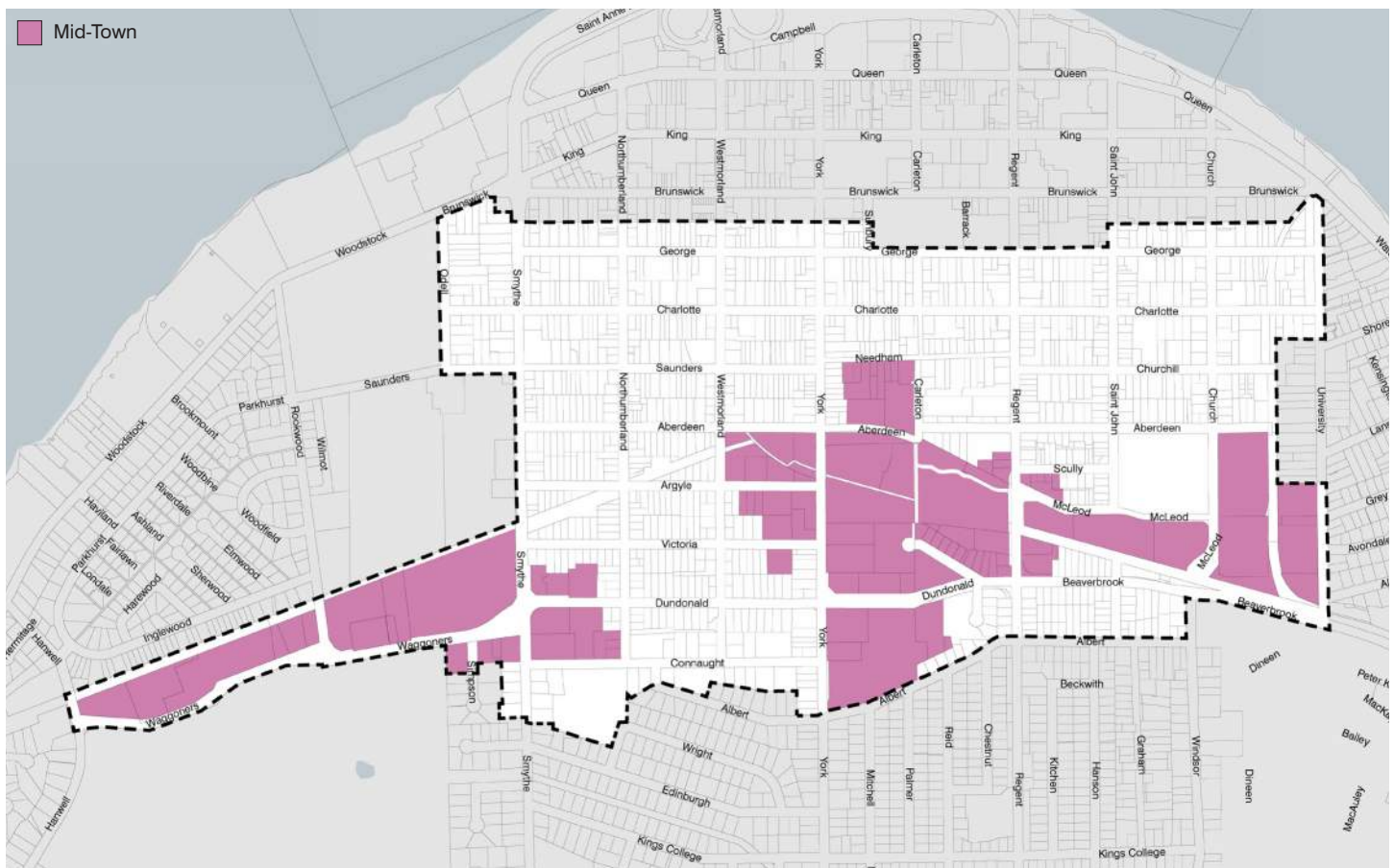
The Mid-Town Area includes the largest parcels of land in the South Core, generally located along the former rail corridor, where the Cross Town, Lincoln, and Valley Trails also run. The area's defining feature is the remnant railway and industrial landscape, which continues to shape both land use and built form.

Key Features

- **Street Design:** A wider and more varied street network than in the Neighbourhood Areas.
- **Lot Pattern:** Large lots and building footprints, an uninterrupted grid pattern in some places, and sizable development parcels.
- **Building Types:** A wide mix of industrial buildings, low- and mid-rise apartments, and commercial structures. The area also contains unbuilt spaces such as surface parking lots and brownfield sites.
- **Architectural Styles:** A blend of vernacular and contemporary architectural styles.
- **Trail Network:** Several trails follow the former railway lines, enhancing pedestrian and cycling connectivity.
- **Industrial Legacy:** Several brick-and-beam industrial buildings remain near the train station, including the landmark Hartt Boot and Shoe Factory, a three-storey structure with a central tower, built circa 1898 on the west side of York Street.



Redevelopment example in the Mid-Town Area



Map of Mid-Town Areas

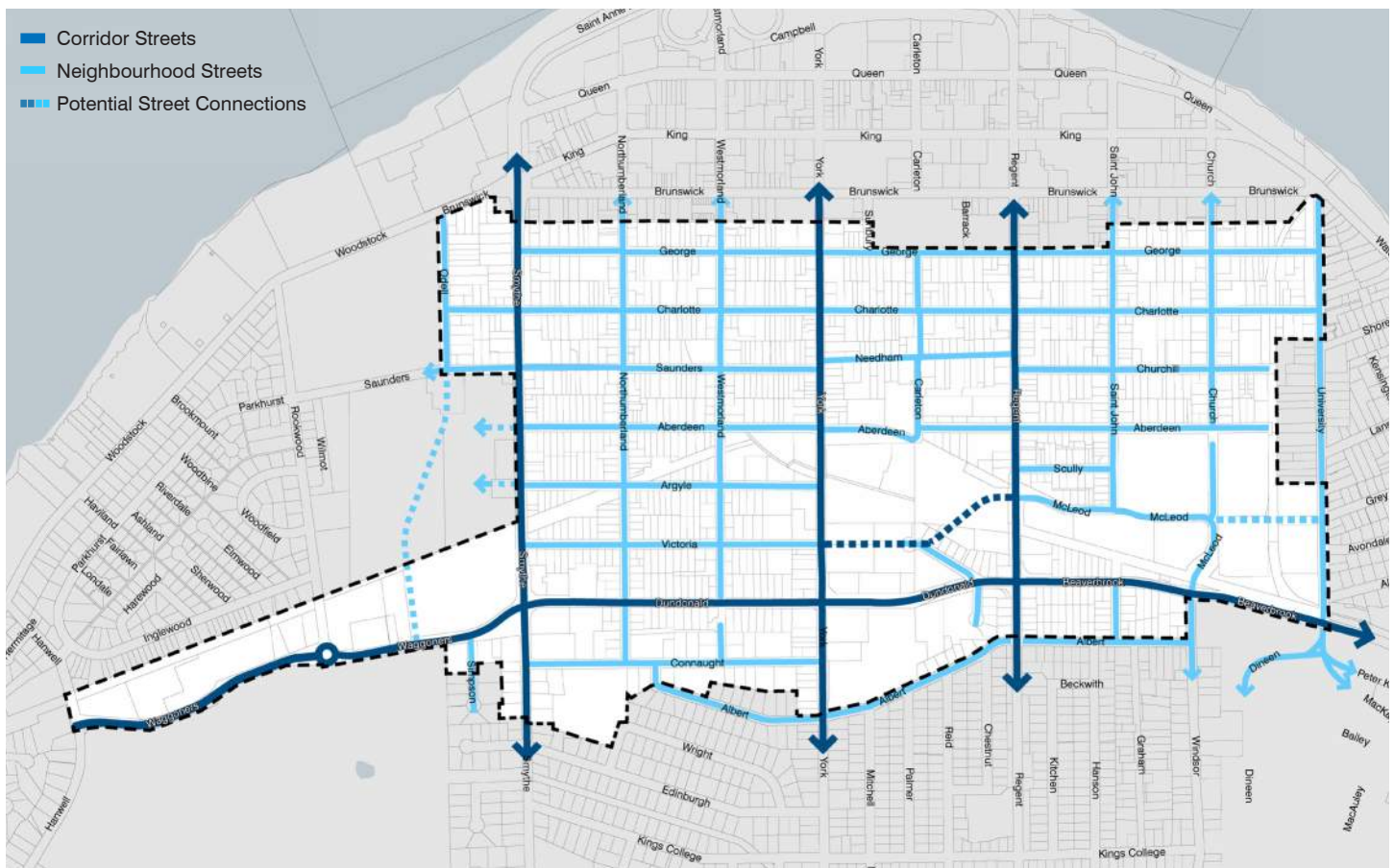
2.5 Streets

Although the automobile is still dominant in the South Core, there is growing appreciation for the many benefits of a balanced mobility system. These benefits include public health, socio-economic access, environmental health, equity, and accessibility.

A balanced approach to the planning, design, and maintenance of streets and trails considers all travel modes and prioritizes safety and comfort for a diverse range of users. These include motorists, motorcyclists, cyclists, pedestrians, individuals with disabilities, transit and school bus users, and emergency responders. The goal is to encourage active transportation, reduce demand on roadways, and increase physical activity as a replacement for some motor vehicle trips.

Key Features

- **Street Rights-of-Way:** A range of street rights-of-way, with a range of boulevard widths.
- **Tree Planting:** Tree planting is inconsistent throughout the South Core.
- **Utility Corridors:** Utility corridors, particularly NB Power infrastructure, limit the ability to plant large canopy street trees.



Map of Corridor and Neighbourhood Streets and potential street connections

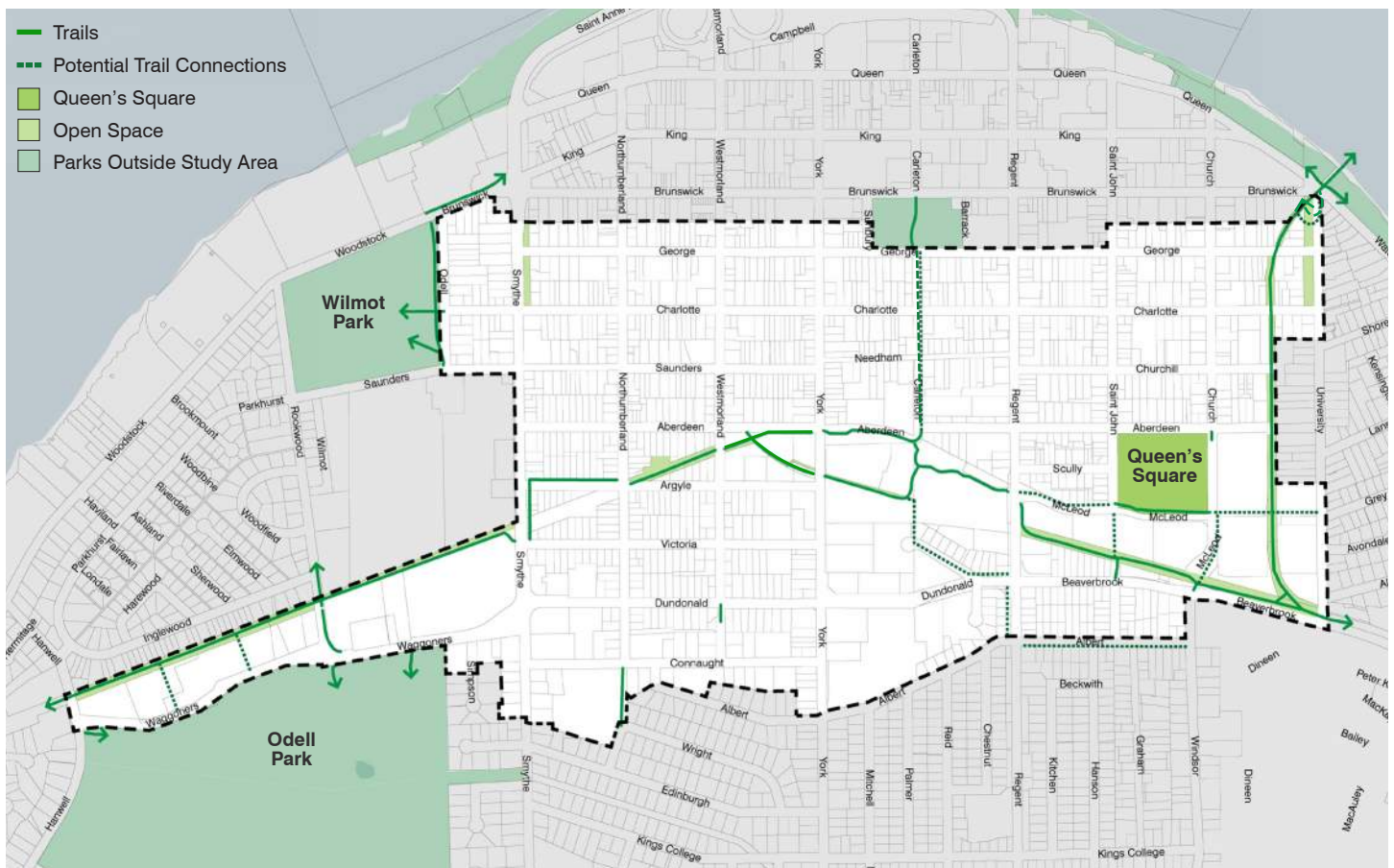
2.6 Urban Open Space

The development of the South Core as a vibrant, healthy, and compact urban community is supported by a hierarchy of urban open spaces, anchored by Queen's Square, which functions as the critical active neighbourhood park. Urban open spaces, unlike traditional suburban parks, serve a passive recreational and social function in denser urban areas.

It should be noted that the large park areas at Wilmot, Odell, and Riverfront Green are important amenities accessible to South Core residents, even though they are beyond the Plan Area.

Key Features

- **Queen's Square:** Queen's Square will remain the primary active park space in the South Core.
- **Size Range:** Urban open spaces are generally smaller and vary from 0.10 to 0.80 hectares in size.
- **Quality of Space:** Quality space is prioritized over quantity of space.
- **Context and Surroundings:** Surrounded by different uses and building forms.
- **Amenities:** Contain a concentration of amenities to appeal to diverse users.
- **Ownership:** May be public or privately owned.



Map of urban open space areas



Queen's Square Park aerial photograph

2.7 Gateways, Prominent Corners and Terminus Sites

Gateways, prominent corners, and terminus sites are important features that provide opportunities for architectural and urban design interventions. They help establish a sense of arrival, improve visibility, and support neighbourhood identity and orientation.

Key Features

- **Gateways:** The Structure Plan identifies two types of gateways based on their location and role in the street network. Primary Gateways are located at significant intersections on each of Smythe, York, and Regent Street and at the threshold with the City Centre (Victoria Circle, Regent Street, and University Avenue). Secondary Gateways are proposed for less significant intersections that act as thresholds into the South Core.
- **Prominent Corners:** These occur at intersections of key streets and sites where buildings frame views, mark transitions, and contribute to a sense of arrival.
- **Terminus Sites:** Notable terminus locations, including the Brunswick Street terminus — which is visible from the South Core—function as visual thresholds into the South Core.



Map of gateways, prominent corners, and terminus sites

- **Design Opportunity:** Gateway locations can be enhanced through signage, landscaping, and building placement to reinforce visibility and identity.
- **Placemaking Potential:** Buildings, public art, signage, and other visual elements at corners and terminus sites should contribute to orientation, signal transitions, and draw people through the neighbourhood.



Primary gateway at George Street Middle School

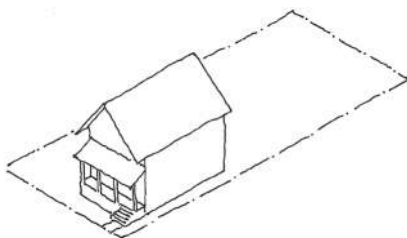
1 Neighbourhood

1.1 Building Typologies

Building typologies permitted in the Neighbourhood Areas include low-rise residential buildings up to 4 storeys in height. Generally, access to individual units occurs directly from ground level, except in the case of low-rise small apartment buildings or walk-up multiplexes.

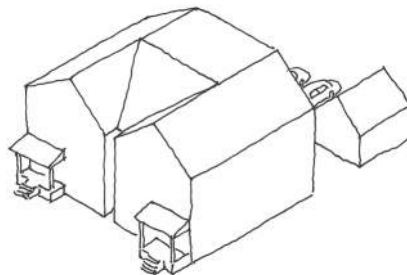
Single Detached

House form buildings with one residential dwelling unit.



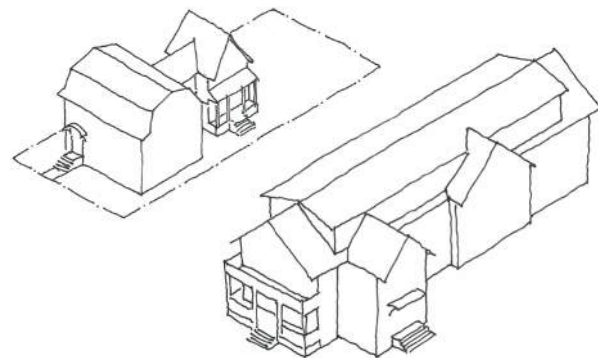
Semi Detached/Duplex

House form buildings with two residential dwelling units either side-by-side or stacked.



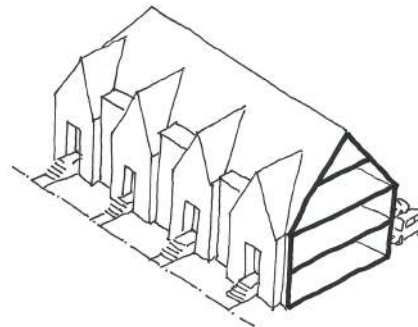
“Accordion House”

House form buildings with one or more additional residential dwelling units attached towards the rear of the original house location.



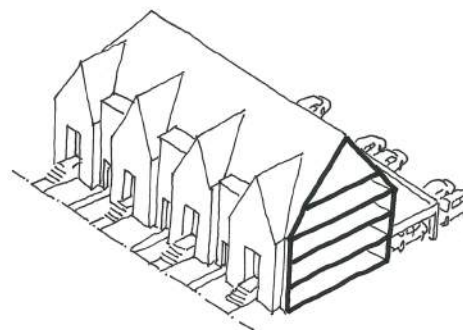
Townhouse

Multiple residential dwelling units attached to each other at their sides, with individual ground-level entrances.



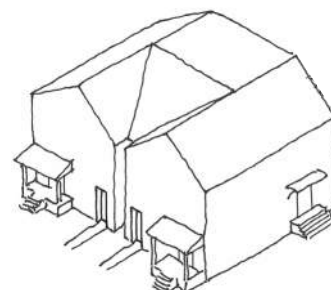
Stacked Townhouse

Multiple residential dwelling units attached to each other at their sides and above (dwelling units are stacked on top of each other), with individual ground-level entrances.



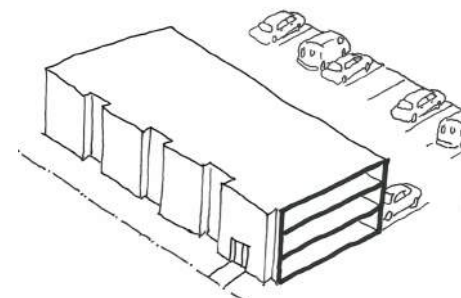
Multiplex

Multiple residential dwellings attached to each other at their sides and above. Not all dwelling units may have ground-level entrances.



Low-Rise Apartment

A building with multiple dwelling units accessed from a centralized entrance and corridors. This typology is limited to Connaught Street and Albert Street, as defined in the Secondary Municipal Plan.





St. John Street carriage house

1.2 Heritage

The Neighbourhood Areas contain a number of designated heritage buildings, as well as other buildings with significant or contributing heritage value that together form the character of the South Core. It is important that new development adjacent to heritage buildings and renovations to heritage buildings maintain and enhance the defining characteristics of the heritage buildings.

Guidelines

- a) Preserve, renovate or adaptively reuse heritage buildings wherever possible.
- b) New construction should preserve the spatial relationships that characterize a property or the immediate surrounding context.
- c) Original, historic, building materials and details should be retained whenever possible, and those that had been previously covered or removed should be uncovered, refurbished or recreated. Historic material should never be covered with modern materials, and unpainted brick should not be painted.
- d) New construction should achieve compatibility through harmonious scale, massing, façade articulation, and materiality.
- e) For additions or renovations to an existing building, incorporate high-quality materials and colours that are consistent with and complement the existing building.
- f) Historical styles of architecture used on new construction should be relevant to the South Core's Neighbourhoods and designed in accordance with the established orders and details of the historic architectural styles in the South Core. Designs that borrow and mix historic stylistic detailing inappropriately or incorrectly should be avoided. Complementary contemporary designs and styles are encouraged.



Charlotte Street Arts Centre



Historic style of architecture

- g) Where new construction or renovation is taller than adjacent buildings, consider incorporating the upper level within the roof structure, or stepping back the upper level adjacent to the lower building(s).
- h) Design rooflines to complement the architectural expression, taking cues from existing buildings on the streetscape. Hipped and steep pitch roofs are preferred for traditional styles, and flat and lower pitches for more contemporary styles.
- i) Pitched roofs should include overhangs sized to provide shade during summer while still allowing sun penetration during winter.
- j) Use dormers and gables on pitched roofs to enhance roof and building designs.
- k) Provide entry features such as porches that are generally consistent with those of adjacent buildings in terms of their design and overall height, depth, and relationship to the street.
- l) Window style and design should be consistent with the building's architectural style and take cues from the surrounding context in terms of size, proportion, and placement.
- m) Window mountings should be part of the window structure and not applied as a decorative element.



Use of dormers and gables



Multiple entry features

1.3 Height and Massing

Building height and massing impacts the character and quality of the street experience and relationships to neighbouring uses. In the Neighbourhoods, it is important that new development is compatible with and enhances the existing character.

Guidelines

- a) New buildings should consider and respect the scale and massing of adjacent residential buildings, providing setbacks as appropriate to prevent adverse impacts on neighbours.
- b) Divide up larger building masses through architectural articulation, varying setbacks and roof lines, emphasizing vertical orientation.
- c) Maximum building height in the Neighbourhood Areas is 4 storeys, with most areas limited to 3 storeys.



New building to match existing scale and massing



New building to match existing scale and massing

1.4 Relationship to Street

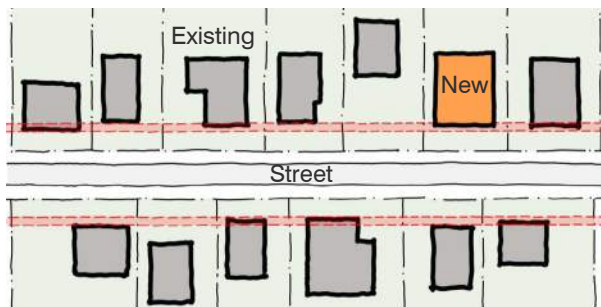
While the location of buildings along the street varies, front yard setbacks are generally similar along each block. A consistent front setback is desired to support a cohesive streetscape.

Guidelines

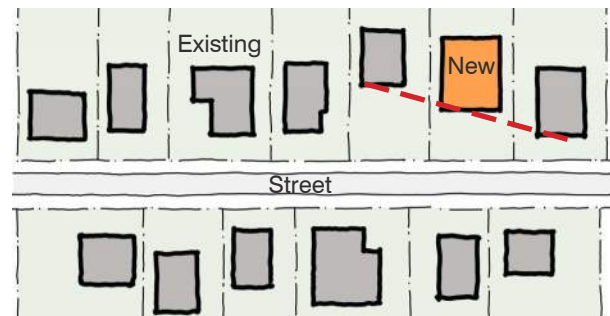
- Place new buildings to fit with the predominant block setback or use the setback average from adjacent properties.
- Where pedestrian mid-block connections or public open spaces are present, consider increasing side yard setbacks to enhance access and visibility.
- Porches, stairs, canopies, and other entrance features may encroach into the front yard setbacks, provided they do not obstruct sightlines.



Consistent building setback



New development in Neighbourhood Areas should be sited at the predominant setback along the streetscape



New development in Neighbourhood Areas should be sited at the average of setbacks on adjacent properties

1.5 Secondary Dwelling Units

Secondary dwelling units are attached or detached additional residential units subordinate to the main residential dwelling unit on a lot. These could take the form of an accessory apartment, basement apartment, garage apartment, or garden apartment.

Guidelines

- a) Locate these units towards the rear of the lot or set back a minimum of two metres from the front wall of the primary residential building.
- b) Position units to minimize impacts to the dripline of existing trees and to preserve or create areas of soft landscaping and permeability in the rear of the lot.
- c) Ensure units complement the architecture and materials of the primary residential building.
- d) Provide separate, well-lit entrances that are clearly visible and accessible from the street wherever possible.
- e) Consider locating combined mailboxes at the main street-facing entrance.
- f) Where entrances are not on the front façade, include signage to clearly indicate their location.



Secondary dwelling unit entrance setback



"Accordion House" typology

1.6 Building Elements

Building elements such as the façade articulation, materials, entrances, porches, and roofs together determine the overall character of a building.

Guidelines

Façade Articulation

The articulation of a building's façade contributes to human scale, a sense of animation, and helps break up larger building surfaces. This reduces the apparent scale of the building. Articulation refers to the placement and relationships of materials, windows, doors, and other architectural elements.

- a) Articulate the main façade (elevation) of new buildings or additions to consider the patterns and rhythms of vertical and horizontal elements found in neighbouring buildings.
- b) Use vertical and horizontal recesses or projections, step backs or changes of plane, and variations of texture, colour, materials, and detailing to establish a clearly defined façade composition.
- c) Where buildings face two streets, both façades should have the same high level of design with architectural elements that respond to the corner.



Façade articulation examples

Materials

The selection of building materials plays an important role in creating a design that is compatible with the Neighbourhood character of the South Core.

- d) Choose materials for both functional and aesthetic quality. Exterior finishes should exhibit quality workmanship, longevity, sustainability, and ease of maintenance. Building materials recommended for new construction include brick, stone, wood, concrete, and glass.
- e) Recommended materials for new construction include brick, stone, wood, concrete, and glass.
- f) Materials should be resilient to changing climate conditions.
- g) Avoid using products that imitate authentic materials.
- h) For the front façade, use one to two primary materials, and up to two accent materials.
- i) Avoid changing materials at building corners. Wrap primary materials around for at least 1.2 metres from the front façade.
- j) Vinyl siding and vinyl windows are discouraged where they face a public street or open space.
- k) Avoid using plastic, plywood, concrete block, or glass that is either tinted or mirrored.



Historic character appearance and entry feature



New building using complementary façade materials

Entrances and Porches

Entrances and porches are often the most visible and frequently used parts of a building façade. They are key reference points when approaching a building.

- l) The main entrance should face the street. The front door should be prominent, clearly visible, and easily accessed from the street.
- m) Highlight front entrances through design features such as porches, verandas, arches, generous overhangs, or upper-storey elements like cantilevers or recesses.
- n) Porches and verandas should be usable spaces, with a minimum depth of 1.5 metres. They must not extend into the public right-of-way.
- o) The design and placement of front entry features should reflect the character of the surroundings and maintain general consistency.



Porch feature for a multi-unit building



Historic character entry feature

1.7 Parking, Driveways, and Garages

Parking is functionally necessary in Neighbourhood Areas, but it should not come at the expense of landscaping or the existing green areas on private lots. It should also not dominate or define the character of the area.

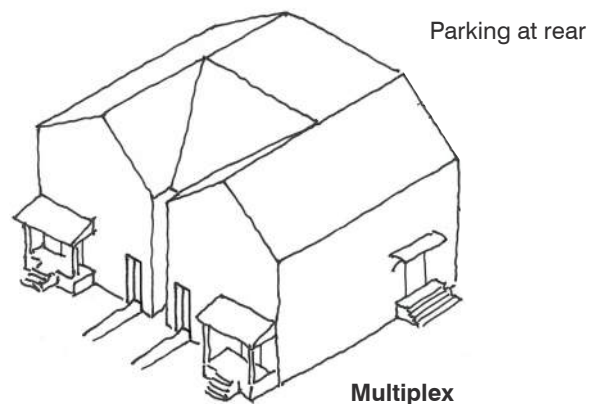
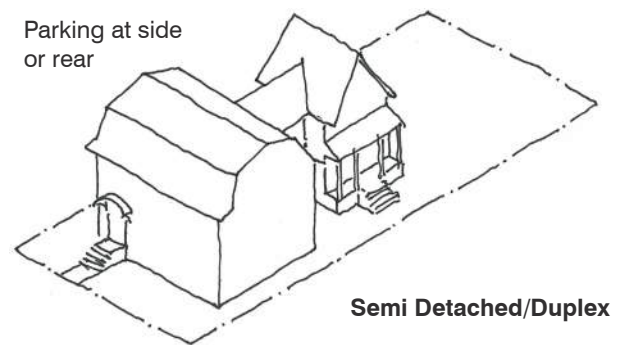
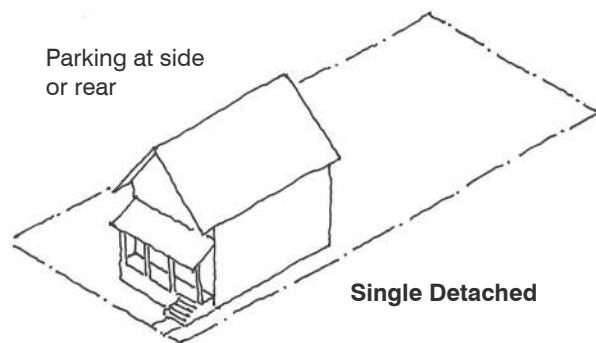
Guidelines

Private Lot Parking

- a) Front yard parking should generally be discouraged. On-site parking should be located on the property's side or rear yard whenever possible.
- b) Surface parking areas for multi-residential building forms should include pavement markings for stalls, pedestrian-scaled lighting, and be screened from public view with low fencing, architectural features, or landscaping.
- c) Walkways should be visually distinguished from vehicular areas through a change in material or by using a planted or sodded edge.

On-Street Parking

- d) Vehicles should not be parked in areas of the City right-of-way, except where on-street parking is permitted.
- e) On-street parking may be used for visitor parking spaces, where permitted and subject to restriction.
- f) Additional parking for accessory units may be accommodated with overnight on-street parking, where permitted and subject to restriction.



Driveways

- g) Minimize driveway width to match the interior width of the garage.
- h) Separate driveways from the side lot lines by a landscaped strip that is a minimum of 0.5 metres wide, where possible.
- i) Driveways should have a separation from the building wall.
- j) Provide walkways and/or landscaping between driveways and building walls.
- k) Locate driveways as far as possible from parks, open space features, public walkways, schools, and intersections.

Garages

- l) Front garages are discouraged in the Neighbourhoods. Place garages along the flanking side yard where rear yard placement is not possible.
- m) Ensure both attached and detached garages are a natural extension of the design, massing, and materials of the main building.
- n) Detached garages should only be permitted in the rear and interior side yards.
- o) Garage doors facing a public street should be set back a minimum of 6 metres from the front property line to allow a car to sit in front of the garage on private property.
- p) Driveways and/or garage doors should not dominate the front façade of the primary building or the view from the street.
- q) Recess front garages 0.5 metres from the main wall of the building to reduce visual impact of garage doors on the streetscape.
- r) A maximum of one garage door may be permitted to face the street, with a maximum width of 40 percent of the building's width.
- s) If a second storey is built over the garage, it must be set back a maximum of two metres.

1.8 Landscaped Area

The maintenance and enhancement of the network of rear-yard trees and vegetation on private lots is one of the primary objectives for the Neighbourhoods. These areas, collectively referred to as the Sponge Area, are characterized by large mature trees, a variety of shrub plantings, grass areas, and in some places, smaller structures such as sheds, garages, and secondary dwelling units. These spaces help define the character of the residential neighbourhood while also providing benefits such as stormwater management and permeability, shade and cooling, and natural corridors or habitat for birds and wildlife.

Guidelines

- a) New buildings, additions, and secondary dwelling units should preserve and protect existing healthy and mature trees, by preserving existing soft landscaped areas within the dripline of existing trees, including throughout construction.
- b) Preserve and protect areas of soft landscaping and permeability in the rear of the lot. Replace built over or paved soft landscaping with equivalent permeable areas elsewhere on the lot.
- c) Minimize the use of hard, paved areas to reduce surface runoff and heat island effect.
- d) Use native, resilient, and drought-tolerant plants. Encourage naturalized plantings.
- e) Avoid planting invasive and non-native species.
- f) Minimize manicured lawn/turf grass and use drought-tolerant varieties, where possible. Consider using alternative low-maintenance groundcovers or wildflower meadows.



Demonstration illustrating secondary dwelling units located to preserve the permeable "Sponge Area" at the rear of lots



Soft landscaped areas in the "Sponge Area"



Example of a Sponge Area in the South Core

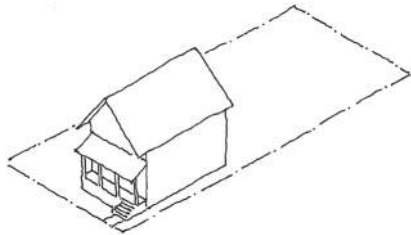
B 2 Corridor

2.1 Corridor Overview

The Corridor Areas allow a range of building typologies, including mixed-use mid-rise buildings from 2 to 7 storeys in height, with active uses at grade facing the Corridor Streets. Residential uses with ground-level access are encouraged along Neighbourhood Streets and may be permitted along Corridor Streets.

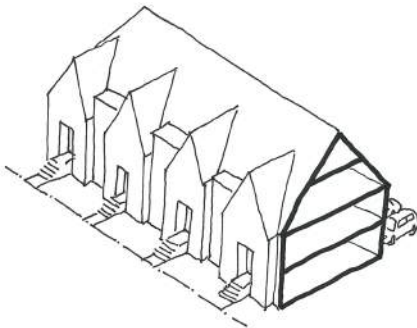
Single Detached

House form buildings with one residential dwelling unit (see **Section B1** for applicable guidance).



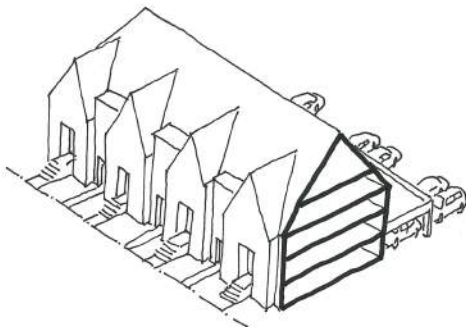
Townhouse

Multiple residential dwelling units attached to each other at their sides, with individual ground-level entrances.



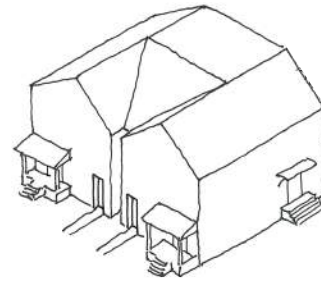
Stacked Townhouse

Multiple residential dwelling units attached to each other at their sides and above (dwelling units are stacked on top of each other), with individual ground-level entrances.



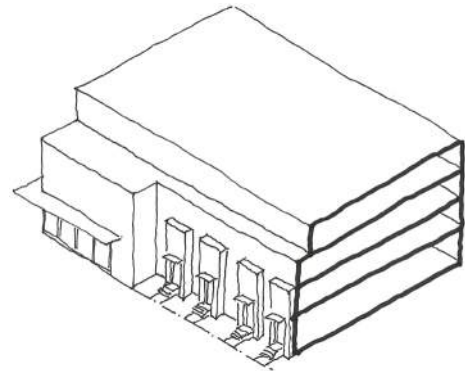
Multiplex

Multiple residential dwelling units attached to each other at their sides and above. Not all dwellings may have ground-level entrances.



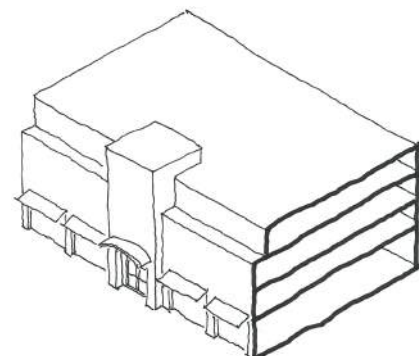
Mid-Rise Residential Building

A building with multiple dwelling units accessed from a centralized entrance and corridors. Some dwelling units may have ground-level entrances.



Mid-Rise Mixed-Use Building

A building with multiple dwelling units accessed from a centralized entrance and corridors, and with active uses at grade.

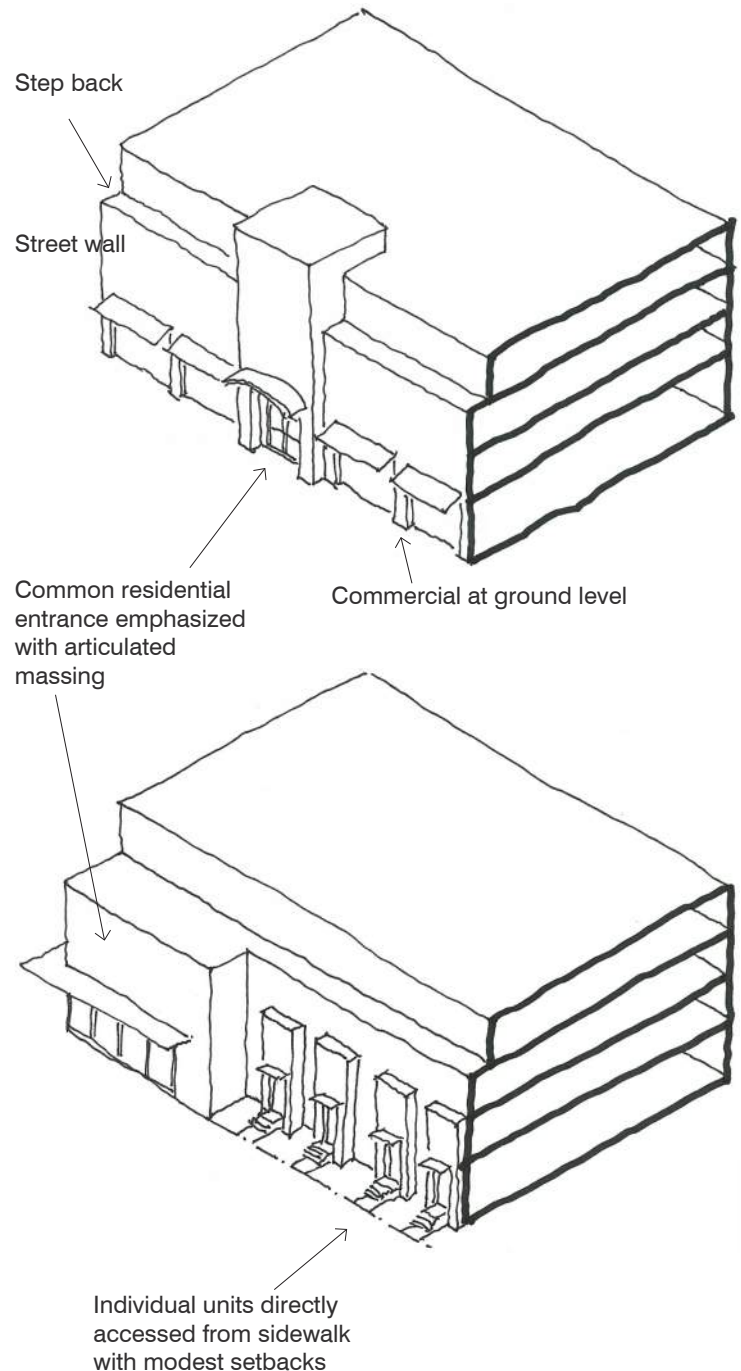


2.2 Height and Massing

Building height and massing shape the character and quality of the street experience and influence relationships with neighbouring uses. In the Corridor Areas, buildings along the street edge help define the public realm. However, reducing the visual mass of large, single buildings is also important.

Guidelines

- a) Building typologies anticipated include townhouses, stacked townhouses, and mid-rise buildings.
- b) New buildings should consider and respect the scale and massing of adjacent residential buildings, providing setbacks, step backs, and building height transitions, as appropriate to prevent adverse impacts on neighbours.
- c) Divide up larger building masses through architectural articulation, varying setbacks and roof lines.
- d) Minimum ground floor height should be 4.5 metres for mixed-use buildings, to permit flexibility and the long-term adaptability of the ground level uses.
- e) On buildings at corner sites and in other visually prominent locations, consider modest exceptions to setbacks, step backs, and height to allow for enhanced architectural expression.

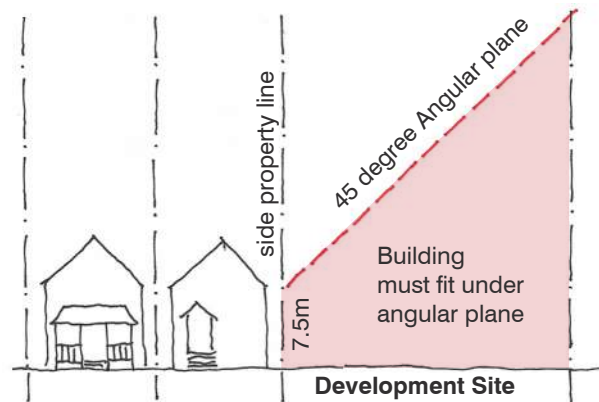
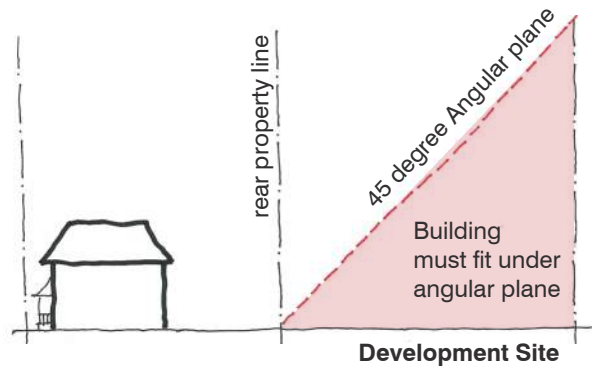


2.3 Transition to Neighbourhoods

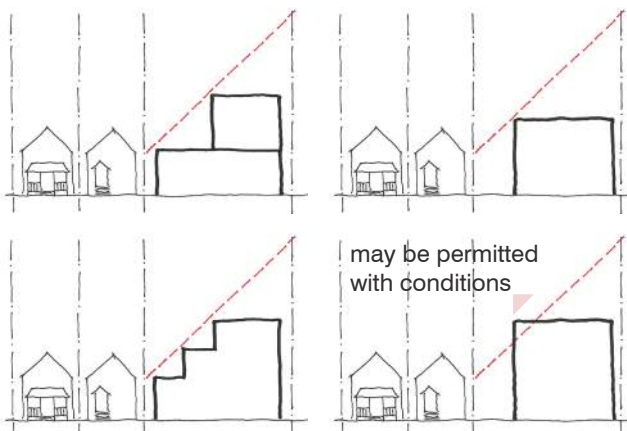
Appropriate transitions are essential between mid-rise buildings in the Corridors and the adjacent low-rise buildings in the Neighbourhood Areas. Applying an angular plane protects adjacent buildings from abrupt and looming transitions to taller buildings, while also ensuring sunlight penetration and privacy for buildings and yards.

Guidelines

- Buildings in the Corridor Areas abutting the rear property line of a Neighbourhood Area must be built at or below a 45 degree angular plane, originating from the rear property line of the Neighbourhood property.
- Buildings in the Corridor Areas abutting the side property line of a Neighbourhood Area must be built at or below a 45 degree angular plane, originating 7.5 metres above grade at the property line.
- Where multiple Neighbourhood properties with inconsistent rear property lines abut a Corridor Area property, the angular plane originates from the average property line.
- Angular planes are not intended to dictate a particular built form response such as “tiers”.
- The top storey of a building or other building elements may be permitted to pierce the angular plane if the City is satisfied there will be no adverse impact on adjacent Neighbourhood Area properties.



Angular planes



All of these buildings conform to the angular plane



Building stepping down to transition to adjacent low-rise housing

2.4 Street Relationship

The orientation and placement of buildings in a consistent line along the street helps to clearly define the public realm and enhance the pedestrian environment by providing visual animation and a sense of enclosure. Fundamental to creating a strong street wall in the Corridor Area is locating buildings at (or close to) the front property line. Above the street wall, a step back defines the top of the street wall.

Guidelines

- a) Orient and address buildings to the street with clearly defined entry points that directly access the sidewalk.
- b) Place buildings at or close to the street edge, with minimal setbacks.
- c) Create a continuous street wall by extending buildings along the full primary property frontage. Side yard setbacks and gaps are generally discouraged, except where required for mid-block pedestrian connections or vehicular access.
- d) On corner sites consider providing greater setbacks on the secondary frontage, using the space to benefit the public realm with wider sidewalks, landscaping, patio, or market space for retail uses.
- e) The street wall should be 2 to 4 storeys in height, with active uses and entrances on the ground floor that connect to the sidewalk.
- f) Above the street wall, taller portions of the building should step back a minimum of 2 metres.
- g) Site buildings to define the edges of public open spaces such as plazas, parks, or squares.



Diagram illustrating street wall height and step back



Mid-rise building with a 3-storey street wall

2.5 Heritage Buildings

The Corridor Areas contain a number of designated and listed heritage buildings, as well as other buildings with heritage value that contribute to the character of the South Core. It is important that new development adjacent to heritage buildings, and renovations to heritage buildings, maintain and enhance the defining characteristics of these structures.

Guidelines

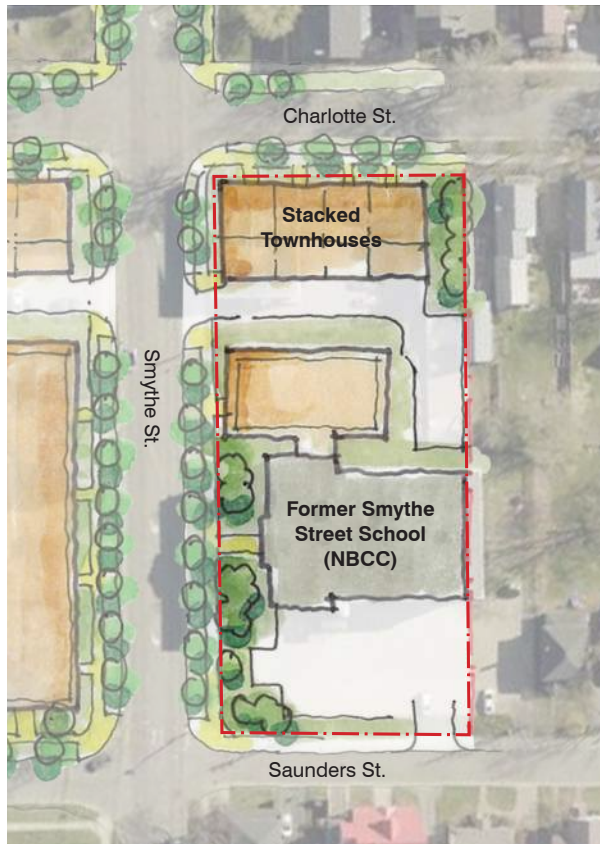
- a) Preserve, renovate, or adaptively reuse heritage buildings wherever possible.
- b) Where heritage buildings cannot be preserved, renovated, or adaptively reused as a whole, integrate the heritage façades or other significant building elements or details into new development. Preserved façades should generally remain in their original locations and appear to be integrated with the new construction in a manner that maintains the building's historical continuity.
- c) Original, historic building materials and details should be retained wherever possible, and those that had been previously covered or removed should be uncovered, refurbished, or restored. Historic material should never be covered with modern materials, and unpainted brick should remain unpainted.
- d) Design new buildings to be compatible with adjacent heritage buildings in massing, setbacks, and materials.
- e) New construction should be visibly differentiated from the old, achieving compatibility primarily through harmonious scale, massing, façade articulation, and material use.
- f) Set back all or part of new buildings adjacent to a heritage building to create a sense of separation for the heritage building and to highlight important features such as towers, roofing, or other significant heritage elements.
- g) Provide additional setbacks for new buildings adjacent to landmark heritage buildings to preserve their prominence and sightlines, including views of key landmark features such as steeples or towers.



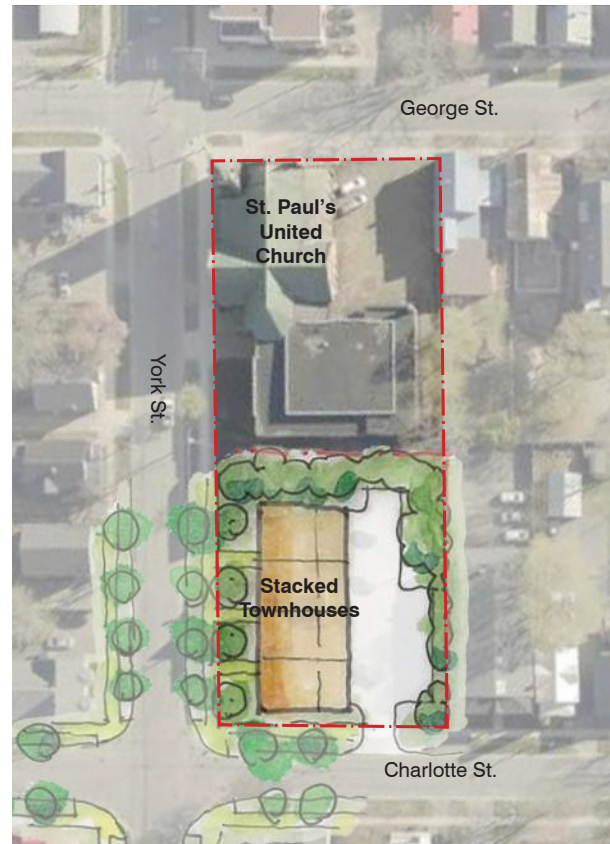
Former St. Dunstan's School on Regent Street adaptively reused and converted into apartments



Preserved façades in a large redevelopment



Demonstration of redevelopment that preserves the former Smythe Street School building



Demonstration of redevelopment that preserves the St. Paul's United Church building



Former Smythe Street School adaptively reused and converted into corporate offices



St. Paul's United Church site on York Street

2.6 Building Elements

Building elements such as the façade articulation, entrances, rooftop, and signage together determine the overall character of a building. This section has the same content as Section 3.6.

Guidelines

Façade Articulation

The articulation of a building's façade contributes to human scale, a sense of animation, and helps to break up larger building surfaces and reduce the apparent scale of the building. Articulation concerns the placement and relationships of materials, windows, doors, and other architectural elements.

- a) Use vertical and horizontal recesses or projections, step backs or changes of plane, and variations of texture, colour, materials and detailing to articulate a clearly defined organization of the building façade.
- b) Create a human-scaled and fine-grained character along the street with a rhythm of vertical elements or bays, and frequent windows and doors.
- c) Articulate both street facing façades of a corner building to the same high level of design, with architectural elements that respond to the corner.



Articulation of a façade with vertical rhythm of bays, windows, and canopies



Corner building that frames both streets with enhanced massing, roofline, material, and entrance

Entrances

Entrances are often the most recognized and used part of building façades, something that people look for when approaching a building.

- d) Entrances to buildings should address the primary street and be clearly articulated and expressed.
- e) Emphasize entrances with architectural forms and detailing such as changes in height and massing, projection, shadow, punctuation, and change in roofline.
- f) Ensure main entrances to public buildings, offices, and residential lobbies are weather protected through use of canopies, awnings, or recesses.
- g) Ensure entrance areas and transitions from inside to outside are barrier free and accessible, with continuous and relatively flat and smooth grading, and do not obstruct the installation and maintenance of building services (i.e. water, sewer, electrical, communication, etc.).



Canopy protecting major building entrance



Entrance to residential units in a mixed-use building

Rooftops and Mechanical Equipment

The design of the roofline has an impact on the character of the streetscape, especially from a distance. Both roof and roofline contribute to architectural quality and skyline views.

- h) The expression of the building top and roof should be clearly distinguished from the rest of the building through treatments such as step backs, materials, cornice lines, and overhangs.
- i) Mechanical penthouses and solar panels should be integrated with the architectural treatment of the roofline and building expression.
- j) Screen rooftop mechanical equipment with materials that are complementary to the building.
- k) Green roofs are encouraged on new buildings to provide aesthetic and sustainability benefits, as well as providing amenity space for building occupants.



Interesting rooflines and rooftop mechanical integrated into design



Green roofs, interesting rooflines, and rooftop mechanical integrated into design (image: Payton Chung, CC BY 2.0, Flickr)

Signage

Building signage plays a significant role in the character and animation of mixed-use areas.

- l) The scale of commercial signage should reinforce the pedestrian scale of the street by locating signs at or near ground level for viewing from sidewalks.
- m) Integrate signs into the organization and design of building façades by placing them within sign bands, architectural bays, friezes, etc.
- n) Signs should not obscure windows, cornices, or other architectural elements.
- o) Commercial signage should not overwhelm the building and/or storefront.
- p) Large freestanding signs such as pylons, signs on top of rooftops such as billboards, and back lit illuminated rectangular sign boxes are discouraged.
- q) Signs should be constructed of durable, high-quality materials and be well-maintained.
- r) Street addresses should be clearly visible for every building.



Address signage integrated into building design



Signage integrate into a sign band as part of the façade design

2.7 Parking, Loading, and Servicing

Parking, vehicle access, and service functions like loading and storage are functionally necessary for buildings and sites. However, their design and placement must not undermine the streetscape and pedestrian life.

Guidelines

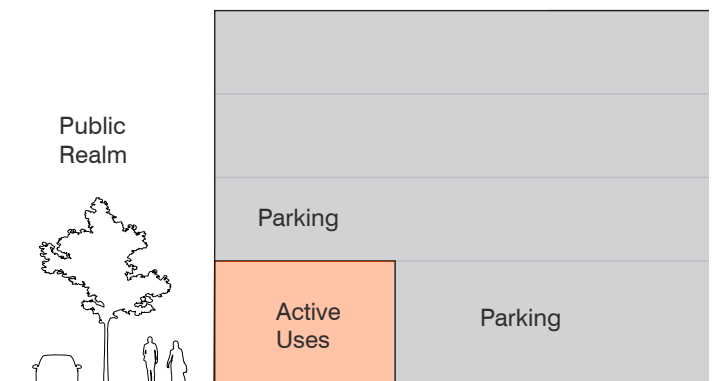
- a) Parking should not be located between buildings and the street edge. Parking is best located underground, where feasible, or to the rear of buildings in small surface lots.
- b) Break up large surface parking areas into smaller parts using trees, lighting, and walkways.
- c) Ensure surface parking is well lit at night, while considerate of adjacent residential uses.
- d) Access to parking and servicing areas should clearly prioritize pedestrian movement and preserve the continuity of the public sidewalks.
- e) Clearly demarcate walkways through parking areas with the use of paving materials, landscaping, and lighting. Where possible, provide multiple pedestrian entry/exit points to surface parking.
- f) Screen surface parking areas from adjacent public sidewalks and public spaces using materials that provide a visual buffer while still allowing clear visibility into the parking areas from adjacent sidewalks, and that meet CPTED requirements. For example, use landscaping, low screen walls, decorative fencing, a trellis, and/or grillwork that does not obstruct sightlines and vehicle movement.
- g) Where possible, provide multiple pedestrian



Pedestrian priority established across vehicular access



Screening of surface parking area from adjacent sidewalk



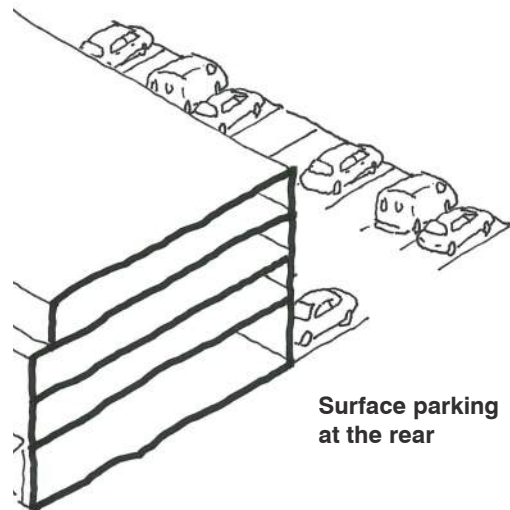
Active uses sleeve parking structures at ground level



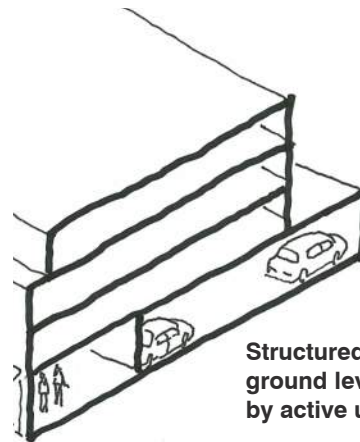
Parking structure with retail uses facing street at ground level

access points to surface parking.

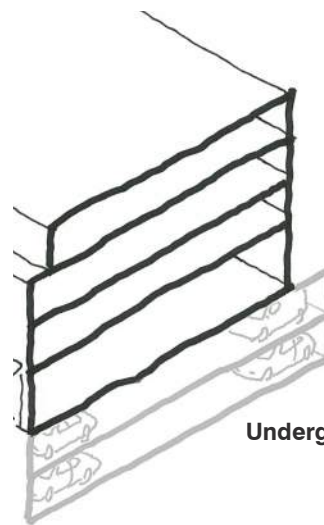
- h) Parking inside a building or parking structure should be separated from adjacent streets with a sleeve of active uses at grade (e.g. retail). Where parking is within a building or structure, separate it from the street with active uses at grade. Where active uses are not feasible, use decorative and integrated materials to screen parking from the street.
- i) Design parking structure with high-quality materials and architectural elements that contribute to a positive streetscape.
- j) Integrate storage units and bike parking into parking structures, where feasible.
- k) Locate servicing and access points where they are minimally visible to the public realm, preferably at the rear of buildings.
- l) Ensure vehicular and servicing access has a minimal impact on the streetscape, by minimizing their size and width and by integrating them with the building design.
- m) Where access and service areas are visible from public spaces, provide high-quality materials and screening elements consistent with the principle building.
- n) Whenever feasible, share laneways, driveways, and servicing areas among multiple buildings.
- o) Wherever feasible, align driveways for multi-residential and mixed-use buildings to reduce potential intersection conflicts.
- p) Integrate waste storage areas into the building design wherever possible, and screen them from view when they are visible from surrounding public streets and spaces.



Surface parking at the rear



Structured parking at ground level screened by active use



Underground parking

Options for parking for mid-rise buildings

2.8 On-Site Features

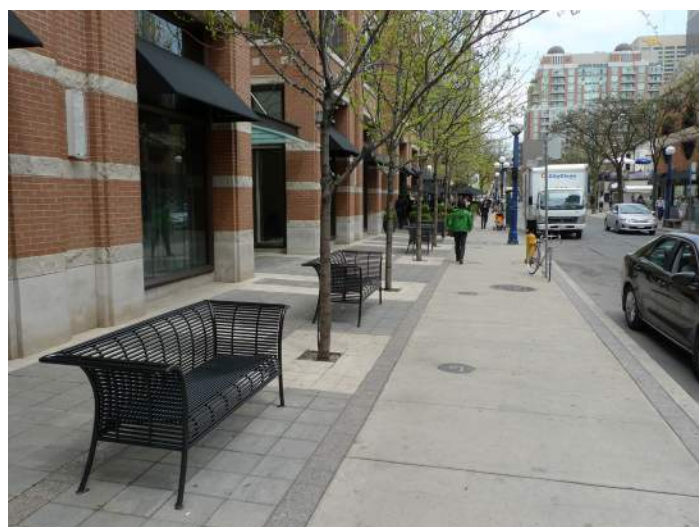
On-site features include the landscaping of spaces around buildings, pedestrian connections, and the location and screening of utilities and servicing infrastructure.

Guidelines

Landscaping

The landscaping and design of the spaces around buildings play an important role in reinforcing high-quality streetscapes, as well as providing amenity for pedestrians and building occupants.

- a) Landscaping should reinforce a well-defined street edge and support the architectural composition of the site.
- b) Where a non-residential building is set back from the street edge, the privately owned land should be designed as an extension of the public realm. Include landscape treatments or pedestrian amenities such as planting areas, seating, lighting, street trees, and public art.
- c) Where residential uses are located at ground level, individual units should be articulated in the façade design and accessed directly from the sidewalk, with a semi-private front yard transition zone that includes landscaping, grade shifts, and low walls or decorative fencing.
- d) Trees planted on private property should reinforce the primary public street tree planting through species selection, location, spacing, and planting conditions.
- e) Landscaping on parts of private property accessible to the public should meet Crime Prevention Through Environmental Design (CPTED) requirements, including the use of low plant materials (under 0.9 metres in height).
- f) Fencing in the Corridors should generally be decorative and low, consistent with CPTED principles. Exceptions may apply where fencing is used to enclose private or non-public shared amenity spaces, or where it functions as a visual screen.



Setback integrated into public realm of streetscape



Transition zone between sidewalk and individual residential units

Utilities

The careful placement and screening of utility infrastructure help to minimize visual and physical impacts on the public realm and pedestrian life.

- g) Discretely locate utility meters, service meters, vents, telecommunications gear, and other necessary mechanical equipment where they are not visible from public spaces. Where visibility is unavoidable, integrate them into the building design using recesses, enclosures, or placement under steps or porches. Screening with landscaping or architectural elements is encouraged.
- h) Where service or mechanical infrastructure must be located apart from the building and is visible from public spaces, it should be screened using landscaping or architectural elements, while remaining accessible for servicing and meter readings.



Screening of utility meters next to a building entrance

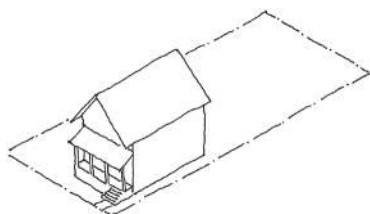
B 3 Mid-Town

3.1 Building Typologies

Building typologies permitted in the Mid-Town area include a range of low-rise to high-rise buildings, from 2 to 15 storeys in height, with active uses at grade facing the Corridor Streets. Residential uses with ground-level access are encouraged along Residential streets or along private streets within larger development sites.

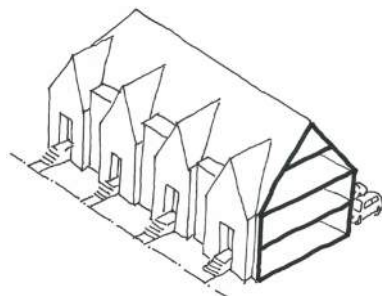
Single Detached

House form buildings with one residential dwelling unit. (See Section B1 for applicable guidance.)



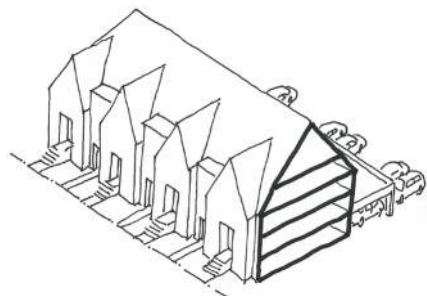
Townhouse

Multiple residential dwelling units attached to each other at their sides, with individual ground-level entrances.



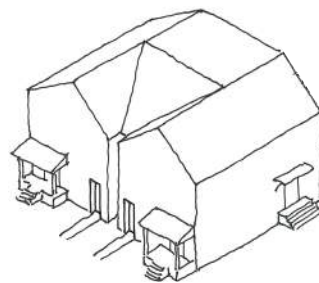
Stacked Townhouse

Multiple residential dwelling units attached to each other at their sides and above (dwelling units are stacked on top of each other), with individual ground-level entrances.



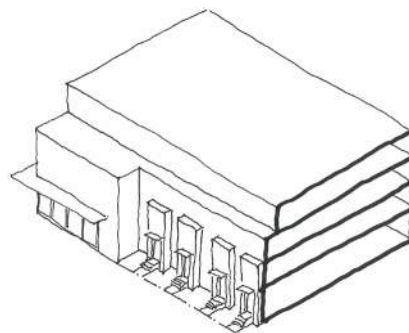
Multiplex

Multiple residential dwelling units attached to each other at their sides and above. Not all dwellings may have ground-level entrances.



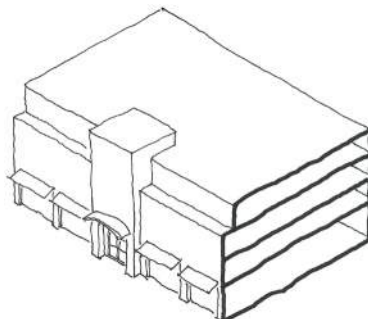
Mid-Rise Residential Building

A building with multiple dwelling units accessed from a centralized entrance and corridors. Some dwelling units may have ground-level entrances.



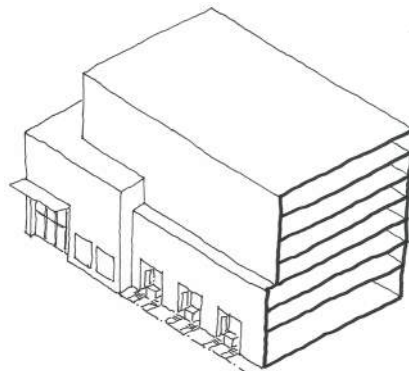
Mid-Rise Mixed-Use Building

A building with multiple dwelling units accessed from a centralized entrance and corridors, and with active uses at grade.



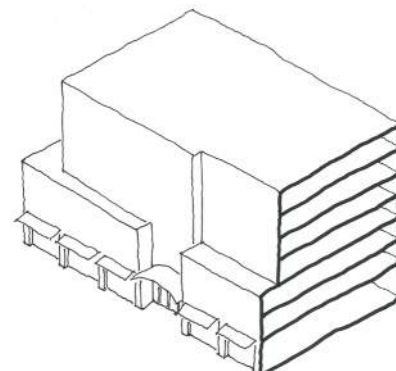
High-Rise Residential Building

A building over 7 storeys in height with multiple dwelling units accessed from a centralized entrance and corridors. Some dwelling units may have ground-level entrances.



High-Rise Mixed-Use Building

A building over 7 storeys in height with multiple dwelling units accessed from a centralized entrance and corridors, and with active uses at grade.

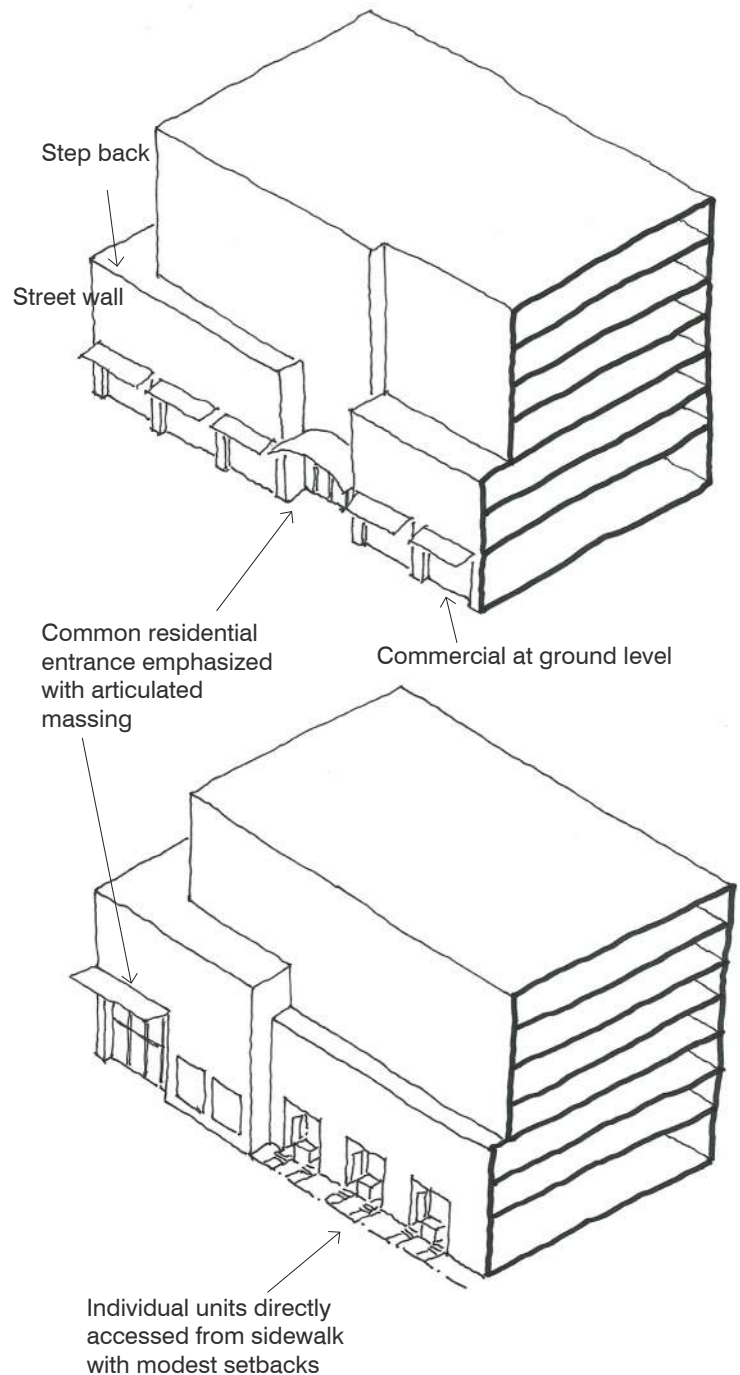


3.2 Height and Massing

Building height and massing impact the character and quality of the street experience and the relationship to neighbouring uses. In the Mid-Town, buildings along the street edge shape the public realm, but it is also important to reduce the visual mass of large buildings.

Guidelines

- a) Divide up larger building masses through architectural articulation, varying setbacks, and rooflines.
- b) Minimum building height in the Mid-Town is 2 storeys. Maximum building height is 12 storeys, except in designated gateways where the maximum height is up to 15 storeys.
- c) Minimum ground floor height is 4.5 metres for mixed-use buildings, to support flexibility, and the long-term adaptability of ground-level uses.
- d) Vary the height of taller buildings across larger sites.
- e) Tower portions of high-rise buildings should be separated from one another by a minimum of 20 metres.
- f) On buildings at corner sites or in visually prominent locations, consider exceptions to setbacks, step backs, or height limits to allow for enhanced architectural expression.



3.3 Gateways, Prominent Corners, and Terminus Sites

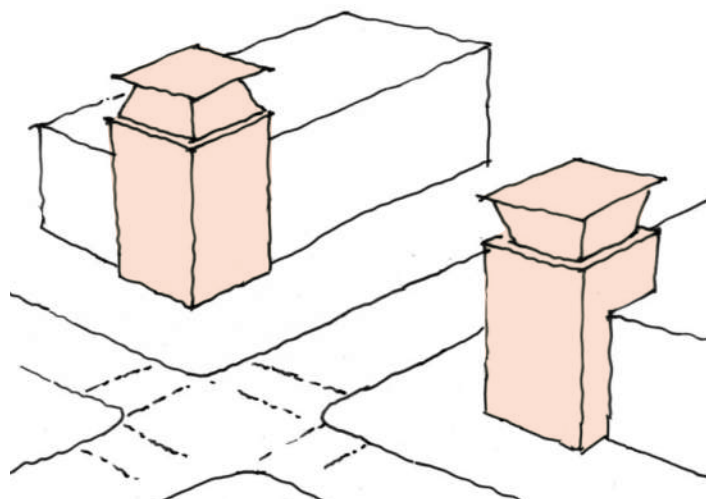
Buildings in gateways, on prominent corners, and at view terminus sites have greater civic obligations to create a design that establishes a landmark and responds to their visual prominence through enhanced architectural treatment, public space, or both.

Guidelines

- a) Enhance the distinction and landmark quality of buildings in gateway and visually prominent locations with taller building elements such as towers, rotundas, porticos, changes in building plane, overhangs, special rooflines, public art, or street wall height exceptions—provided that such elements exhibit compatibility with adjacent context and scale, compatibility with the principal building expression, and design excellence.
- b) Buildings in gateway areas should include high-quality architectural treatment and consistent material use on all building façades visible from the public realm.
- c) Create paired corner buildings on either side of a street to emphasize a sense of entry.
- d) New development and landscaping should frame, not block, public views of landmark sites, natural features, public art, or other significant elements.
- e) Buildings that terminate long view corridors should be designed to feature a prominent architectural element, such as a tower or vertical massing feature.
- f) Off-street parking lots should be screened from the street edge at gateway or landmark locations.



Landmark building element



Paired corner buildings to emphasize a sense of entry

3.4 Street Relationship

The orientation and placement of buildings in a consistent line along the street helps to clearly define the public realm and enhance the pedestrian environment by introducing visual interest and a sense of enclosure.

Fundamental to creating a strong street wall in the Mid-Town is locating buildings at or near the front property line. Above the street wall, a step back defines the top of the wall. For high-rise buildings over 8 storeys, the podium (a low-rise base for the tower above) establishes a consistent street wall, with the tower set back to reduce visual impact at the street level.

Guidelines

- a) Orient and address buildings to the street with clearly defined entry points that directly access the sidewalk.
- b) Place buildings at or close to the street edge, with minimal setbacks.
- c) Create a continuous street wall by extending buildings along the full primary property frontage. Side yard setbacks and gaps are generally discouraged, except where required for mid-block pedestrian connections or vehicular access.
- d) On corner sites, consider providing greater setbacks on the secondary frontage to benefit the public realm with wider sidewalks, landscaping, patios, or market space.
- e) The street wall or podium should be 2 to 4 storeys in height, with active uses at grade.
- f) Above the street wall or podium, taller portions of the building should step back a minimum of 2 metres.
- g) Site buildings to define the edges of public open spaces such as plazas, parks, or squares.



Diagram illustrating street wall/podium height and step back



Podium establishing the street wall with a tower step back

3.5 Heritage Buildings

The Mid-Town contains a number of designated and listed heritage buildings, as well as other buildings with heritage value that contribute to the character and history of the area. It is important that adjacent new development and renovations to heritage buildings maintain and enhance the defining characteristics of the heritage buildings.

Guidelines

- a) Preserve, renovate or adaptively reuse heritage buildings wherever possible.
- b) Where heritage buildings cannot be preserved, renovated or adaptively reused as a whole, integrate the heritage façades or other significant building elements or details into new development. Preserved façades should generally be located in their original locations and appear to be integrated with the new construction in a manner that suggests the entire building has been retained.
- c) Original, historic, building materials and details should be retained whenever possible, and those that had been previously covered or removed should be uncovered, refurbished or recreated. Historic material should never be covered with modern materials, and unpainted brick should not be painted.
- d) Design new buildings to be compatible with adjacent heritage buildings in massing, setbacks, and materials.
- e) New construction should be visibly differentiated from the old, achieving compatibility primarily through harmonious scale, massing, façade articulation, and materiality.
- f) Set back all or part of new building adjacent to a heritage building to create a sense of separation for the heritage building and to highlight important features such as towers, roofline, or other significant heritage elements.
- g) Provide additional setbacks for new buildings adjacent to landmark heritage buildings to maintain the prominence and sightlines of the building, and/or its key landmark features (eg. tower, steeple).



Former Hartt Boot and Shoe Co. Factory, South Core



Preserved façades in a large redevelopment

3.6 Building Elements

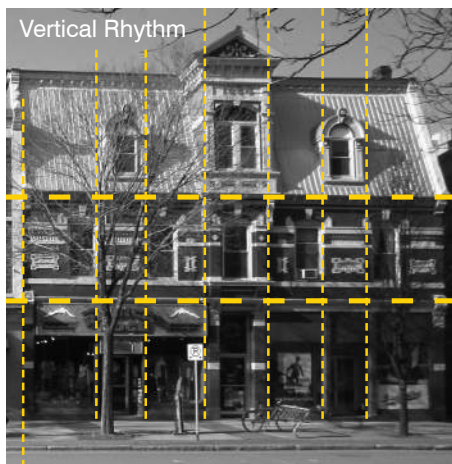
Building elements such as the façade articulation, entrances, rooftop treatment, and signage together determine the overall character of a building. This section has the same content as Section 2.6.

Guidelines

Façade Articulation

The articulation of a building's façade contributes to human scale, a sense of animation, and helps to break up larger building surfaces and reduce the apparent scale of the building. Articulation concerns the placement and relationships of materials, windows, doors and other architectural elements.

- a) Use vertical and horizontal recesses or projections, step backs, or changes of plane, and variations of texture, colour, materials and detailing to articulate a clearly defined organization of the building façade.
- b) Create a human-scaled and fine-grained character along the street with a rhythm of vertical elements or bays, and frequent windows and doors.
- c) Articulate both street facing façades of a corner building to the same high level of design, with architectural elements that respond to the corner.



Articulation of a façade with vertical rhythm of bays, windows, and canopies



Example of a building with strong vertical rhythm elements



Use of material, colour, variation of setback, and roof line

Entrances

Entrances are often the most recognized and used part of building façades, something that people look for when approaching a building.

- d) Entrances to buildings should address the primary street and be clearly articulated and expressed.
- e) Emphasize entrances with architectural forms and detailing such as changes in height and massing, projection, shadow, punctuation, and change in roofline.
- f) Ensure main entrances to public buildings, offices, and residential lobbies are weather protected through use of canopies, awnings, or recesses.
- g) Ensure entrance areas and transitions from inside to outside are barrier-free and accessible, with continuous and relatively flat and smooth grading.



Frequent doors, windows, and pedestrian generating uses



Architectural expression emphasizing main entrance location

Rooftops and Mechanical Equipment

The design of the roofline has an impact on the character of the streetscape, especially from a distance. Both roof and roofline contribute to architectural quality and skyline views.

- h) The expression of the building top and roof should be clearly distinguished from the rest of the building through treatments such as step backs, materials, cornice lines, and overhangs.
- i) Mechanical penthouses should be integrated with the architectural treatment of the roofline and building expression.
- j) Screen rooftop mechanical equipment with materials that are complementary to the building.
- k) Green roofs are encouraged to provide aesthetic and sustainability benefits, as well as providing amenity space for building occupants.



Interesting rooflines and rooftop mechanical integrated into design

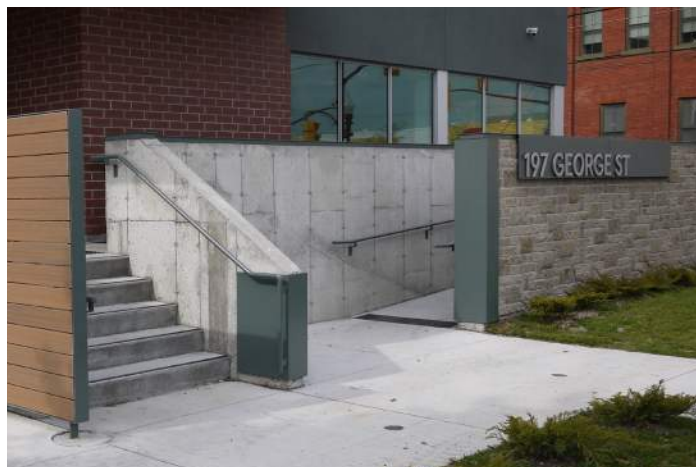


Green roof on a commercial building (image: Sookie, CC BY 2.0, Flickr)

Signage

Building signage plays a significant role in the character and animation of mixed-use areas.

- l) The scale of commercial signage should reinforce the pedestrian scale of the street by locating signs at or near ground level for viewing from sidewalks.
- m) Integrate signs into the organization and design of building façades by placing them within sign bands, architectural bays, friezes, etc.
- n) Signs should not obscure windows, cornices, or other architectural elements.
- o) Commercial signage should not overwhelm the building and/or storefront.
- p) Large freestanding signs such as pylons, signs on top of rooftops such as billboards, and back lit illuminated rectangular sign boxes are discouraged.
- q) Signs should be constructed of durable, high-quality materials and be well-maintained.
- r) Street addresses should be clearly visible for every building.



Address signage integrated into a wall at the entrance



*Retail signage integrated into building design
(Copyright Queen's Printer for Ontario, photo source: Ontario Growth Secretariat, Ministry of Municipal Affairs)*

3.7 Parking, Loading, and Servicing

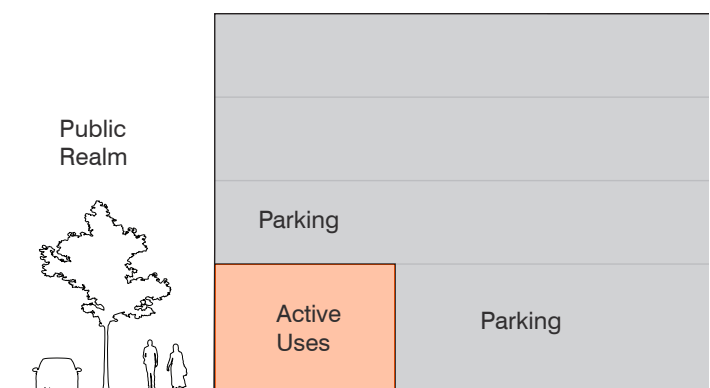
Parking and vehicular access to buildings and properties, and servicing needs such as loading and waste pickup are necessary for the Mid-Town to function properly, but care must be taken to minimize their physical and visual impacts on the public realm and pedestrian life.

Guidelines

- a) Parking should not be located between buildings and the street edge. Parking is best located underground, where possible, above ground in structures or to the rear of buildings in small surface lots.
- b) Break large surface parking areas into smaller parking units through the use of trees, lighting, and walkways.
- c) Ensure surface parking is well lit at night.
- d) Access to parking and servicing areas should clearly prioritize pedestrian movement and the continuity of the public sidewalks.
- e) Clearly demarcate walkways through parking areas with the use of paving materials, landscaping, and lighting.
- f) Screen surface parking areas from adjacent public sidewalks and public spaces using materials that provide a visual buffer while still allowing clear visibility into the parking areas from adjacent sidewalks, and that meet CPTED requirements, for example by using landscaping, low screen walls, decorative fencing, a trellis, and/or grillwork.
- g) Where possible, provide multiple entry/exit points to surface parking.
- h) Parking inside a building or parking structure should be separated from adjacent streets with a sleeve of active uses at ground level (e.g. retail). When an active use at grade is not feasible, the parking may be screened with attractive and decorative materials that integrate with the streetscape and design of



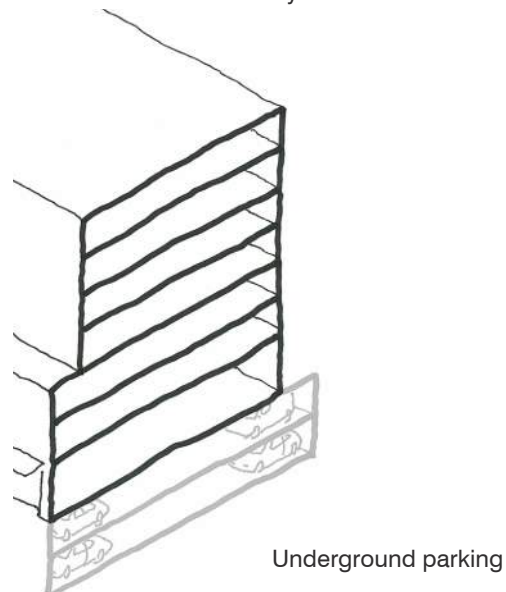
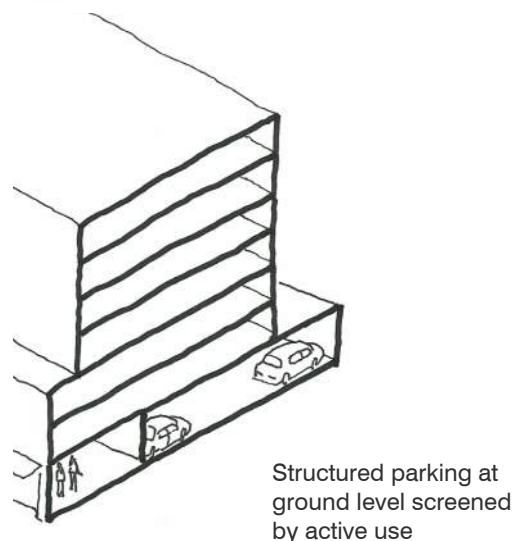
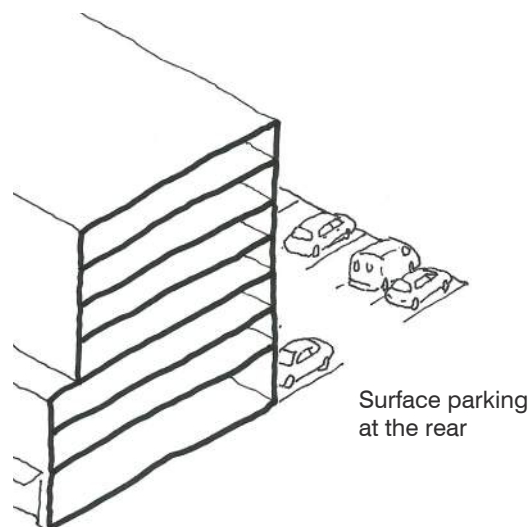
*Parking structure with rhythmic façade design
(image: La Citta Vita, CC BY-SA 2.0, Flickr)*



Active uses sleeve parking structures at ground level

the building.

- i) Parking structure façades should be articulated with high-quality design and materials that contribute to a positive streetscape.
- j) Integrate storage units and bike parking into parking structures, where feasible.
- k) Locate access and servicing where they are minimally visible to the public realm, preferably at the rear of buildings.
- l) Ensure vehicular and servicing access has a minimal impact on the streetscape, by minimizing their size and width and by integrating them with the building design.
- m) Where access and service areas are visible from public spaces, provide high-quality materials and screening elements consistent with the principle building.
- n) Whenever feasible, share laneways, driveways, and servicing areas among multiple buildings.
- o) Wherever feasible, align driveways for multi-residential and mixed-use buildings to reduce potential intersection conflicts.
- p) Integrate waste storage areas into the building design wherever possible, and screen them from view when they are visible from surrounding public streets and spaces.



3.8 On-Site Amenities

On-site amenities include the landscaping of spaces around buildings, pedestrian connections, and the location and design of utilities.

Guidelines

Landscaping

The landscaping and design of the spaces around buildings play an important role in reinforcing high-quality streetscapes, as well as providing amenities for pedestrians and building occupants.

- a) Landscaping should reinforce a well-defined street edge.
- b) Where a non-residential building is set back from the street edge, the privately owned land should be designed as an extension of the public realm, including landscape treatments and/or pedestrian amenities such as planting areas, seating, lighting, street trees, and public art.
- c) Where residential uses are located at ground level, individual units should be articulated in the façade design and accessed directly from the sidewalk or a pedestrian connection, with a semi-private front yard transition zone that includes landscaping, grade shifts, and low walls or decorative fencing, but does not obstruct the installation and maintenance of building services (e.g. water, sewer, electrical, communication, etc.).
- d) Trees planted on private property should reinforce the primary public street tree planting through species selection, location, spacing, and planting condition.
- e) Fencing in Mid-Town should generally be decorative and low, meeting CPTED requirements, except where the fence encloses exterior private or non-public shared amenity space or is being used as a screen.



Setback integrated into public realm of streetscape



Transition zone between sidewalk and individual residential units

Pedestrian Connections

Providing publicly-accessible pedestrian connections through larger development sites is important to connect to trails and provide mid-block connections to improve the overall pedestrian connectivity of Mid-Town.

- f) The clearway in pedestrian connections should be a minimum of 1.8 metres wide.
- g) Provide mid-block pedestrian connections to supplement the street pattern and connect open spaces, major destinations, and transit corridors.
- h) Mid-block pedestrian connections faced with ground-related residential units should allow for a minimum 10 metre separation distance between building faces.
- i) Use distinctive pedestrian surface materials that are continuous and clearly distinguishable across driveways.
- j) Design sidewalks and crosswalks to be barrier-free and accessible.
- k) Pedestrian connections should be well-lit and adhere to CPTED requirements.



Mid-block connection with plantings, lighting and clear sightlines



Demonstration Plan illustrating a mid-block connection that also connects to the Crosstown Trail

Utilities

The careful placement and screening of utilities helps to minimize their physical and visual impacts on the public realm and pedestrian life.

- l) Locate utility meters, service meters, vents, telecommunications gear, and other necessary mechanical equipment discretely and, where they are visible from public spaces, integrate them into the design of the building through techniques such as recesses, enclosures and under steps or porches, or screen them with landscaping or architectural elements.
- m) Service/mechanical elements that must be separated from the building, where they are visible from public spaces, should be screened from view with landscaping or architectural elements, while remaining accessible for readings.



Screen and planting around gas metres

3.9 Institutional Uses

Institutional uses include schools, public libraries, museums, community centres, or other similar uses that meet the recreational, health, social, educational, and cultural needs of residents.

Guidelines

- a) Site stand-alone community service use buildings prominently and where possible, to terminate views or create landmarks.
- b) Terminate important views and vistas with special architectural elements, massing, etc.
- c) Locate stand-alone community service use buildings close to the street to reinforce the street wall and define intersections. Consider further setbacks to create a wider public realm or entry plaza.
- d) Provide main entrances and active frontages with a high level of visual transparency and permeability along the public street.
- e) Consider integrating community service uses into mixed-use, residential, or multi-storey buildings in order to maximize the use of the site and services, promote cost-effectiveness, minimize the building footprint, contribute to the creation of compact neighbourhoods, as well as contribute to an urban street condition.
- f) Consider co-locating or sharing facilities with other community service uses or other compatible uses.
- g) Avoid blank, uninterrupted walls on elevations exposed to public view.
- h) Locate large internal spaces such as gymnasiums or auditoriums to the sides, rear, or interior of buildings.
- i) Provide integrated weather protection elements at main entrances that complement the building's design.



Library located close to the road to frame the street edge



Example of the use of architectural features to denote landmark community facilities

- j) Provide a sufficiently sized and shaded gathering space designed as an outdoor amenity space for community service uses where significant numbers of people are expected to gather or wait outside the main entrance.
- k) Community service use buildings should include public art, either integrated into the building or in a prominent and publicly-visible location on the site.



Plantings and seating area/plaza in front of a library

1 Streets

1.1 Neighbourhood Streets

Neighbourhood Streets are streets with primarily residential uses, located predominantly within the Neighbourhood Areas.

Guidelines

- a) Street trees should be located along streets in Neighbourhood Areas with the goal of creating as continuous a canopy coverage as possible.
- b) Preserve, protect, and incorporate existing healthy and mature trees when implementing changes to the street cross section wherever possible.
- c) Ensure appropriate planting conditions and soil volume for trees (see section C4.2).
- d) Base planting strategies in the public right-of-way to meet Urban Forest Management Strategy for year-round interest, hardiness, drought, salt and disease tolerance, climate change, and biodiversity.
- e) Minimize the use of hard, paved areas to reduce surface run-off and heat island effect.
- f) Provide 1.5 to 2.5 metre wide sidewalks on both sides of the street. Where narrow sidewalks are dictated by existing conditions, consider widening the sidewalks where possible, such as close to main intersections, at bus stops, or near other pedestrian-generating uses.
- g) Consider the use of curb bumpouts and blended curbs at major/busy intersections to reduce the pedestrian crossing distance and slow traffic.
- h) Ensure the design of lighting is bird friendly (directionally downwards) and avoids light spill onto abutting properties and adjacent residential areas.



Existing conditions on Charlotte Street

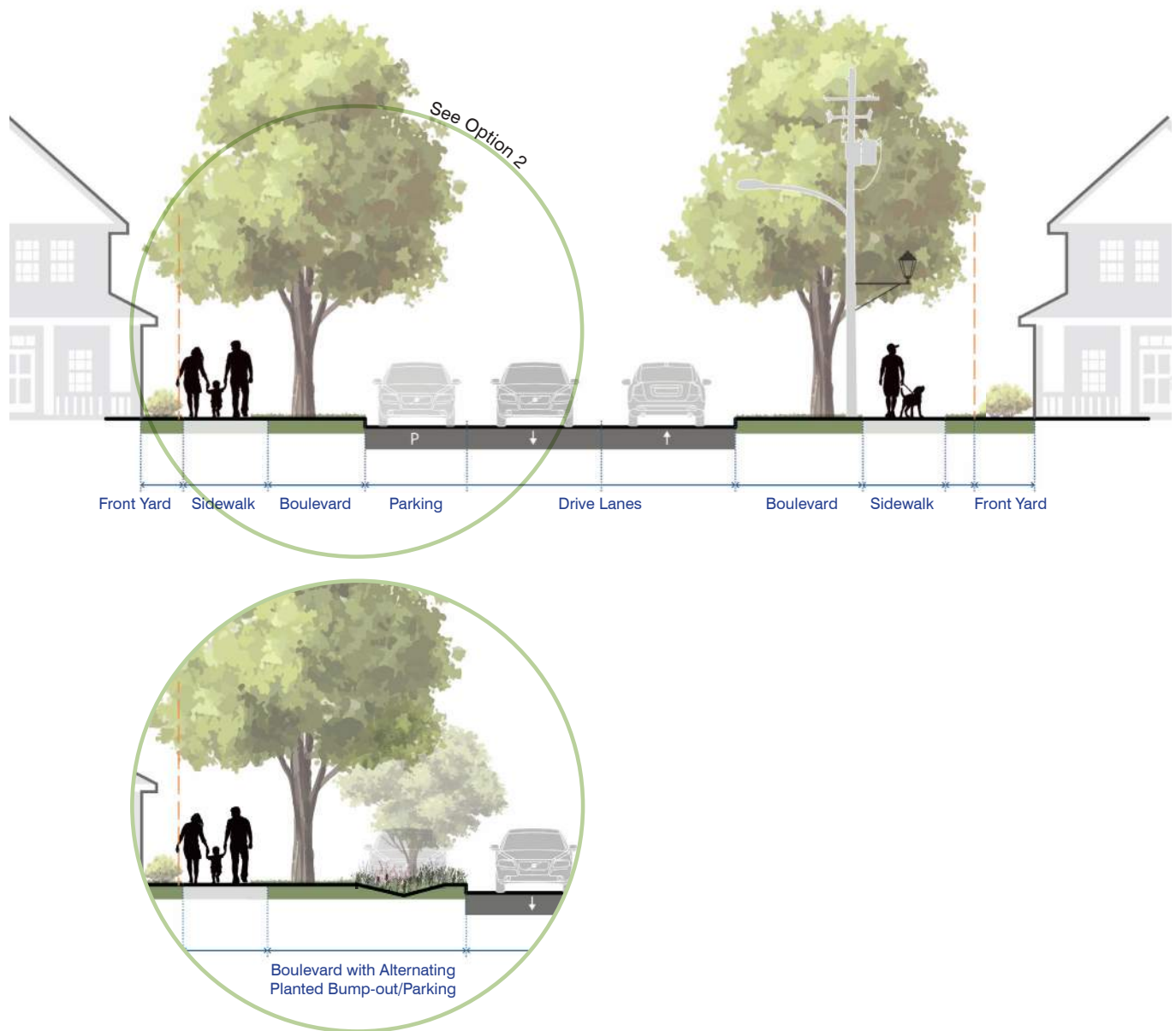


Photomontage illustrating the potential for on-street parking and planted bump-outs on Charlotte Street



Rain garden/bioswale

Proposed typical cross section of Neighbourhood Street



Option 2 alternates on-street parking with planted bump-outs

1.2 Corridor Streets

Corridor Streets include streets intended to be a mix of retail and residential streets or have active uses at grade and include the main streets in the Corridors and Mid-Town Areas.

Guidelines

- Street trees should be located as continuously as possible along Corridor Streets.
- Provide a softscape boulevard with large canopy street trees on both sides of the street.
- Ensure appropriate planting conditions and soil volume for trees (see section C4.2).
- Plant trees in open planting beds wherever feasible. Open planting beds are better for trees, ensuring adequate aeration and water as long as there is protection for roots from compaction.
- Base planting strategies in the public right-of-way on year-round interest, hardiness, drought, salt and disease tolerance, and biodiversity.
- Provide sidewalks with a minimum 2.1 metre wide pedestrian clearway on both sides of the street wherever possible.
- Provide paved market zones adjacent to retail frontages.
- Consider the use of curb bumpouts at intersections with local streets to reduce the pedestrian crossing distance and slow traffic.
- Consider on-street parking to support retail uses and provide traffic calming, subject to restriction.
- Ensure the design of lighting is bird friendly (directionally downwards) and avoids light spill onto abutting properties and adjacent residential neighbourhoods.



Existing conditions on Regent Street

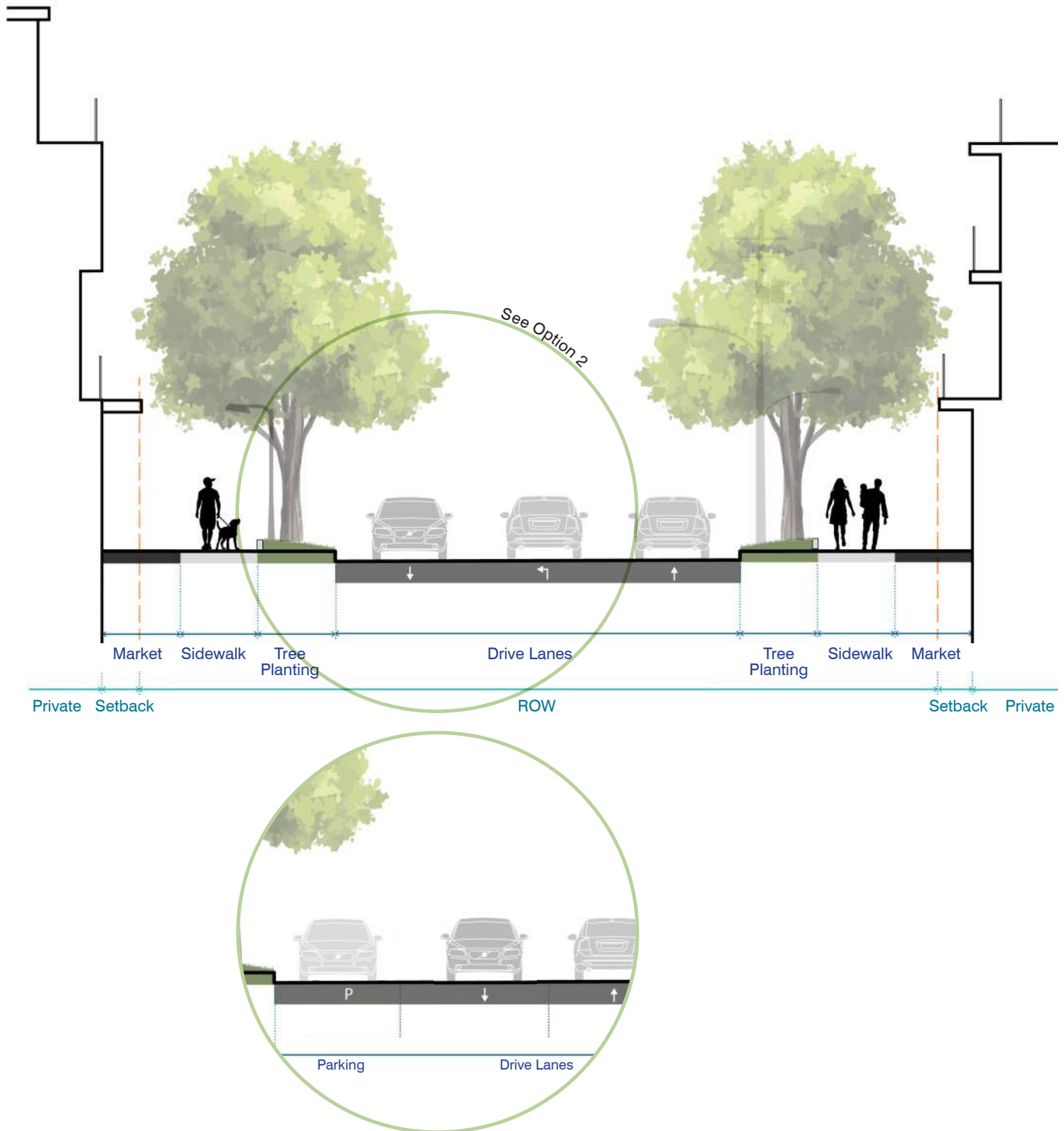


Photo montage illustrating the potential for streetscape improvements on Regent Street



Mixed-use street with planted boulevards, street furniture, and retail space

Proposed typical cross section of Corridor Street



*Option 2 with on-street parking where space allows
(for example when no left turn lane is required)*

C 2 Open Space

2.1 Queen's Square Park

Queen's Square is a significant 3 hectare historic public park in the South Core. The current layout and uses of the park has a typical suburban park feeling, with a swimming pool and sports fields. Revitalization of Queen's Square as a neighbourhood-scale urban park would significantly enhance its role within the South Core and its attractiveness as a neighbourhood destination.

Guidelines

- a) Enhance the character of the surrounding public realm through public art, site furniture, seating areas, and landscape treatments.
- b) Provide shaded areas through a combination of tree canopy cover and shade structures.
- c) Use distinctive, high-quality paving treatments for the hardscape areas.
- d) Include community and civic event spaces as well as playful elements for children.
- e) Include ample seating and a full furniture program, such as lighting, amenities for seniors, children and youth, and public art.
- f) Queen's Square should be well-lit and adhere to CPTED requirements.
- g) The clearway of main pedestrian routes through Queen's Square should be a minimum of 2.1 metres wide.



Planting beds do not obstruct views into the park



Lee Lifeson Art Park, Toronto

2.2 Urban Open Space

Urban Open Spaces are public open spaces that are smaller than traditional neighbourhood parks and include plazas, parkettes, and squares. Urban Open Spaces may be owned by the City or be POPS made available to the public by way of legal easements and/or agreements. It is important that these spaces are universally accessible to ensure residents and visitors of all ages and abilities are welcome.

Guidelines

- a) Locate new urban open spaces to achieve significant public exposure and access with frontage on 2 public streets, where possible.
- b) New city-owned urban open spaces should be between 0.25 to 1 hectare in size, but should not take a linear form.
- c) POPS should be a minimum of 75 square metres in size and minimize hard landscaped areas, where possible.
- d) POPS should be connected to, and have at least 7.5 metres of direct frontage along the public sidewalk or trail system.
- e) Adjacent buildings should have primary and active frontages facing the open space.
- f) Design new urban open spaces to enhance the character of the surrounding public realm through public art, site furniture, seating areas and places to eat, landscape treatments, as well as street-related activities such as vendor and exhibit space.
- g) Provide shaded areas through a combination of tree canopy cover and shade structures.
- h) Use distinctive, high-quality paving treatments for the hardscape areas of new urban open spaces.
- i) Include community and civic event spaces, as well as performance venues and playful elements for children.



Public art, seating, and shaded areas



Carleton Street paving treatment



Unobstructed paving surface for pedestrians and cyclists

- j) Include secure bike parking in accessible and visible areas.
- k) Include ample seating and a full furniture program, such as lighting, opportunities for outdoor cafés and restaurants, facilities for seniors, children and youth, water features, and public art.
- l) New urban open spaces should be well-lit, accessible, and adhere to CPTED requirements.
- m) The clearway of main pedestrian routes through urban open spaces should be a minimum of 1.8 metres wide.



Pentagon Row, Arlington, VA
(image: Solomon Abrams, CC BY 2.0, Flickr)

2.3 Linear Greens

Two linear “Greens” on Smythe Street and University Avenue near George Street (the Smythe Street Green and the University Avenue Green) form small open spaces in public ownership that sit between the roadway and privately owned houses.

Guidelines

- a) Adjacent buildings should have primary and active frontages facing the Greens.
- b) Provide a double row of trees, with a widened sidewalk of a minimum of 2.1 metres width between the two rows, where possible.
- c) Consider relocating the sidewalk further from the roadway or the nearest private property line to allow planting of trees, if necessary.
- d) Provide seating and other furniture such as pedestrian-scaled lighting and waste receptacles.
- e) Consider locating public art in the Greens.
- f) Consider low-maintenance planting areas to reduce the area of lawn.
- g) Ensure the Greens are well-lit and adhere to CPTED requirements.



Smythe Street Green



University Avenue Green

2.4 Trails

Several trails connect the South Core to the City Centre, the riverfront, and other areas of Fredericton. The City has been expanding and connecting the Cross Town Trail through the South Core to create a robust active transportation route, connected to Fredericton's larger trail network.

Guidelines

- a) Trails should be 3.5 metres wide to facilitate two-way cyclist or pedestrian movement.
- b) Pedestrian and cycling lanes should be painted on multi-use paths or clearly identified by other means to minimize pedestrian and cycling conflicts.
- c) Ensure trails include adequate amenities including seating, waste receptacles, and signage.
- d) Redevelopment of properties adjacent to the Cross Town Trail should include active frontages that face the Trail.
- e) Provide frequent access points along the Cross Town Trail from adjacent streets, trails, open spaces, and nodes of activity.
- f) Provide tree planting along trails per the Urban Forest Management Strategy to shade and enhance user experience in the summer.



Multi-use path designed to accommodate a range of users



Rookwood Avenue multi-use path



Activated multi-use path along a mixed-use corridor



Metalworks Condominium multi-use path



Existing conditions along the Cross Town Trail (McLeod Avenue)



Demonstration of development along the Crosstown Trail with townhouses fronting on the trail and new pedestrian connections

C 3 Public Art

Public art enhances the experience of the public realm, adds visual richness, and provides landmarks within a community. On private sites, public art distinguishes the development itself. Public art is also an important tool to celebrate local heritage and ground new development in the history and character of its context. Both public and private art contributions are important to the visual identity of the community.

Guidelines

- a) Identify priority locations for public art that can include visually prominent locations such as gateways, corners, landmark sites, and important view corridors, while not obstructing sightlines.
- b) Public art should be considered throughout the planning and detail design process with an artist included as a core member of the team.
- c) Public art should be clearly visible and physically accessible to the public.
- d) Public art should enhance the public realm through artistic excellence and originality, and be appropriate to the site or location's physical and cultural context.
- e) Negotiate public art for significant private development projects, including as part of POPS.



The Birth of Venus (André Lapointe 2009), Sculpture Gallery at the Beaverbrook Art Gallery



Butterfly planting along the Wolastoq (Saint John River) riverfront



Public art installation in a park

- f) Consider the full range of possibilities for public art including freestanding work and site specific work that is integrated into paving, lighting, furnishings, retaining walls, etc.
- g) Public art should not obstruct pedestrian, cyclist or vehicular circulation, entrances, windows, or sightlines to important natural and built features.
- h) Public art should exhibit high-quality construction, installation and materials, as appropriate for its intent.



Watermark by Gerald Beaulieu



Cartier Avenue lighting installation by Lightemotion, Quebec City



Koilos by Michael Christian, Distillery District, Toronto (Copyright Queen's Printer for Ontario, photo source: Ontario Growth Secretariat, Ministry of Municipal Affairs)

C 4 Green Infrastructure

4.1 Stormwater Management

Reducing impervious surfaces improves stormwater absorption, and retaining and treating stormwater runoff helps protect natural watercourses. These interventions help mitigate the impact of future climate conditions, such as increased rainfall events. It is important to note that development must adhere to all City of Fredericton stormwater management guidelines.

Guidelines

- a) Use Low Impact Development strategies such as: soakways, infiltration trenches and chambers; perforated pipe systems; and, rain gardens.
- b) Retain stormwater on-site through rainwater harvesting and on-site infiltration.
- c) Direct stormwater flow to landscaped areas and rain gardens and minimize the use of hard surfaces in order to reduce the volume of runoff into the storm drainage system.
- d) Store snow piles away from drainage courses, storm drain inlets, and planted areas.
- e) Use infiltration trenches, dry swales, and naturalized bioswales adjacent to parking areas to improve on-site infiltration.
- f) Introduce green infrastructure, such as bioswales or bioretention planters to enhance ground water infiltration and improve water quality as part of a comprehensive water management plan.
- g) Use perennial, native, and/or deep-rooted plants in bioswales and other planting areas to bind soil together, prevent washing out of soils, and improve absorption.



Example of an innovative stormwater management facility.



Bioretention planters for stormwater management, Portland OR

- h) Encourage rainwater harvesting to provide the passive irrigation of public and private greenspace, including absorbent landscaping, cisterns, rain barrels, underground storage tanks, infiltration trenches, etc.
- i) Consider the installation of subsurface basins below parking lots to enable stormwater to be stored and absorbed slowly into surrounding soils.
- j) Where feasible, use curb cuts along walkways and driveways to allow stormwater to flow into planted zones or infiltration basins.



Curb cut allowing rainwater runoff into planting area, Portland, OR

4.2 Tree Planting

A central challenge in the urban environment is the incorporation of trees. Trees are invaluable piece of green infrastructure, acting as urban lungs. The proper selection and detailing of tree plantings will contribute to their long-term health and success. Providing for increased soil areas, native and drought tolerant species, and giving trees ample space to grow will increase their chances of reaching maturity, and increase their lifespan. Trees provide a range of benefits, including providing shade, reducing ambient temperatures, and contributing to the character of the space and surrounding neighbourhood. A variety of strategies will increase the likelihood of success of planting canopy trees. The City's Urban Forest Management Strategy will guide the ongoing growth and maintenance of public realm trees.

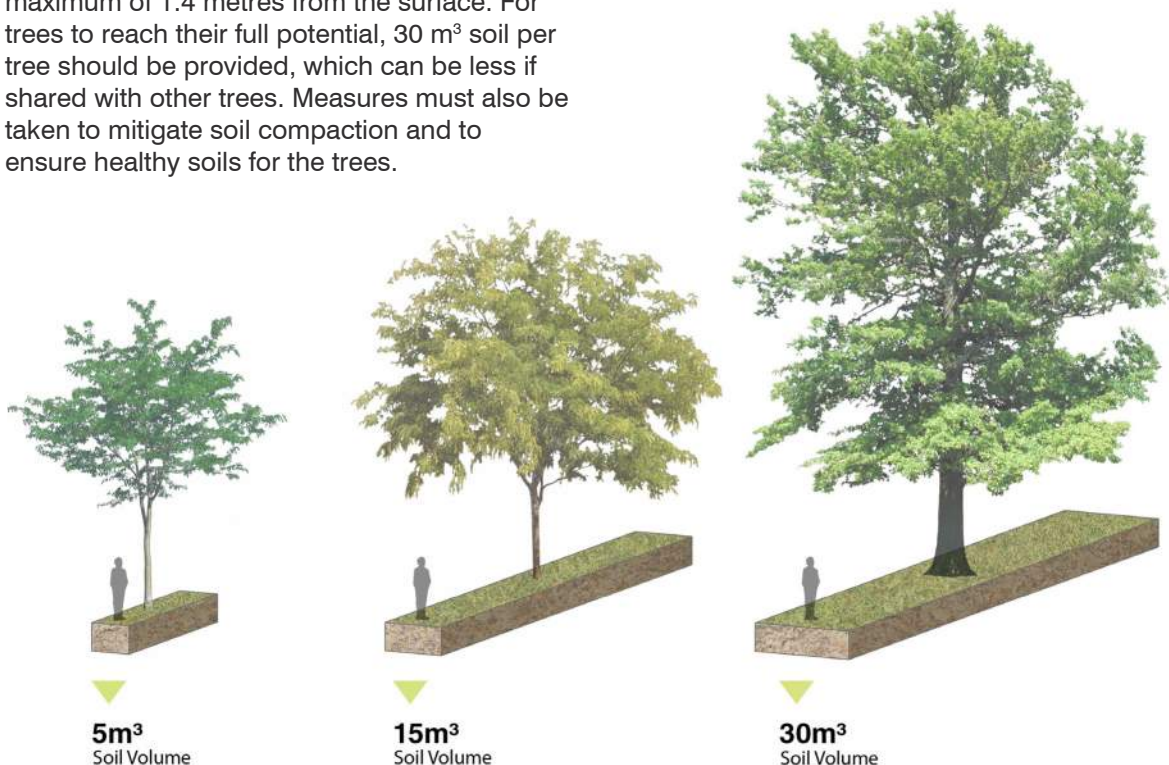


Tree planting along Front Street in the West Don Lands, Toronto

Guidelines

- a) Preserve and incorporate existing trees wherever possible and ensure existing trees are healthy and protected from impacts during construction and development.
- b) Street trees require a minimum 20 m³ uncompacted soil volume per tree, within a maximum of 1.4 metres from the surface. For trees to reach their full potential, 30 m³ soil per tree should be provided, which can be less if shared with other trees. Measures must also be taken to mitigate soil compaction and to ensure healthy soils for the trees.

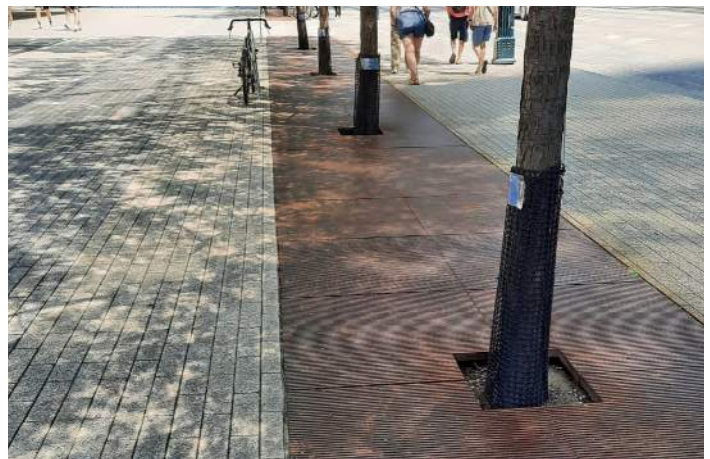
Diagram illustrating the relationship between uncompacted soil volume and tree size



- c) Where minimum uncompacted soil volumes cannot be achieved, use structural soil cells (a system of structural plastic units). Structural soils and structural sands can be used to connect adjacent soil volumes.
- d) Where space is limited and trees must be placed in a hardscape condition to maximize at grade pedestrian space, use of open planters with curbs is preferred. When using tree grates, size the openings to allow tree trunks to grow.
- e) Plant a diverse selection of resilient canopy tree species, with preference given to native (or near-native) species and migrating species expected to do well under future climate conditions.
- f) Provide species diversity across the South Core to promote resilience in the ecosystem.
- g) Use trees to establish a comfortable microclimate (e.g. provide wind and noise reduction and cooling effects).
- h) Ensure tree planting areas have adequate drainage, such as through the provision of sub-drains.
- i) Implement a watering program during the establishment period of the tree. Provide watering in times of drought.
- j) Avoid conflicts with underground and above grade infrastructure and utilities, as well as known development construction plans, by arranging reviews with City stakeholder agencies early in the development process.



*Soil cell installation at Lincoln Center New York
(Source: DeepRoot on Flickr.com)*



Trees in hard paving with connected soil volumes



Trees contribute to comfortable microclimates

Appendix

Demonstration Plan

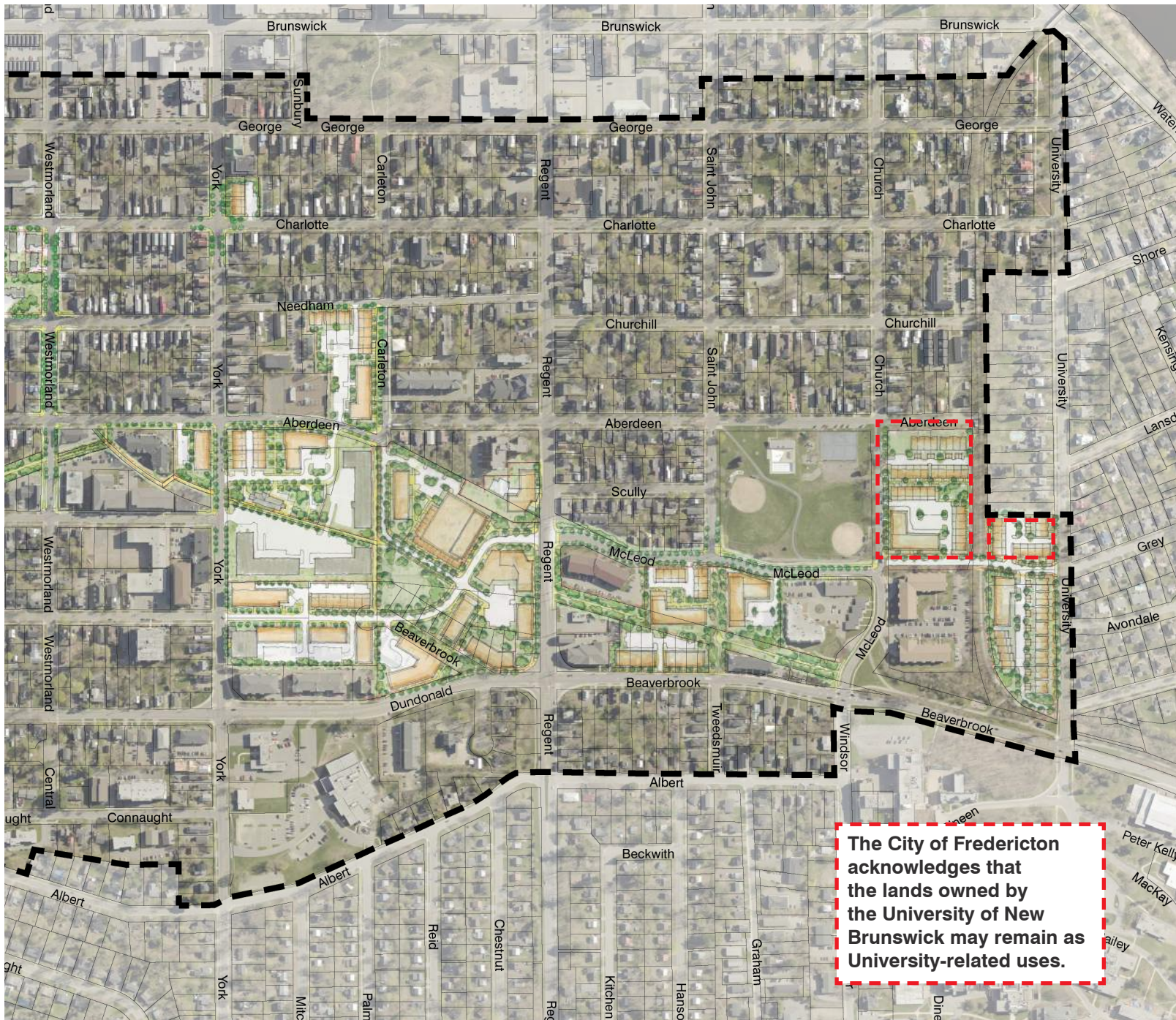
At the first design studio, an initial set of demonstration plans were developed to illustrate principles and concepts. These plans were revised and refined through the subsequent design studios and consultations with landowners.

The Demonstration Plan is an illustration intended to show a scenario of how the Vision and Design Principles of the South Core Municipal Secondary Plan may be achieved. It is further intended to spark creativity and challenge the current state of the built form.



The Demonstration Plan is not a detailed design plan, nor does it represent any actual development proposals. It does not account for any technical, economic, phasing, or ownership constraints, among other considerations. The Demonstration Plan highlights the development of the new Mid-Town urban centre, a primary

intensification area linked by the existing Cross Town Trail as well as new trails and open spaces, connected to the City Centre by pedestrian-oriented Corridors. The Demonstration Plan also shows examples of redevelopment in the Neighbourhood and Corridor Areas consistent with the South Core Plan.



FREDERICTON

SOUTH CORE PLAN



The Planning
Partnership

Trace Planning & Design

CBCL

ERA Architects

FREDERICTON SOUTH CORE PLAN

What We Heard Report #1

August 2024

Land Acknowledgement

We acknowledge that the City of Fredericton is situated on traditional Wolastoqiyik territory. The territory of the Wolastoqey people is recognized in the Peace and Friendship Treaties to establish an ongoing relationship of peace, friendship, and mutual respect between equal nations. The river that runs through our city is known as the Wolastoq, along which lived the Wolastoqiyik, “the people of the beautiful and bountiful river.”

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1.0 Introduction

The City of Fredericton is developing a Secondary Municipal Plan for the South Core that will take into account recent growth in Fredericton, while maintaining the neighbourhood's community character. This plan will provide a framework to guide growth and land use within the area.

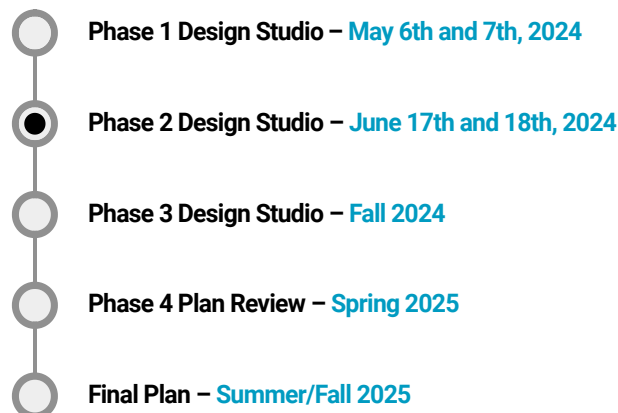
Online and in-person public engagement is integral to the South Core Secondary Municipal Plan. In April 2024, the City set up a dedicated project web page on their Engage Fredericton website. The page is the home base for the project including frequently asked questions, the project timeline, links to background information and the team's work in progress.

In person engagement is organized around three South Core Design Studios during which there are multiple forums and venues to share input on the team's work in progress. This first What We Heard Report summarizes the input we received at Design Studio #1 held on May 6 and 7, 2024 at the Boyce Farmer's Market, and Design Studio #2 held on June 17 and 18, 2024 at the Fredericton Capital Exhibition Centre.

A Design Studio is a working format where multiple members of the project team "set up shop" in a venue to work exclusively on a project for a few days. The team works in the Design Studio preparing drawings, completes site photography, meets with City staff, stakeholders and the public, and undertakes site analysis.



Study process:



2.0 First Nation Engagement



The City of Fredericton acknowledges it is situated in the traditional territory of the Wolastoqiyik People. Over the past several years the City has established a collaborative engagement process with the Wolastoqey Nation in New Brunswick (WNNB) as well as the Wolastoq Grand Council, as Rightsholders. Ongoing engagement with WNNB includes the Consultation Manager, Environmental Assessment Coordinator, Ethnohistorian and six Resource Development Consultative Coordinators (RDCC) who represent and report to their respective Wolastoqey Communities. Ongoing engagement with the Wolastoq Grand Council includes the Grand Chief and Grandmothers who are from various Wolastoqey Communities. The City initiated collaborative engagement with WNNB & the Grand Council in 2021 and continues to regularly meet monthly.

In addition to the monthly updates on the South Core Plan, WNNB was invited to meet directly with the Consultants. Unfortunately, due to scheduling conflicts the in-person meeting did not transpire. However, the City will continue to provide information and updates on this important project as it moves forward and seek input from the Wolastoqiyik People.

3.0 Input Received Through Design Studio #1

The first Design Studio kicked off the planning process at the Boyce Farmer's Market on May 6th and 7th, 2024. The Studio included a sequence of team working sessions, meetings with mayor and councillors, one-on-one discussions with residents and large format events for the community to share their thoughts. Over the course of two nights, we heard from over 100 people and had many productive discussions on the early ideas for the South Core. The following is a summary of what we heard during Design Studio #1.

3.1 Conversations with the Mayor & Councillors

May 6, 2024

5
meetings

Urban Development and Design

- Possibility of extending boundaries to Waterloo Row and Beaverbrook to align with the South Core and Queens Square.
- Strong design guidelines and preservation of historical streetscape are necessary in light of downtown densification.
- Concerned about demolition and new construction erasing neighbourhood character.
- Need better guidelines to prevent developers from bypassing regulations.
- Encourage a shift towards mixed-use neighbourhoods with less emphasis on parking requirements.
- Supports taller buildings (up to 10-11 stories) closer to the river.
- Prefer house form buildings.
- Support for secondary units in backyards but need careful zoning considerations.

Heritage and Preservation

- Concern over the restoration of Christ Church Cathedral and maintaining heritage sites.
- Strong design guidelines and preservation of historical streetscape are necessary in light of downtown densification.

Parks and Green Spaces

- Lack of small urban parks.
- Preserve green areas and promote walkability over car-centric development.
- Lack of north-south trails and calls for better-connected green spaces.

Housing and Community

- Need greater mix of housing ownership.
- Propose temporary street closures in summer for community events.

Cycling and Transportation

- Need cycling connections and infrastructure (separated bike lanes on key streets).
- Concerned about safety of cycling in the South Core and the need for safer routes.

3.2 Conversation with the Planning Advisory Committee

May 7, 2024

General Comments

- There are few repercussions for non-compliance to details of planning approvals.
- Street safety is part of the Fire Chief's mandate and is crucial for everyone.
- Organic hierarchy of streets with multiple entrances to multi-unit buildings is important.
- Real entrances at street level are critical for street life.

Parking and Accessibility

- Street parking and opportunities for shared parking can help relieve parking problems.
- Existing park space is valued, but there's a lack of central smaller parks.
- Potential for green space on Sobey's property and urban parks in railway land development.
- Importance of safe park access and crossings.

Heritage and New Development

- New development should have a distinguishable modern look that references heritage materials and guidelines.
- Guidelines needed for rooflines, setbacks, and overall character consistency.
- Incentivizing heritage property upkeep is essential due to the area's historical significance.



Alward Harned House on George Street.



The Old Rectory Home of the Roberts Family on George Street.

3.3 Conversation with the Heritage Preservation Board

May 6, 2024

General Comments

- Recommendations from the Heritage Program and Services report are being implemented.
- Importance of managing increased density while maintaining character.
- Indigenous heritage and portage routes need acknowledgment.

Heritage Property Designation

- Few new properties have been added to the preservation by-law.
- Need to consolidate different heritage databases.
- Climate and waste impacts of demolishing buildings are a concern.

Queen's Square and Provincially-Owned Buildings

- Queen's Square heritage designation failed; some individual houses were designated.
- Provincial buildings like the Centennial Building have significant heritage value.

Demolition by Neglect and Incentives

- Discussion on preventing demolition by neglect with incentives.
- Lack of city grants or tax incentives for heritage preservation.

Additional Issues

- Need for intermediate steps before formal designation.
- Flexible zoning and variances can undermine heritage efforts.
- Incentivize compliance with energy codes.
- Fire escapes on heritage buildings and new parking lots are aesthetically problematic.
- Exhibition grounds not included in the current study, despite being part of the South Core.



Odell Cottage on George Street.

3.4 Conversations with Major Landowners/Developers

May 6, 2024

General Comments

- Considering development for 8 sites.
- Needs 10-12 floors for financial viability but can only build 8.
- Requires downtown rents of \$2,500 - \$3,000 for 8-floor buildings.
- Advocates for walkable communities to reduce parking ratios, suggesting 1:1 parking or offering shared use vehicles.
- Low rental absorption rates downtown.
- AODA compliance for buildings with more than 4 units.
- Emphasis on accessibility, energy efficiency, flood risk management, and stormwater within future planning decisions.
- Consistent setbacks for unit types, limited side yard setbacks, and improved fire access to backyards.

3
conversations



8-10 storey residential buildings on Queen Street and Northumberland Street.

3.5 One-on-one Conversations with Residents

May 7, 2024

An invitation was posted on the project webpage and through social media inviting residents to register for a one-on-one conversation with a member of the consulting team during Design Studio #1. Eight conversations were held with residents. The following summarizes the common themes.



Crime and Safety Concerns

- **Increased Crime:** Rising incidents of petty theft and break-ins, especially on Smythe Street, with many not being reported.
- **Neighbourhood Watch:** Efforts to coordinate neighbourhood watch to address these issues.

Housing and Development

- **Rental Properties:** Issues related to rental properties, such as lack of fencing and easy backyard access, affecting neighbourhood security.
- **Landlords and Property Use:** Landlords buying multiple properties for parking or rental purposes, impacting the community.
- **Development Concerns:** The city's development guidelines are not being followed, leading to poorly integrated and unattractive buildings.
- **Fire on Northumberland:** Concerns about rebuilding after a fire, including the need for better planning and maintaining streetscape aesthetics.

Urban Planning and Zoning

- **Intensification:** Concerns about intensification on Smythe, Regent, and York Streets, with developments not including affordable housing despite density bonuses.
- **Public Participation:** Loss of public trust in the city's development processes and reduced community involvement.
- **Zoning and Heritage:** Issues with zoning changes and the concentration of social facilities in small areas, with fears of inadequate enforcement in heritage areas.

Infrastructure and Public Spaces

- **Trails and Connectivity:** Need for improved connectivity and completion of the Cross Town trail, along with securing land for trails.
- **Traffic and Calming Measures:** Suggestions for traffic calming, such as lower speed limits, more crosswalks, and better traffic management on busy streets.
- **Commercial Areas:** Allowing more commercial development on certain streets to balance residential needs.

Community and Social Inclusion

- **Public Realm and Social Inclusion:** Emphasis on creating a public realm that supports social inclusion and health, with spaces that foster relationships between different community members.
- **Diversified Housing and Facilities:** Need for diversified housing options and common gathering spaces to support community cohesion.
- **Transit and Accessibility:** Importance of efficient transit services to connect the city centre with outlying areas and improve accessibility.



Valley Sentier Fredericton Trail.

3.6 Public Workshops

Design Studio #1 included two evening public workshops. The first evening began with a kick-off presentation to describe the public realm, great neighbourhoods, the role of buildings in contributing to neighbourhood character, and the evolution of the South Core. People were asked to describe what attributes, conditions or qualities contribute to neighbourhood character.

100±
participants

Here's an example of worksheets filled out by participants:



Photos from the public workshop for Design Studio #1.

The following is a summary of comments grouped under headings from the first evening's public session. Comments focused on identifying opportunities for change.

Streets, Trails, Transit

- Improve trail connections and more green spaces are necessary.
- North-south active transportation options are needed.
- Need for more separated bike lanes and better planning for bicycles.
- Enhance traffic calming measures through landscaping and reducing high speeds.
- Make it easy to walk to services and stores within neighbourhoods.
- Increase the number of crosswalks, especially on York, Smythe, and Regent streets.
- Better snow clearing for sidewalks, with homeowners handling it after the city.
- Address issues with one-way streets and directional closures.
- Improve public transit to run 7 days a week with more robust service to reduce traffic.
- Better connectivity between the north and south sides of the river, possibly with a ferry.
- Incorporate uniform, artful street lighting to enclose streets and calm traffic.
- Urban tree canopy is valued for enhancing walkability needs renewal program.

Yards

- Use large yards for organic farming to address expensive groceries.
- Promote minimal front setbacks to create a sense of enclosure.
- Allow sensitive, small-scale development in yard spaces through bylaw changes.
- Maintain or increase the percentage of landscaping.
- Encourage trees and shrubs on private lots.
- The townhouses at Westmorland and Brunswick are good examples of density.
- Explore alternative driveway construction to manage stormwater effectively.
- Protect green spaces between houses and maintain the area's character with lots of trees.



Photo from the public workshop for Design Studio #1.

Parks and Open Space

- Maintain and maximize current green spaces for water runoff and retention.
- Increase the number of smaller neighbourhood parks and trees.
- Develop parks in unused spaces and incorporate more trees in public spaces.
- Incorporate green spaces in new developments and adapt to climate change.
- Promote street calming measures with street trees and appealing street lights.
- Enhance access to parks like Queen's Square for informal recreation.
- Encourage tree bylaws to limit the removal of mature trees and replace them with new vegetation.
- Need better utilization of existing parks; small parks are hard to manage.
- Leisure Master Plan - Potential for Queen's Square, but active facilities may need relocation.
- Sobeys's property and Railway lands are potential lands for green space.
- Remove streets to connect parks and consolidate green spaces.

Buildings

- Ensure design guidelines are enforced to retain Fredericton's unique character.
- Promote environmental standards and net-zero buildings.
- Address the lack of affordable housing units for families with children.
- Increase affordable senior housing and community services like daycares.
- Limit building heights to 3-4 stories and avoid massive apartment buildings.
- Maintain small lots and avoid lot consolidation.
- Incorporate design features that mitigate heat islands and manage flood planning.
- Repurpose industrial buildings in South Core for residential or light commercial use.
- Encourage mixed-use buildings with retail and coffee shops on main streets.
- Limit vinyl siding and promote the use of more brick in building facades.
- Promote low-rise developments to scale and preserve the "sky view" with height limitations and setbacks.



Photo from the public workshop for Design Studio #1.

Social and Environmental Sustainability

- Community hub - need for social infrastructure and inclusive spaces, particularly along Smythe Street.
- Strategy needed for managing aging trees.
- Off-leash dog areas proposed for Beaverbrook and McLeod.
- Public realm design should target green house gas reductions and consider green infrastructure.



Photo from the public workshop for Design Studio #1.

Here's what people said at Workshop 1, day 1

(size of bubbles is adjusted to fit the text)



Below: Photos and a filled out worksheet from the public workshop for Design Studio #1.



Parks and Open Space

15 minutes city concepts

Current green space needed for water runoff and retention

Smaller neighbourhood parks needed

Climate change adaptation

We appreciate Queen's Square informal recreational character

Calm streets + street trees

Maintain green spaces

Tree bylaws? Limit the removal of mature trees on private property or introduce a calculation on how much new vegetation needs to be replaced

Streetlamp (ex Regent between King/George) are aesthetically nice to have

New developments have no green space

Yards

Bylaw changes for allowing sensitive + small scale development within available yard space

Minimal front setbacks to create enclosure

Enforce setbacks

The townhouses at Westmorland and Brunswick street are great example of density for housing

How can we promote net zero buildings (no emission) or small scale electric grids (ie community solar gardens) electrical grid resiliency

Yards could be used for organic farming, groceries are very expensive

Encourage trees and shrubs on private lots



The second evening was set up with discussion tables where the team's work in progress on understanding streets, parks, character areas and possibilities for redevelopment was displayed. Hand drawn sketches were set out on four tables, each staffed with a member of the team. The topic focused discussions enabled residents to move from table to table to speak to a member of the team. Residents were invited to add sticky notes to the drawings to record comments on the initial ideas and to add other suggestions.

Here's an example of worksheets filled out by participants:



Photos from the public workshop for Design Studio #1.

The following is a summary of comments written on sticky notes at each one of the discussion tables:

Parks and Green Network Model

- **Community Gardens:** Emphasis on integrating community gardens between apartment buildings and in City-owned spaces to promote food growth and community engagement.
- **Bike Routes and Active Transportation:** Need for designated bike routes similar to bus routes and infrastructure promoting active transportation, inspired by Guelph, Ontario.
- **Green Spaces and Heat Mitigation:** Suggestions to prevent heat islands through green spaces instead of pavement, and implementing light/white roofs.
- **Small Parks and Open Spaces:** Calls for more small neighbourhood parks and maximizing existing green spaces for informal recreation and tree replacement.
- **Environmental Concerns:** Addressing water runoff and retention in green spaces, and proposing tree cutting bylaws to protect tree canopy.
- **Lighting and Green Corridors:** Increase in street lights, particularly on Charlotte St., and connecting green corridors for active transportation.

Demonstration of Redevelopment On Large Sites

- **Mixed-Use Development:** Encouragement of mixed-use developments without high-rises, favoring 3-4 storey buildings with commercial opportunities at street level.
- **Affordable Housing:** Addressing the lack of affordable housing, particularly for families, and supporting high-rise developments with affordable housing components.
- **Density and Growth:** Planning for growth with a focus on small local parks, good transit links, and preserving views and sunlight, rather than simply increasing density.
- **Community Services:** Highlighting the need for community services like daycare and communication about proposed changes in development nodes.
- **Architectural Considerations:** Calls to maintain neighbourhood character through exterior building materials and preserving historical corridors.

Streets and Active Transportation

- **Bike Lanes:** Demand for bike lanes, especially for east-west connections and designated cycling lanes on York Street.
- **Traffic Calming:** Need for traffic calming measures, such as stop signs and reduced speed limits (e.g., 30 km/h), and addressing high traffic areas and dangerous intersections.
- **Snow Clearance:** Concerns about snow clearance in winter, suggesting that sidewalk snow removal should be homeowners' responsibility after city clearance.
- **Public Transit:** Improvement in public transit frequency, especially for north-south connections, and 24/7 connectivity to the airport.
- **Parking Management:** Proposals to increase on-street parking to allow for more green space and reduce intensity of commercial development in certain areas.

Character/Institutional/Community Buildings

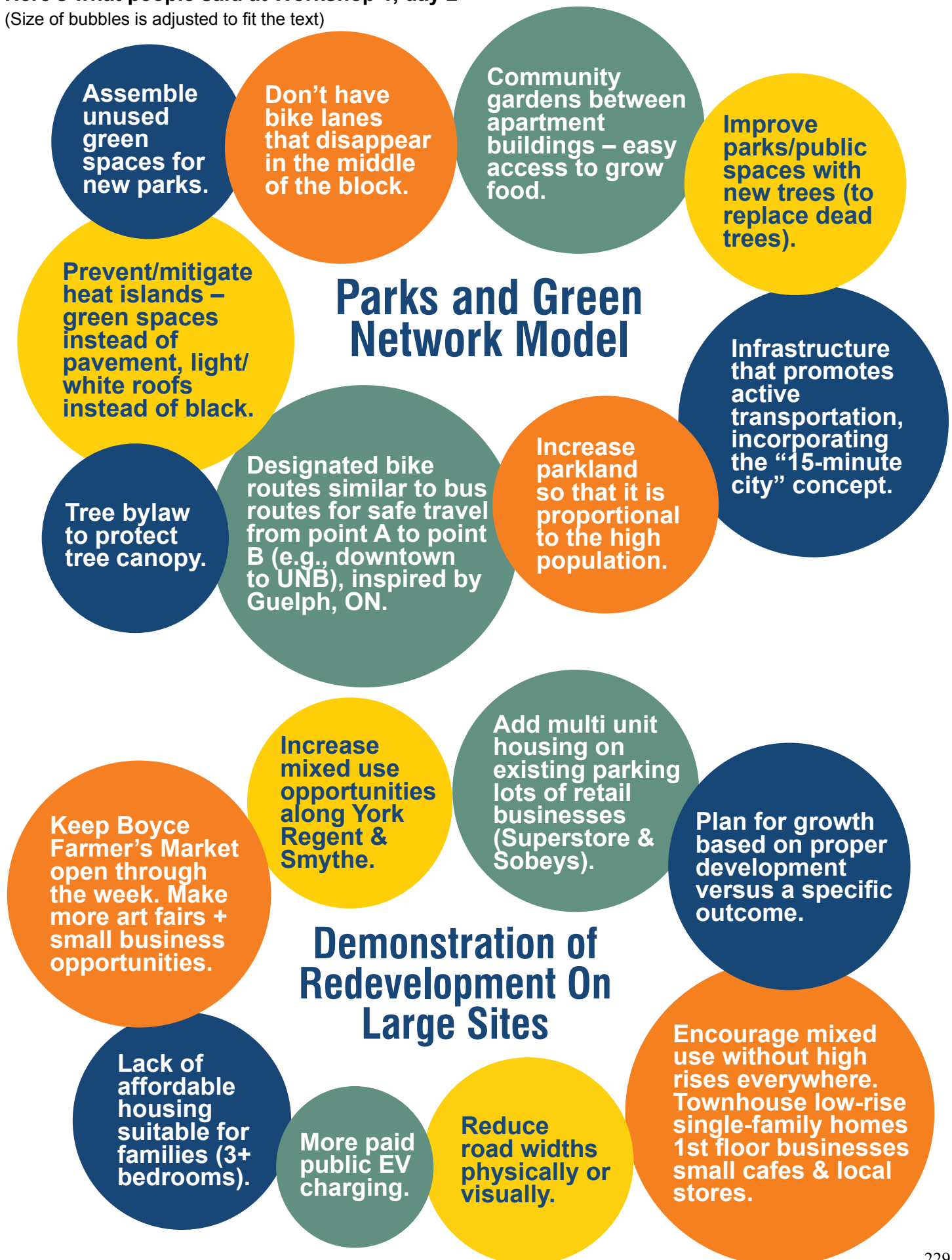
- **Low-Rise Development:** Preference for low-rise forms and maintaining small lots while increasing the number of units per lot.
- **Safety and Heritage:** Addressing safety concerns in mixed-use neighbourhoods and preserving industrial heritage and historical corridors.
- **Community Amenities:** Need for more community centres, meeting spaces, and childcare facilities, with a focus on preventing gentrification in diverse neighbourhoods.
- **Environmental Design:** Proposals for alternative driveway construction for stormwater management and preserving green spaces between houses.

The input received from Design Studio #1 provided the team direction to look at the South Core through four lenses:

- Building in the South Core
- Green the South Core
- Moving Around the South Core
- Character Areas in the South Core

Here's what people said at Workshop 1, day 2

(Size of bubbles is adjusted to fit the text)



Streets and Active Transportation

Need for bike lanes especially for east/west connections.

Close opening from Exhibition Grounds and Smythe @ Aberdeen OR Provide traffic crossing for pedestrians.

Uniform artful lighting to enclose streets, calm traffic.

Traffic considerations should be different between East and West side of Regent Street. Very different parts of town.

More crosswalks on York below Dundonald.

Stop signs needed at each intersection along Charlotte St. between Smythe + Regent; yield sign on Charlotte + Saint John.

Brunswick/Church intersection dangerous needs signal.

Need big farmers market open during weekend.

Accessibility for persons with autism – traffic circles are not great (need yes/no for any signal) – yield is not good.

Mixed income/ability housing interspersed (not in separate buildings), avoid stigma.

Increasing safety concerns in mixed use neighbourhood

Keep small lots, but increase number of units per lot

Maintain low-rise forms / converted dwelling in interior lots (not facing arterials)

Park amenities, seating, garbage cans

Look carefully – York St. is and should be a “Heritage Corridor”

Character, Community and Institutional Buildings

Need for community centre/meeting space

Affordable seniors housing with efficient accessibility to surrounding amenities

Maintain current city patterns despite growth avoid destructive urban renewal

Need for alternative driveways construction to allow for stormwater management

Student housing and rooming housing

4.0 Input Received Through Design Studio #2

June 17 and 18, 2024

The Design Studio #2 was held at the Fredericton Capital Exhibition Centre on June 17th and 18th, 2024. The purpose of this Studio was to receive input on the team's early thinking on preliminary frameworks and ideas organized around four themes: Greening the South Core, Building in the South Core, Moving around the South Core and the Character of the South Core. During Design Studio #2 there were conversations with Committees of Council, Government of New Brunswick, city staff from various departments, stakeholder groups, and the public.

4.1 Conversations with Committees of Council

Affordable Housing

Community Design and Building Height:

- Imagine Fredericton feedback: No major concerns about tall buildings.
- Emphasis on designing buildings with common areas to foster community.

Housing Initiatives and Affordable Housing:

- Support for bylaw changes to enable single-room occupancy and housing-first approaches.
- Cooperative housing efforts with organizations like the United Church.
- Existing units at Charlotte Street Christ Church and George Street, contributions from the Fredericton homeless shelter and Westmorland.
- Limited affordable housing development in the city.
- CMHC's 30% income definition for affordability; aim to provide housing 10% below market rates.
- Cooperative housing at 70% of market rent.
- Prioritizing deeply affordable housing with set targets.

- Developer incentives: inclusionary zoning, provincial rent subsidies, city housing accelerator, and land acquisition for nonprofits.

- Preference for mixed-income buildings.

Housing Market Challenges:

- Gap in mid-range housing (\$1100-\$1500/month).
- Many low-income families spend over 50% of their income on housing.

South Core Housing:

- South Core is ideal for affordable housing due to central location.
- Existing 308 apartments support affordable housing models.

Immigrant Housing Needs:

- Multicultural Association of Fredericton developing housing for new immigrants.
- Seeking financial partners for mixed-use buildings.

Innovative Housing Models:

- Intergenerational housing and home-sharing models.
- Emphasis on co-ops, nonprofit housing, and dedicating South Core areas to these models.
- University of New Brunswick is exploring affordable student housing options with high-capacity, low-cost rooms.

Age Friendly

- Represent age-friendly housing needs, focusing on affordability for those on fixed incomes.
- Emphasized proximity to services, walkability, and accessible transportation.
- Noted the lack of bus service at Stepping Stone Seniors Centre.
- Advocated for parks with equipment for all ages, more seating, and accessible sidewalks.
- Stressed the need for a mix of housing and family activities.
- Mentioned potential repurposing of George Street School and other buildings for housing.
- Highlighted successful models like the Lutheran church's combination of housing and sanctuary.

Tree Commission

- Discussed street tree infrastructure and private backyard trees.
- Highlighted the city's efforts to create more housing opportunities, affecting single-family homes.
- Addressed the presence of dead trees and the need for better tree management.
- Advocated for conservation and the creation of a comprehensive urban forest strategy.
- Stressed the importance of tree planting plans and the integration of trees into urban planning.
- Suggested strategies for tree canopy creation in light of climate change and the benefits of maintaining existing trees over replanting.



The former St. Dustan's Elementary School re-purposed for residential units.



Tree-lined street in the South Core.



Tree-lined street with a vegetated round about in the South Core.

4.2 Conversations with Stakeholder Groups

June 18, 2024

New Brunswick Coalition of Persons with Disabilities

- Represent all disabilities but not service providers.
- Accessibility often misunderstood; requires curb cuts and space for bikes, motorized scooters, and wheelchairs.
- Snow removal issues, especially at bus stops.
- Need accessible kneeling buses and a walkable city in all seasons.
- On the social inclusion committee, advocating for accessible curbs.
- Propose developing an online portal (like 511) for accessibility.
- Stress the importance of proper signage using best practices.
- Advocate for universal design in housing to be more cost-effective.
- Highlight New Brunswick's Accessibility Act, noting 35% of the population has some type of disability.

Government of New Brunswick

- George Street Middle School is slated for closing; new site design and construction to start by September 2028.
- FREX site is a front-runner for the new school, potentially with sports fields and green spaces.
- Decision on the new site expected before the end of 2024.
- Discussed other schools like Montgomery Street and Connaught Street, and the possibility of combining schools with daycares.
- Once no longer needed, school buildings are turned over to DTI and may be repurposed for housing.
- Francophone schools also face growth and may require new builds or additions.
- Zoning overlay for schools in the South Core discussed.

Inclusion Fredericton

- Advocated for mixed-income, accessible housing with social and physical accessibility.
- Cited examples like Wellington in Saint John Non-Profit Housing Organization.
- Stressed the need for safety supports to help people live independently.
- Highlighted the benefits of mixed-income models and the role of small organizations in housing.
- Emphasized inclusive housing sprinkled throughout the city.
- Suggested feasibility studies for inclusive housing models and density infill incentives for developers.
- Addressed transportation needs and the importance of accessible trails and paths for motorized vehicles.

Fredericton Heritage Trust

- Recognized the inevitability of growth and the need to manage existing building stock.
- Emphasized the importance of heritage in urban planning.
- Called for a unified approach to planning and preserving the city's heritage.
- Highlighted the role of the Trails Commission in the trail network and the need for holistic planning of parks and trees.



Valley Sentier Fredericton Trail - need for more accessible trails and paths.

4.3 One-on-one Conversations

June 18, 2024

A major landowner/developer shared comments:

- Concrete buildings are expensive and not financially feasible, with high rental costs (\$1,700/month).
- Developers need incentives, such as tax reductions for affordable housing in 5-10 storey buildings.
- High provincial and municipal taxes are a major barrier to development.
- Prefers 6-storey wood construction over concrete for cost and efficiency.
- Town staff are excellent, but high taxes and some resident attitudes are challenges. Supports current initiatives but stresses financial viability.

Postal workers also provided input:

- If a residential unit is not registered with "the government" mail will not be delivered, even if there's a mail box on the property.
- Postal workers are not required to deliver mail to a mail box not visible from a street because of safety considerations.
- Concern about the lack of crosswalks, particularly on Charlotte, especially with children walking home from school.

Design Studio #2 included an interactive public workshop in the evening of June 17, 2024 where residents were invited to listen to a presentation on the key themes and emerging strategies for the South Core, followed by table group discussions. The table group discussions were organized to receive input on the four themes: Building in the South Core, Greening the South Core, Moving around the South Core and the Character of the South Core.



Building in the South Core

Residential Infill Development

- Remove mandatory parking minimums, reduce pavement.
- Maintain heritage character, enhance tree canopy.
- Need more urban squares and green spaces.
- Developers should pay for variances.
- Avoid unattractive large apartments, ensure consistent designs.
- City Sponge Zone (rear yard green space standards): Reduce setbacks.
- intergenerational Housing: Support for developing “granny suites.”

Avenues Development Directions

- Support buildings facing streets, gradual height transitions.
- Reduce cut-through traffic, minimize parking.
- Concerns about accommodating 4,000 new residents by 2041.
- Mixed-Use: Developments should be mixed-use, accessible, and affordable.
- Building Levels: Mixed opinions on building heights.
- Accountability: Ensure builders provide green spaces and accessible housing quotas.

Rail Adjacent Development Directions

- Ensure at-grade entry, accessible van parking.
- Guarantee accessibility in commercial areas.
- Include affordable housing ratios.



Greening in the South Core

Urban Parks

- Community Gardening: Provide small vegetable plots for apartment residents.
- Queen's Square Planning: Support for relocating ball diamonds and adding trees.
- Prefer trees and flower beds over metal structures; avoid peacock designs.

Urban Squares

- Development Integration: Create urban squares around larger developments.
- Community Spaces: Increase spaces for informal gatherings and art.

Boulevards / Linear Parks

- Smythe Street: Enhance walkability, greenery, and aesthetics; make it quieter.
- Accessibility: Ensure drainage grates with small gaps and accessible rest areas.

Green Infrastructure

- Support for reusing rainwater for green spaces.
- Heat Mitigation: Use non-black pavement to reduce heat absorption.
- Climate Change: Plan for future climate conditions.

Sponge City

- Implement strategies to regenerate old trees.



Moving Around in the South Core

Parking On-Street

- Event parking for cathedral and cultural events.
- Van-accessible spaces for para-transit.
- Support for on-street parking and reduced speed limits (30 km/h).
- George Street as a major E-W street with high volume.
- Proposed bikeway on Charlotte Street and Carleton Street trail.
- Traffic calming measures: bump outs and reduced curb radii.

Trails Extended

- Extend and connect trails into downtown.
- Increase core area trails.
- Reference to Bicycle Design Manual.
- Advocate for plowing trails in winter.

Transit

- Accessible shelters with curb cuts.
- New signals at Aberdeen & Westmorland, Aberdeen & Smythe.
- Bike parking at bus hubs (Dundonald, Northumberland, Brunswick, Carleton).
- Pedestrian-focused improvements: pedestrianize Queen Street, reduce speed limits, traffic calming.
- Suggested bus routes passing by Fredericton Capital Exhibition Centre, greater frequency.
- Intersection improvements: traffic circle at Forest Hill/ Waterloo Row, cycle circle at Brunswick & Smythe.
- Support for “bike boulevards” for commuting.

Wayfinding/Accessibility

- Emphasis on color contrast, tactile signage, tactile indicators, audio signals.
- Elevated cycle lanes, trail lighting, cycle lanes outside parked cars, bicycle lights with advance green.
- Overhead traffic lights for crosswalks, full stop red signals.
- Encourage neighbourhood grocery stores and local vegetable sales.
- Additional crosswalk west of Dundonald and Carleton.
- Promote bilingual (English and French) signage for the capital city board.



Character in the South Core

Residential Character Areas

- Emphasis on redevelopment and infill in residential neighbourhoods and corridors.
- Integration of local materials to maintain the area's design character.
- Scale of entries and fenestration (arrangement of windows) considerations.
- Preference for clapboard over industrial vertical siding.
- Goal to maintain older buildings, increase density, and enhance green canopy.

Avenues Character Areas

- Use of wood shingles instead of vinyl for aesthetic consistency.
- Promotion of mixed-use developments.
- Call to build green avenues and enhance public spaces with consistent facades.
- Desire to create more enjoyable walking environments with less noise and pollution.
- Concerns about building materials, advocating for durable choices that age well (e.g., brick over white surfaces like the NB Power building).



Photos from the public workshop for Design Studio #2.

Here's what people said at the workshop

(Size of bubbles is adjusted to fit the text)

Building in the South Core

We need to ensure quotas for accessible housing. We cannot be a homogeneous inner core.

Need more urban squares with development

Great work! Keep sunlight in our streets. I like the transition of height and massing

Allow for development of intergenerational housing / "granny suites"

New buildings need green spaces. Who holds builders accountable?

If we are developing a wider green boulevard on Smythe, new builds or builds incorporating old buildings should be consistent, right now there are some ugly gaps + little canopy

Need to have a variety of building shapes & sizes that fit into current landscape... Create gathering spaces around new builds. Incorporate access to grocery stores in bottom level of large buildings

Maintaining/ respecting heritage character

35.3% of New Brunswickers have a mobility disability - important to consider in new buildings

Great ideas for reusing rainwater for green spaces

Losing a lot of our old trees in these areas – need a regeneration strategy

Spots where people who live in apartments can have a small vegetable plot

Consider non-black colour pavement to reduce heat absorption during the summer

Create more spaces for informal gatherings, art

Excited to hear of Queen's Square planning. Relocating the ball diamonds would be fantastic! And adding trees!

Greening in the South Core

Drainage grates with small gaps and accessible transitions

Tree canopy matters greatly – aesthetic + environmental

Rest areas attached but set back from main travel lane with adequate space around benches where a wheelchair can pull up beside

Moving Around in the South Core

Reduced
curb radii

Connect the
cross-town
trail!

Extend
trails into
downtown

Para-transit
– van
accessible
spaces

Could bus
routes
pass by the
FREX?

Pedestrian
Downtown -
shrink streets,
traffic calming

Colour
contrast, tactile
signage, tactile
indicators,
audio signals

Plow trails
in winter!

I would
love “bike
boulevards”
for some more
commuting
focused cycling
infrastructure

Make the
capital city
board shine!
En français
aussi

Yes to
parking on
street + 30km
reduced
speed

Encourage
neighbourhood
grocery stores,
people selling
their vegetables

Character in the South Core

Build green
avenues, fill out
the space with
consistent facades.
Add green spaces
and make walking
enjoyable. Currently
loud and dirty, no
nice landscape, lots
of cars

Wood
shingles –
no vinyl

Maintain old
buildings, add
density and
green canopy

Mixed-
use

Scale of
entries –
fenestration
– clapboard
rather than
industrial
vertical siding

Tie redevelopment
– infill in
residential
neighbourhoods
& corridors - to
integration of
local materials to
maintain design
character

5.0 Next Steps

The team is considering the full range of input received to develop the draft public realm and built form guidelines and planning policy for the South Core. Suggestions were made with respect to height, building materials and use for infill development, new buildings on the "Avenues" and on the larger development sites along the railway. There is strong support for extending trails, enhanced transit, better wayfinding and slowing traffic.

Design Studio #3 is planned for October/November 2024 where concept for the public realm and built form guidelines and planning policy will be presented and discussed with opportunities to share input.

FREDERICTON

SOUTH CORE PLAN



FREDERICTON SOUTH CORE PLAN

Summer Engagement Report

September 2024

Land Acknowledgement

We acknowledge that the City of Fredericton is situated on traditional Wolastoqey territory. The territory of the Wolastoqiyik people is recognized in the Peace and Friendship Treaties to establish an ongoing relationship of peace, friendship, and mutual respect between equal nations. The river that runs through our city is known as the Wolastoq, along which lived the Wolastoqiyik, “the people of the beautiful and bountiful river.”

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1

Introduction

The City of Fredericton is developing a new South Core Secondary Municipal Plan, aimed at re-examining how we grow while preserving the neighbourhood's community character. This Plan will provide a framework to guide the ongoing evolution of the area. Over the course of the next year, the project team will explore the conditions, features and attributes of South Core, shaping strategies for street design, land use, public space and new development. Therefore, establishing design guidelines and policies to provide direction for future change.



Current South Core Plan area boundary

Why organize summer engagement events?

Summer engagement events provided ongoing community outreach between consultant-led engagement sessions (referred to as Design Studios) and offered a casual and welcoming experience for those who wished to learn more about the South Core Plan. The summer events were specifically held in areas where people were gathering, connecting with residents both inside and outside of the Plan area boundaries. Members of City Staff and the consultant team set up booths with informative and interactive panels at 4 separate events, conducting conversations and receiving feedback from participants. Through this work, our project team was able to receive a wide range of thoughts and ideas while helping our citizens become more informed about this project - as well as planning in general.

2

Engagement Events

EVENT 1

115
Participants

Date: July 18th

Location: Garrison Night Market

The first event aimed to inform the public about the South Core Plan, its significance, timeline, and the impacts of population growth projections. It served primarily as a platform to gather community feedback on how residents envision the future of the South Core neighbourhood.

The City of Fredericton's Community and development staff set up an informational booth focused on raising awareness about the initiative of drafting a new Secondary Municipal Plan for the South Core. This included explaining these boundaries for those who were unfamiliar with this term.

A panel titled "**South Core is a neighborhood where...?**" was set up to receive public feedback. The responses were grouped and amalgamated into four main categories:

1. Recreational and Public Spaces

- There is increasing desire for third spaces, a space where people can go in their leisure time apart from work and home
- More community gardens for informal gatherings
- Better green Infrastructure

2. Urban Infrastructure and landscape

- Sensitivity towards urban landscape and promote native shrubs and flowering species
- Better street furniture like bike racks, garbage bins, street furniture and lighting

3. Active Transportation & Accessibility

- Better trail connections and reduced use of private vehicles
- Improved walkability and accessibility

4. Heritage and Future Development

- Preserve the Historic character in the vicinity
- Mixed-use residential and commercial development
- Vocal for local businesses
- Promote more community level businesses rather than supermarkets/Super Stores
- More inclusivity and sense of community



City staff talking to residents about the South Core Secondary Municipal Plan

EVENT 2

Date: August 5th, N.B Day

Location: Beaverbrook Art Gallery Green

205
Participants

The second event was formulated to ignite discussions and gather valuable opinions from citizens on their aspirations for future infill development in the South Core area of the city. It primarily focused on understanding the public perspective on connectivity and movement in the South Core vicinity.

There were four main components that guided the conversation are as under:

1. Building Construction

- Focus on how both residents and non-residents imagine the future neighbourhood, particularly regarding growth and infill.
- Emphasis on heritage preservation and the neighbourhood character that the area holds

2. Moving Around Circulation

- Need for Para Transit Coordinated System with proper ramps and curbing for accessibility, including ramps and curbs at an interval of one block
- Implementation of Safe Crossing
- Enhanced vibrancy in Queen Square
- Improvement in street infrastructure – streetlights, street furniture
- Better connection to downtown area
- Creating raised crosswalks to calm traffic

3. Greening

- Incorporating avenue of green trees for street enhancement and foliage
- Call for an increase in amenities in parks, such as dog parks
- Extension of trail connections near UNB campus
- Addition of anchor parks at different scales of neighbourhoods

4. Character Elements

- Promoting initiatives like car share and addressing the Parking issue in the neighbourhood



City staff discussing future infill development and mobility with the residents

EVENT 3

75
Participants

Date: August 10th

Location: Boyce Farmers Market

The event aimed to quantify common themes using the Dotmocracy Panel, along with examples of what future development might look like.

The eight themes below represent some of the concerns and inputs put forward by the public in previous engagements.

Through the Dotmocracy panel, the participants were asked to identify top three priorities/ concern, drawn out from the survey and additional comments were also listed.

What is most important to you? (listed from highest to lowest response rate)

1. Safe Cycling routes through the South Core

- Need for more secure infrastructure such as bike parking facility
- Separate children cycling lanes
- Requirement of bike lane design safety

Total Vote – 17, South Core Residents – 8, Outsiders – 9

2. Preserve neighbourhood character

- Maintain the contextual built scape, material significance and height

Total Vote – 14, South Core Residents – 6, Outsiders – 8

3. Enhanced Pedestrian Connections

- Better Street infrastructure (More benches on walking trails/ routes/ streets for accessibility) and Connectivity (Bus Stops)
- Connect trails with neighbourhood

Total Vote – 11, South Core Residents – 6, Non-Residents– 5

4. Enhanced public Green Space

- Need for safe, accessible green spaces
- More neighbourhood parks are required along with garden benches
- Opportunities for recreational and social spaces

Total Vote – 11, South Core Residents – 7, Outsiders – 4

5. Slow down traffic on the residential streets

- Strategies or Traffic Calming Techniques to ensure road safety for neighboring Communities.

Total Vote – 11, South Core Residents – 7, Outsiders – 4

6. More Housing: Diverse types and affordable

- Increase affordable housing module
- More housing options for Missing Middle
- Efficient Transition of Zoning & Housing typology

Total Vote – 10, South Core Residents – 6, Outsiders – 4

7. Preserve green space in rear yards

- Efficient Transition of Zoning & Housing typology

Total Vote – 8, South Core Residents – 3, Outsiders – 5

8. More Shops and Services

- Vocal for local businesses – Promote more community level businesses rather than supermarkets/ Super Stores
- Need for more Vibrancy/ More gathering Spaces
- Total Vote – 5, South Core Residents – 2, Outsiders – 3



The City Staff encouraging attendees to participate in the Dotmocracy Analysis

EVENT 4

84
Participants

Date: August 22nd

Location: Garrison Night Market

This event's primary focus was to spread awareness of the live survey for the South Core Plan, encouraging residents to share their input on key issues such as new developments, green streets, on-street parking, etc. Members of the project team handed out bookmarks with links to the survey webpage.

Through discussions with participants, some of the comments included:

- Demands for a stronger social housing model
- Better affordable housing options
- Zoning transitions from high-density infill development to low-density housing



The city's planning team actively raising awareness about the live survey on the South Core Plan

3

Analysis

The four summer engagement events offered the opportunity to discuss the planning process with residents in a casual 1-on-1 format. A majority of the participants had not yet heard of the South Core Plan, so first and foremost, this method helped bring new participants into the process. People of all ages, backgrounds, and communities attended these events, allowing our project team to hear from a wide range of experiences.

Several themes emerged from these discussions. Recognizing the current housing challenges within the city, more affordable housing was a frequently heard comment. Participants emphasized the importance of maintaining the character of the area while creating opportunities for more recreational spaces and improved urban infrastructure. There was also a strong desire for safer cycling routes, enhanced walking trails, and a reduction in the reliance of private vehicles, with a focus on making the city more walkable and accessible.

Participants in the younger age demographic (approx. 18 to 35 years old) brought up the need for more “third spaces” – public areas where people can spend their leisure time beyond work and home. These spaces are ideally universally accessible with low financial barriers. When further discussing recreational spaces, a strong preference was given to neighborhood level parks and playgrounds, both preserving and enhancing existing spaces and finding ways to incorporate new spaces where possible.

Safety concerns were shared regarding inadequate lighting on trails and certain streets. Trails are an integral part of Fredericton’s urban landscape, and residents expressed the desire to further enhance the overall system. Positive feedback was received on the focusing future high-density development towards the vacant railway adjacent land, allowing mid-rise mixed-use development along avenues (i.e. Smythe, York, and minor infill in the residential town plat area, with a suggestion to ensure appropriate transition of infill to accommodate new growth.

Overall, the engagement sessions achieved immense support and positive feedback for the initiative taken by City.

What is most important to you? | Qu'est-ce qui est le plus important pour vous?

Enhanced pedestrian connections

✓✓✓✓✓✓

More
Connect trails
w/ other
neighbourhoods
More
Plus de logements
MISSING
Middle
economy
HOUSING!
BETTER
CONNECTIVITY
(BUS STOPS)
More connected
spaces with
small shops to
help small
enterprises
More gather
spaces/
vibrancy
Preserving
what exists in
the South Core
Area & more
density on the
sub urban areas
connected by transit
More
Neighbourhood
character
specific
buildings

Connexion piétonnière

Preserve green space in rear yards

✓✓✓✓✓

Préserver les espaces verts dans les cours arrière

More housing: diverse types and affordable

✓✓✓✓✓✓✓✓

MISSING
Middle
of the Ferry

*Affordable
Housing!*

of Learning & typology

Plus de logements
plus grande et des prix abordables

More shops and services

✓✓✓✓✓

BETTER
CONNECTIVITY
(BUS STOP)

More Commercial
Spaces and
small shops to
help small
enterprises

More gather
spaces /
vibrancy

Centre de services

Preserve neighbour
character

✓✓✓✓✓✓✓✓✓✓

- Preserving what exists in the South Core Area of moose density on the sub-wilderness areas (owned by H&W)

- PMA
- Nucleic
- Chrom
- Spec
- Chem
- Bio

Préserver le caractère du quartier

Safe cycling routes through the south core

✓✓✓✓✓✓✓✓✓✓✓✓✓✓✓✓

More secure
bike infrastructure

Bike
Parking

consider bike
are design
safety.

children cycling lanes

Des pistes cyclables sûres dans le nouveau sud

Enhance public
Green space

✓✓✓✓✓

safe
accessible
spaces

More
Social
Services

more small
in parks
neighborhood
(all)

Keynotes

155
M

les es
verts publics

Slow down traffic on residential street

11/11/11

→ 5
ISS
Ab
x 5
5

Ralentir la circulation
dans les rues résid.

FREDERICTON SOUTH CORE PLAN

What We Heard Report #2

November 2024

Land Acknowledgement

We acknowledge that the City of Fredericton is situated on traditional Wolastoqiyik territory. The territory of the Wolastoqey people is recognized in the Peace and Friendship Treaties to establish an ongoing relationship of peace, friendship, and mutual respect between equal nations. The river that runs through our city is known as the Wolastoq, along which lived the Wolastoqiyik, "the people of the beautiful and bountiful river."

La ville de Fredericton est située sur le territoire traditionnel des Wolastoqiyik. Le territoire du peuple Wolastoqey est reconnu dans les traités de paix et d'amitié qui établissent une relation permanente de paix, d'amitié et de respect mutuel entre des nations égales. La rivière qui traverse notre ville est connue sous le nom de Wolastoq, le long de laquelle vivaient les Wolastoqiyik, "le peuple de la rivière belle et généreuse."

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1.0 Introduction

The City of Fredericton is developing a Secondary Municipal Plan for the South Core that will take into account recent growth in Fredericton, while maintaining the neighbourhood's community character. The South Core Plan will provide a framework to guide growth and land use within the area.

Online and in-person public engagement is integral to the South Core Secondary Municipal Plan. In April 2024, the City set up a dedicated project web page on their Engage Fredericton website. The page is the home base for the project including frequently asked questions, the project timeline, links to background information and the team's work in progress.

In person engagement is organized around three South Core Design Studios during which there are multiple forums and venues to share input on the team's work in progress. The first What We Heard Report summarizes the input we received at Design Studio #1 held on May 6 and 7, 2024 at the Boyce Farmer's Market, and Design Studio #2 held on June 17 and 18, 2024 at the Fredericton Capital Exhibition Centre. This What We Heard Report summarizes the input received at Design Studio #3 held from October 28-30, 2024.

A Design Studio is a working format where multiple members of the project team "set up shop" in a venue to work exclusively on a project for a few days. The team works in the Design Studio preparing drawings, meets with City staff, stakeholders and the public, and refines emerging ideas.



Study process:

- Phase 1 Design Studio – May 6th and 7th, 2024
- Phase 2 Design Studio – June 17th and 18th, 2024
- Phase 3 Design Studio – October 28th to 30th, 2024
- Phase 4 Plan Review – Spring 2025
- Final Plan – Summer/Fall 2025

2.0 First Nation Engagement



The City of Fredericton acknowledges it is situated in the traditional territory of the Wolastoqiyik People. Over the past several years the City has established a collaborative engagement process with the Wolastoqey Nation in New Brunswick (WNNB) as well as the Wolastoq Grand Council, as Rightsholders. Ongoing engagement with WNNB includes the Consultation Manager, Environmental Assessment Coordinator, Ethnohistorian and six Resource Development Consultative Coordinators (RDCC) who represent and report to their respective Wolastoqey Communities. Ongoing engagement with the Wolastoq Grand Council includes the Grand Chief and Grandmothers who are from various Wolastoqey Communities. The City initiated collaborative engagement with WNNB & the Grand Council in 2021 and continues to regularly meet monthly.

In addition to the monthly updates on the South Core Plan, WNNB was invited to meet directly with the Consultants. The City will continue to provide information and updates on this important project as it moves forward and seek input from the Wolastoqiyik People.

3.0 Input Received Through Design Studio #3

Oct 28-30, 2024

The Design Studio took place over the course of three days from October 28th to 30th, 2024.

Monday, Oct. 28: The first day began at City Hall with a series of working sessions with City staff, Lunch and Learn with Councillors and the Mayor, and a Public Presentation to Council reviewing the study progress, engagement so far and Design Studio #3 agenda.

Tuesday, Oct. 29: Held at the Grace Memorial Baptist Church, the second day included meetings with various stakeholder groups and two public sessions to discuss the team's emerging concepts and to gather input.

Wednesday, Oct. 30: Also at Grace Memorial, the final day included working sessions with the City's planning team, meetings with various committees of Council, and meetings with the Preservation Review Board.

Over the course of the three days, we heard from over 100 people and had many productive discussions on the emerging ideas for the South Core.

This chapter summarizes what we heard during Design Studio #3.

We heard from
100±
individuals +
stakeholders



Mixed-use building in the South Core



Small group discussion

3.1 Stakeholder Meetings

Oct 29 & 30, 2024

Multicultural Association of Fredericton

- The Centre helps 7000 newcomer clients, working with 300 community partners
- Face challenges with housing for newcomers and a lack of recreation facilities
- Currently doing a feasibility study to determine whether they can afford to build a new facility, they noted that the former exhibition lands are an ideal location
- Want to contribute positively and fit into the South Core Plan
- Want to be in the downtown hub where transit is accessible, as most of their clients don't have vehicles
- Looking to build a residential building with supportive facilities up to 50 units in 7-8 storeys max; some for newcomers, some for market housing
- Access to green space and proximity to recreation facilities are important to them

Stepping Stone Senior Centre

- The senior centre has 606 members who are 50 + years old
- Their current space is in a building owned and maintained by the City
- They've outgrown the current space and are looking for the possibility of new larger space to accommodate their programs

University of New Brunswick

- Interest in developing a rugby, track, field house, lighting and parking on their property, currently fundraising for construction
- Facilities would also be available for use by the City, high schools and elementary schools
- Current plans encroach onto Church Street
- They commit to maintaining the east-west trail access through and around their property
- Interest in coordinating plans with Queen's Square

Downtown Fredericton Inc. (BIA)

- Strong desire for a small- to mid-size open meeting space
- Retail and restaurants are strong in the South Core
- Support for intensification and infill, especially since it's within walking distance of downtown and will support the businesses
- New business owners are less concerned with the provision of parking than the more established business owners
- Support for development along the key connecting streets

George Street Middle School

- The middle school has 750 kids in grades 6-8 with 85 staff members
- Interested in the where the new school will be located
- Love their downtown location and the proximity to the Art Gallery, river trails, etc.
- Emphasized the importance of balancing walkability with the recognition that many people work outside of the downtown area
- Want to make sure the landscape and urban parks are protected and planned for
- Concerned about at-grade uses, questioning the demand for on-street commercial
- Concerned about the location of some types of health services relative to schools

Preservation Review Board

- Support for the direction of the work
- Ensure the guidelines place special emphasis on the wide boulevard along Smythe
- Interested in the guidelines for the open space around the train station and how the City will get the parks and open space needed
- Expressed that it would be helpful for the Preservation Review Board to have sample prototypes for additional dwelling units
- Interested in an inventory of heritage buildings on the corridors

Affordable Housing Committee

- General support for the direction of the work

Ignite Fredericton

- Regional service commission, cluster of innovative tech companies
- Expressed that Fredericton is one of seven smartest communities in the world through the Intelligent Community Forum
- Interested in where office space will be permitted



Stakeholder discussions

Tree Commission

- Face challenges with the predominance of rental units in the South Core where building owners don't prioritize greening as much
- Support for the idea of the "Sponge" area, concerned about what happens when a tree dies
- Seek to protect the existing features from further erosion, proposing management measures to promote and maintain naturalization
- Support for allowing for more housing
- Assist City's planning staff to redevelop landscape standards
- The neighborhoods' character is sensitive to change; it was noted that street parking is unfamiliar to residents in Fredericton, and shifting their perspective may be challenging
- In line with the urban forest strategy in terms of connectivity, increasing greenery with a variety of plants and maturity of plants

3.2 What We Heard from Public Workshops

Oct 29, 2024

Design Studio #3 included two public sessions on October 29, 2024 between 2:30 - 4:30 pm and 6:30 - 8:30 pm. Approximately 85 people attended.

Each session began with a presentation to summarize what we've heard so far, the four lenses, and concepts for infill and intensification to inform the design guidelines and the Secondary Municipal Plan.

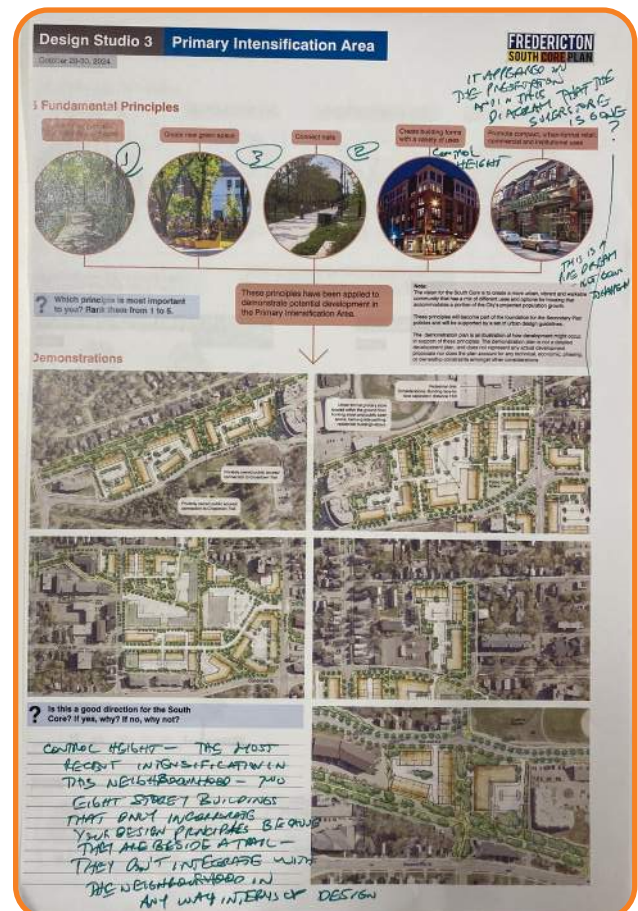
Worksheets were provided and people were asked to provide feedback on the fundamental principles, demonstration plans and sample street cross sections.

85
participants

Photos from the public workshop



Sample of a completed worksheet



4 Fundamental Principles

Design buildings with similar height and massing



Locate buildings to frame the street



Maintain the rear landscape areas/ 'sponge'



Promote complementary building design



? Which principle is most important to you? Rank them from 1 to 4.

These principles have been applied to demonstrate potential development in the Neighbourhood Infill Area.

Demonstration



? Is this a good direction for the South Core? If yes, why? If no, why not?

Here are the worksheets provided at the public sessions

4 Fundamental Principles

Step down building height to create a transition to adjacent neighbourhoods



These principles have been applied to demonstrate potential development in the Corridor Intensification Area.

Stacked townhouse form as transition to existing 2-story dwellings

4-story apartment on a 2.50m deep site can accommodate 1:1 parking on site (i.e. no structured parking)

15 units

Chalmers St

16th Avenue

Saunders St

Maintain front setbacks. Provide additional setbacks to existing dwelling



Here are the worksheets provided at the public sessions

Design Studio 3 Primary Intensification Area

October 28-30, 2024

FREDERICTON
SOUTH CORE PLAN

5 Fundamental Principles

Connect the street grid and make smaller blocks



Create new green space



Connect trails



Create building forms with a variety of uses



Promote compact, urban-format retail, commercial and institutional uses



? Which principle is most important to you? Rank them from 1 to 5.

These principles have been applied to demonstrate potential development in the Primary Intensification Area.

Demonstrations



? Is this a good direction for the South Core? If yes, why? If no, why not?

Here are the worksheets provided at the public sessions



Design Studio 3 Streetscapes

October 28-30, 2024

FREDERICTON
SOUTH CORE PLAN

4 Fundamental Principles

Design complete streets



Implement green infrastructure



Prioritize pedestrian safety and comfort



Adapt design based on the context



? Which principle is most important to you? Rank them from 1 to 4.

These principles have been applied to demonstrate potential changes to the streetscapes in the South Core

Neighbourhood - Charlotte Street

Existing



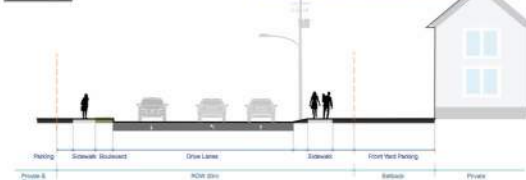
Proposed - Option 1



? Is this a good direction for streets in the Neighbourhood Intensification Area? If yes, why? If no, why not?

Corridor - Regent Street

Existing



Proposed - Option 1



? Is this a good direction for streets in the Corridor Intensification Area? If yes, why? If no, why not?

Intensification - Dundonald Street

Existing



Proposed



? Is this a good direction for streets in the Primary Intensification Area? If yes, why? If no, why not?

Here are the worksheets provided at the public sessions

3.2.1 Infill and Intensification Areas

The following is a summary of comments on the Infill and Intensification Areas worksheet, grouped under headings. Comments focused on housing, development, transportation, and green space.

Housing and Development

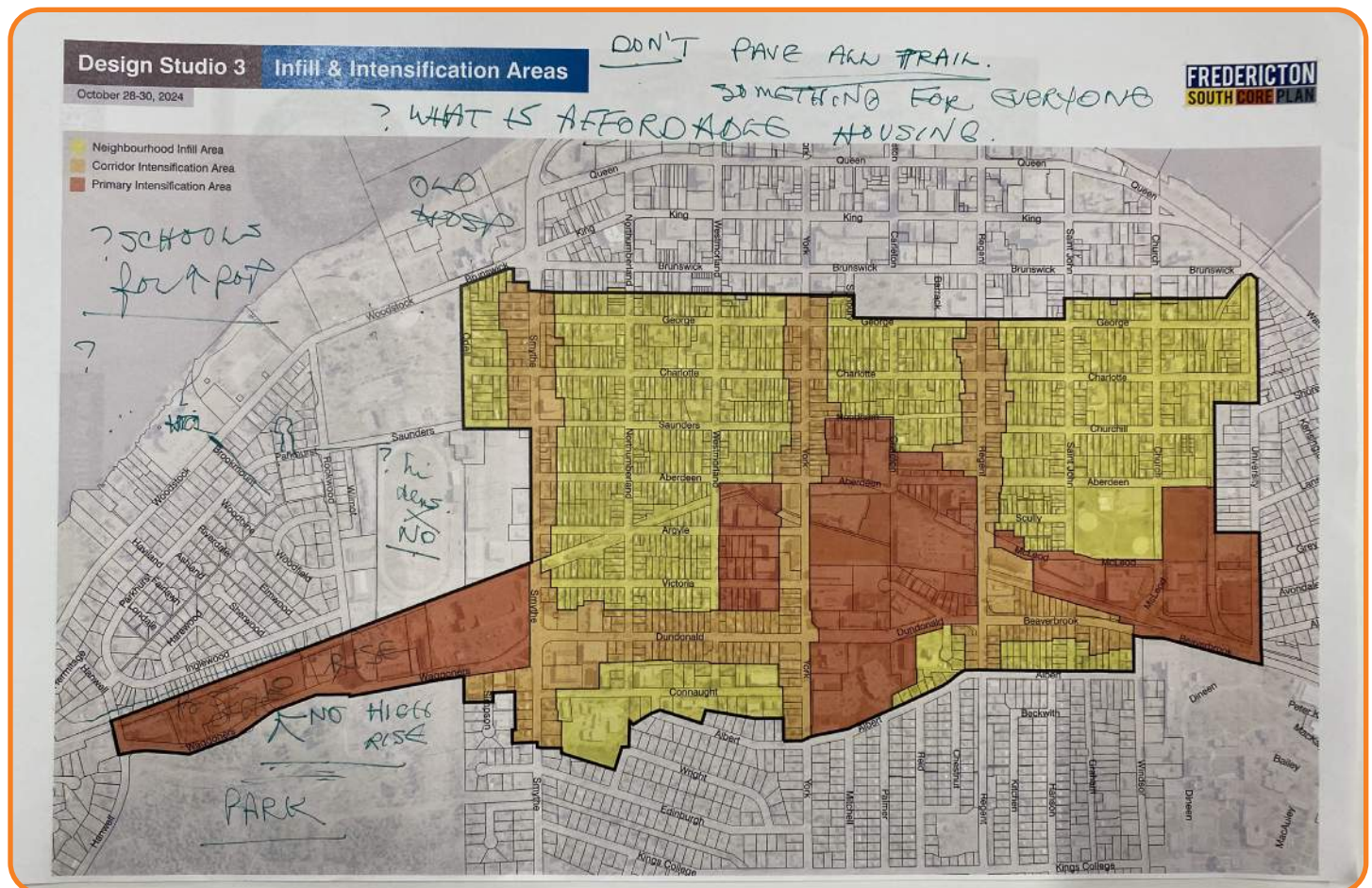
- When the South Core Plan is adopted, there should be quick staff approval for projects. Planning Advisory Committee and Council should only be rejecting in unusual cases.
- The plan for the old exhibition grounds needs to align with the South Core Plan.
- Comment stating that infrastructure on York Street will not support 1200 more people.
- Some participants disagree with higher density proposed in/ nearby lower rise neighbourhood.
- Some questioned what is defined as affordable housing.

Transportation

- Comment regarding the proposed transportation being out of scale to the city.
- Some disagree with designated bike lanes, but others believe biking is not just for leisure and should be an essential part of the transportation system.

Green Space and Trails

- The sports field associated with the school should be a preserved green space.
- Some don't want paved trails.



Sample of a worksheet from the public workshop

3.2.2 Neighbourhood Infill Area

Participants were asked to prioritize the principles below by ranking them from 1 (most important) to 4 (least important), and provide comments on the demonstration plan. The top priorities were both "Locate buildings to frame the street" and "Maintain the rear landscape areas/'sponge,'" while "Design buildings with similar height and massing" was often ranked as the #4 priority. Here's a summary of the comments from this section.

4 Fundamental Principles



Demonstration



Preservation of Neighbourhood Character and Heritage

- Maintain heritage architecture and neighbourhood "feel."
- Support for diverse architectural styles and materials that reflect the South Core's character.
- Preference for narrow, deep lots and a socio-economic mix in the area.
- Preserve green space and neighbourhood character alongside development.

Environmental Considerations

- Green or "sponge" areas are important for stormwater management and temperature control.
- Concerns about reduced green space leading to increased flood risks.
- Suggestions to balance greenery with flexibility, allowing private green spaces without strict mandates.

Community Infrastructure Needs

- Keep schools and parks walkable and accessible.
- Requests for planning new school locations to accommodate growth.
- Desire to maintain educational opportunities that connect children with the community.

Housing Diversity and Affordability

- Interest in diverse housing, including larger homes for families and affordable options.
- Requests for clarification on the definition and implementation of "affordable housing."
- Broad support for increasing urban density with planned infill, garden suites, and micro homes.

Here's some of what people said about the Neighbourhood Infill Area

(size of bubbles are adjusted to fit the text)

Yes. Keep the current neighbourhoods as they are with the character and the lined streets. Allow some modest infill, granny houses etc.

Yes, we definitely need a "sponge" for flood mitigation

No doubt this is good

Allow for a variety of styles & building materials that reflect the character of the South Core

I think that microhomes are a good option for these additions

Small dwellings will be the future

Have snow removal costs been considered?

Importance of having school in downtown/ downtown core needs to be considered

What is your definition of "affordable housing"? 30% household income?

Please keep schools walkable for this area. The enrichment opportunities are so important to tie the children to the community

Photos from the public workshop



3.2.3 Corridor Intensification Area

The top priority was "Step down building height to create a transition to adjacent neighbourhoods." At the same time, that priority was also commonly ranked as the #4 priority. Here's a summary of the comments from this section.

4 Fundamental Principles



Demonstrations



Building Height and Density

- Mixed feedback on height: support for lower buildings (2-3 storeys) near single-family homes; preference for a maximum of 4 storeys.
- High-density areas like the superstore area are appropriate for taller buildings, residential zones should have lower building heights.
- Concerns over "building wall" effect, creating heat traps, excessive wind, and potential for too many cars.
- Like the idea of concentrating density on main streets but want gradual transitions near lower-density residential zones.

Parking and Accessibility

- Calls for reducing surface parking, exploring underground or stacked parking, and discouraging excessive car use to prioritize walkability and public transit, and to preserve green space.
- Suggestions for improving bike access and safety, along with enhanced transit services to support the area's growth.

Walkability and Public Realm

- Prefer dense, walkable environment with amenities accessible within walking distance.
- Improve sidewalks for safer, more walkable streets, like avoiding single-side sidewalks in residential areas.
- Encouragement for mixed-use buildings with commercial spaces to meet the needs of a growing population.



Photo from the public workshop

Community Engagement and Implementation

- Appreciate general planning direction but questions on implementation, including regulations and enforcement.
- Request for more public input opportunities with better meeting notifications, possibly through utility bills.
- Desire for continued consultation and flexibility in design rules, especially around maintaining the neighbourhood feel and respecting existing residents' expectations.

Here's some of what people said about the Corridor Intensification Area

(size of bubbles are adjusted to fit the text)



3.2.4 Primary Intensification Area

The top priorities were "Create new green space" and "Promote compact, urban-format retail, commercial and institutional uses," while "Connect the street grid and make smaller blocks" and "Connect trails" were typically ranked as their #5 priority. The following page summarizes the comments from this section.

5 Fundamental Principles



Demonstrations



Densification and Mixed Use

- General agreement with intensification, especially incorporating mixed-use spaces and gathering areas for residents in multi-unit buildings.
- Support for infill development, pedestrian-friendly corridors, and vibrant downtown spaces to enhance economic activity and community engagement.
- Positive outlook on developing trails and connected green spaces to support walkability and recreational access.

Transportation and Accessibility

- Support for improved transit options, protected bike lanes, and accessible parking for people with disabilities.
- Requests for strategic street parking near popular areas like Odell Park to accommodate visitors.
- Interest in traffic calming measures and pedestrian-friendly street design to enhance safety and reduce reliance on cars.

Green Space and Environmental Considerations

- Strong preference for maintaining and expanding green spaces, including community gardens and shade trees, to address climate resilience and quality of life.
- Concerns about maintaining old trees and avoiding planting tall trees under powerlines.
- Emphasis on planning for green spaces and climate change impacts, especially in areas prone to flooding.

Height and Integration

- Mixed opinions on building heights, with calls for height limits that align with neighbourhood aesthetics and design principles.
- Encouragement to integrate new developments with existing neighbourhoods, ensuring they fit visually and functionally within the community.
- Concern about excessive commercial development in some residential areas, with a preference to maintain a neighborhood feel over commercial expansion.

Here's some of what people said about the Primary Intensification Area

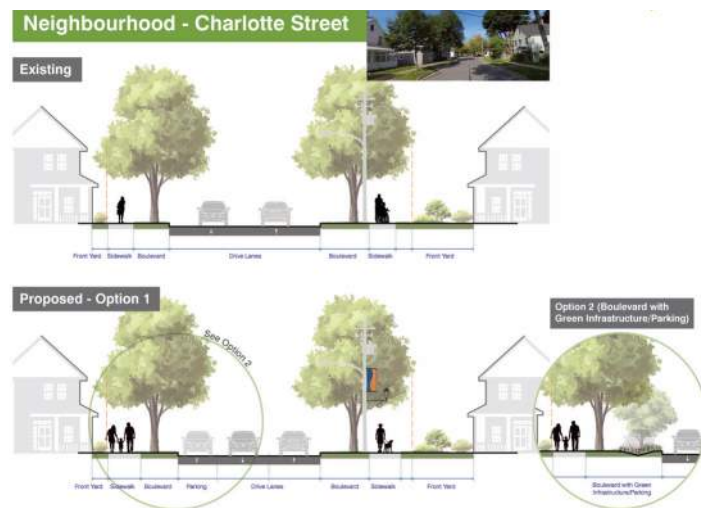
(size of bubbles are adjusted to fit the text)



3.2.5 Streetscapes

The most consistently ranked as a #1 priority was "Design complete streets." People mostly ranked "Adapt design based on the context" as their #4 priority. The following summarizes the comments on this worksheet page.

4 Fundamental Principles



Neighbourhood – Charlotte Street

- Support for complete streets with bike lanes, separated where possible from pedestrian areas, noted that bike lanes weren't visible in the current examples.
- Mixed opinions on on-street parking: appreciated for accessibility and visitor parking, but concerns about snow removal and cost implications.
- Preference for slowing traffic, suggestions to reduce speeds to 30 km/h, narrow roadways, and consider one-way streets to manage flow and add parking.
- Support for green infrastructure and maintaining community character, attention to upkeep of green spaces and accommodating garbage/recycling services.
- Want pedestrian-level lighting for safety, separation of cyclists and pedestrians for improved usability.

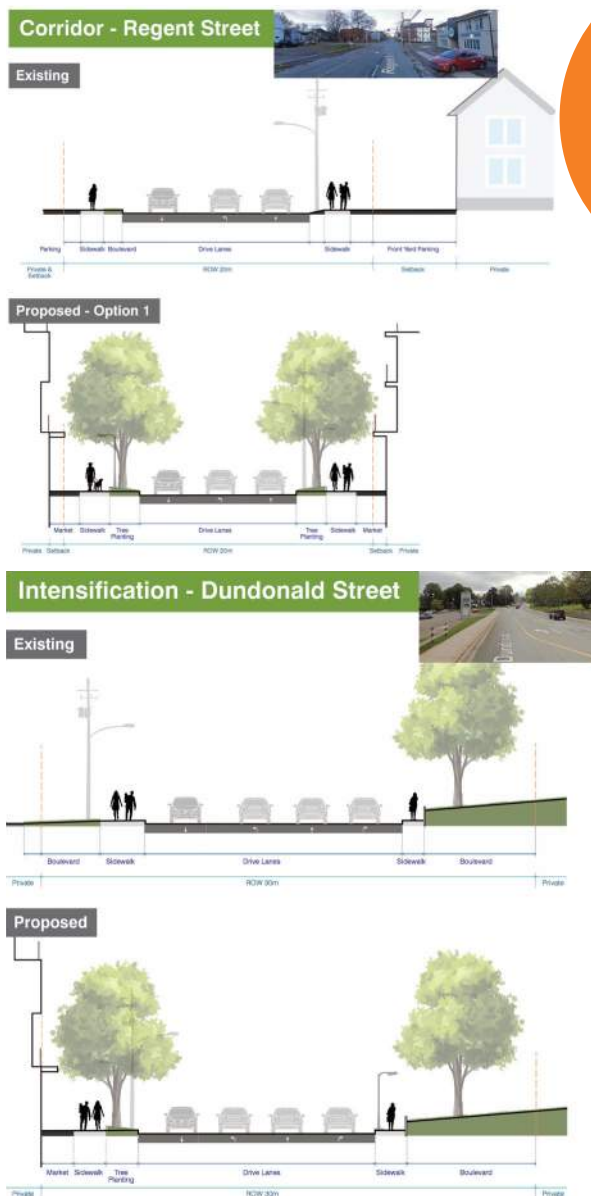
Like more street parking but ensure more cross-town N/S & E/W cycling routes.

Yes – I'll pick on the principle of green infrastructure – critical for climate adaptation

Love the complete street with separated bike lane. I would like to live in a city with this arrangement on every street.

Separate bicycles and (most definitely) motorized scooters from pedestrians

Yes – lighting needs to be pedestrian level for safety/light in terms of "greening shadows"



Corridor – Regent Street

- General agreement with the direction for Regent Street, with calls to widen sidewalks and prioritize a pedestrian-friendly urban feel.
- Support for on-street parking year-round, with winter maintenance considerations; shifting parking to side streets was also mentioned.
- Suggestions to improve transit, including bus lanes, queue jumps, and enhanced bike lane infrastructure.
- Desire greening and tree-planting along the street to mitigate heat, support pedestrian safety, and enhance the boulevard aesthetic.
- Concerns about traffic speed, with calls to introduce more stop signs to improve walkability and safety near businesses.

Intensification – Dundonald Street

- Reduce car dependency through better transit routes and active transportation options, including bike infrastructure and bus space allocation.
- Green buffers between sidewalks and roads to enhance walkability, reduce snow accumulation, and increase safety.
- Interest in market or commercial spaces that align with a walkable, neighbourhood-friendly design, with services accessible to new residents.
- Calls for extensive tree-planting along both sides of the street to maintain community character and provide shade, along with dedicated bike lanes and parking for cyclists.
- Agreement with the general direction, with additional suggestions for elevated cycle paths and barriers between streets and sidewalks for improved safety.

We love on-street parking all year long. Winter should not be considered a barrier.

No bike lane?

Good direction, but I think it needs to go further for Regent St. Sidewalks should be much wider. Push the market back and allow more of a pedestrian-friendly urban street

No, doesn't consider transit needs long-term, queue jumps, reserved lanes, etc. will be needed in the future

Bus infrastructure needed on Dundonald; should reserve more space for the future

I don't see cycling infrastructure being considered. Parking for bikes is also essential.

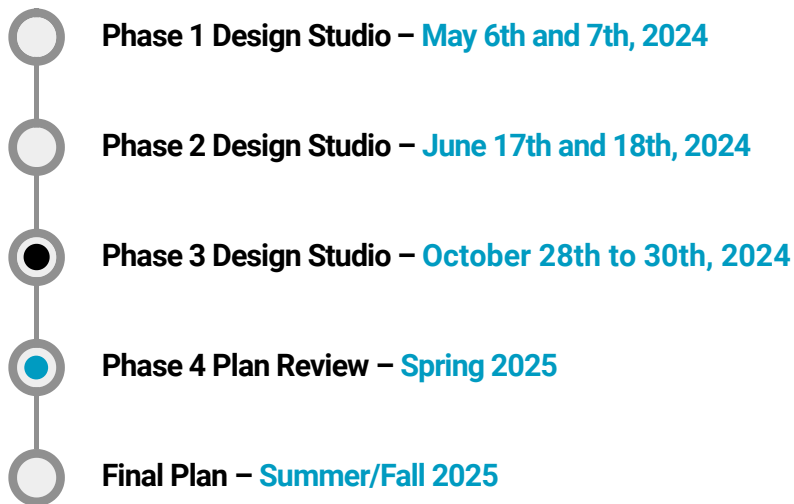
Yes, maintains community

Where possible, yes. I would love more services that I, and these 6000 new people can walk to.

The market concept is appealing

4.0 Next Steps

The team is looking at all the input received. In the next few months, the team will draft the urban design guidelines and the policy for the Secondary Municipal Plan. We continue to welcome public input through the project web page.



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FREDERICTON

SOUTH CORE PLAN



FREDERICTON SOUTH CORE PLAN

Survey Results

October 2024

 The Planning
Partnership

Trace Planning & Design

CBCL

ERA Architects

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Introduction

Aug - Sept, 2024

An online survey was launched on August 9th, 2024, and ran until September 27th, 2024. This was one of several methods to better understand the priorities of residents and to seek their input on specific ideas being explored for the South Core. There was a total of 693 participants providing 16,594 data points. The survey was posted on the Engage Fredericton South Core project web page, and was promoted via social media, digital newsletters, and in-person engagement events.



Who We Heard From

Do you live in the South Core?

yes	60 %
no	40 %

How old are you?

younger than 18 years old	0.2%
19-30 years old	19%
31-45 years old	34 %
46-65 years old	33%
over 66 years old	13%

What is your household structure?

single	22%
couple	41%
family with young children	18%
family with teenagers/young adults living at home	15%
single living with room mates	4%

Do you own/run a business in the South Core?

yes	12%
no	88%

Where do you live in the South Core?

house	64%
in an apartment in a house	10%
in a multi building (apartment or condo)	26%



What's most important to you?

The first part of the survey sought input in key considerations when thinking about planning and urban design in the South Core. The following are the results when respondents were asked to choose their top three priorities from a list of eight.

Rank 1

More housing

One of the key drivers of change is the demand for housing across the entire city. The South Core includes vacant properties and large sites that could accommodate new housing, and single residential lots that could accommodate building expansions for additional dwelling units such as a garden suite.



Rank 2

Safe cycling routes

Bike lanes are identified on Smythe Street, York Street, and Church Street as dedicated or signed lanes. The Cross Town Trail provides partial connections across the South Core. Construction is expected to begin in 2024 to complete the missing link in the Cross Town Trail in the South Core, providing a safe cycling route.



Rank 3

Enhance pedestrian connections

Enhanced pedestrian connections by improving and maintaining sidewalks, trails and intersection crossings will help to ensure pedestrian safety across the South Core.



Rank 4

Preserve neighbourhood character

The character of the South Core reflects its evolution over time. The buildings, streetscapes and landscapes are key defining features of the neighbourhood.



Rank 5

Slow traffic down

Enhanced streetscape, on-street parking, and travel speeds reduced to 30 km/hr can help to slow down traffic on the residential streets.



Rank 6

Enhance public green space

Wilmot Park, Odell Park, and Queen's Square are key parks in and adjacent to the South Core. Through redevelopment of the larger parcels of land, it will be important to include small parks to offer places to play and socialize. Additional street trees and preserved boulevards are important components of public green space.



Rank 7

More shops and services

The South Core has large commercial areas such as the Sobeys grocery store and small stores, restaurants, and services. Mixed-use neighbourhoods offer great places to live and proximity to shops and services within a short walk. The Secondary Municipal Plan could allow more shops and services, particularly on Smythe, York, and Regent Streets, and integrate them with residential uses in the larger areas that may be redeveloped.



Rank 8

Preserve green space in rear yards

Preserve the green space in rear yards of the residences in the neighbourhoods to allow for stormwater infiltration and to maintain the landscape character of the South Core.



Ideas being explored for the South Core

The second part of the survey sought input on Ideas being explored for the South Core. Questions were grouped under the topics of Character in the South Core, Building in the South Core, Greening in the South Core and Moving Around in the South Core. The survey asked “thumbs up” (yes) or “thumbs down” (no) whether the idea was a good direction to explore. The results of the percentage of the respondents replying “thumbs up” follow.

Character in the South Core

Compatible Infill Development

Encourage infill development in the Residential Character Area (green shaded area on map) that is compatible in scale with adjacent buildings.

👍 78%



Four to Eight Storey Buildings

Promote buildings that front, face, and feature the street in 4—to 8-storey buildings along the Avenues Character Area—Smythe, York, Regent Streets, and Dundonald/Beaverbrook (yellow shaded areas on the map).

👍 78%



Higher Density Development

Focus higher density development in the larger areas along the old rail corridor - Rail Adjacent Lands (blue shaded area on the map).

👍 65%



Building in the South Core - Residential Area and Avenues



Additions

Locate additions to existing dwellings behind the main building wall; new additions should be smaller than the principal dwelling.



Alignment of New Buildings

Locate new buildings to align with neighboring houses to create a consistent streetscape. The purple-shaded area in the photos illustrates a building located along a consistent setback from the street.



Form of New Buildings

Design new dwellings (white building in the photo above) with a form that is similar to the buildings in the immediate area.



New Buildings along the Avenues

Locate new buildings on the Avenues at the street edge with interesting facades to enhance the pedestrian area of the street and frame the street.

Building in the South Core - Rail Adjacent Lands



👍 91%

Breaking up Large Development blocks

Encourage new development with smaller blocks and new street connections to existing neighbourhoods.



👍 96%

Pedestrian links through Development Blocks

Provide pedestrian connections and links through development blocks to parks and trails.



👍 93%

Framing Streets, Parks and Trails

Locate and orient buildings to frame existing and new streets as well as parks and trails.



👍 95%

Incorporating Retail Uses

Provide opportunities for retail and commercial uses on the ground floor of buildings.

Greening in the South Core



Preserved Treed Boulevards

Preserve the treed boulevards on streets in the Residential Area.



Key East West Streets for Greening

Identify key east west streets as priorities for green infrastructure – bioswales, ground covers, shrubs, trees.



Green Space along the Cross Town Trail

The Cross Town Trail should be designed as a green ribbon with nodes of public green space interspersed along its length.



Tree Planting in New Development

Encourage the replacement of existing trees that have been removed for development with new trees planted in conditions that will support long-term health.



Rear Yards

Protect and preserve the existing rear yards as green space and to serve an ecological, stormwater management function for the neighbourhood.

Moving Around in the South Core



On-street parking

Permit overnight on-street parking on all streets to support infill housing, enable the preservation of green space on lots, and provide traffic calming.



Traffic calming

Use planted bumpouts, traffic islands and other methods to slow traffic and make shorter and safer pedestrian crossings.



Cycling routes

Provide a complete, safe, and interconnected network of bike routes, including dedicated on-street bike lanes and off-street trails.



Pedestrian/cyclist crossing safety improvements

Enhance pedestrian/cyclist crossings at major intersections.



Wayfinding and signage

Add wayfinding and signage to identify key public facilities, trails, features, etc.

FREDERICTON

SOUTH CORE PLAN



FREDERICTON SOUTH CORE PLAN

Technical Background Report

October 2024

 The Planning
Partnership

Trace Planning & Design

CBCL

ERA Architects

Fredericton

Land Acknowledgment

We acknowledge that the City of Fredericton is situated on traditional Wolastoqey territory. The territory of the Wolastoqiyik people is recognized in the Peace and Friendship Treaties to establish an ongoing relationship of peace, friendship, and mutual respect between equal nations. The river that runs through our city is known as the Wolastoq, along which lived the Wolastoqiyik, “the people of the beautiful and bountiful river”.



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Map of the South Core Secondary Municipal Plan Area Boundary

1 Introduction

The City of Fredericton is growing. As the provincial capital of New Brunswick, the city has benefited from a stable economy, strong institutions, and continues to offer a high quality of life for its citizens. In turn, it has attracted new residents from across Canada and many parts of the world. By 2051, Fredericton's population is projected to increase to approximately 114,000. To ensure growth is accommodated in a compact, efficient, and sustainable manner, intensification of the city is a major focus, where 25% of this growth is expected to occur.

New Secondary Municipal Plans for the City Centre and North Core Main Street have been prepared, leaving the South Core as a key area without an up-to-date planning document. It is a mature, diverse, and historic area that stitches together the many urban communities south of the Saint John River (traditionally known as the Wolastoq). The Residential Town Plat Secondary Municipal Plan has guided most of the South Core area for over twenty years but does not address the current challenges or match the vision of the Imagine Fredericton Municipal Plan. A new plan is crucial to ensuring appropriate built form, sustainable density, and a healthy street environment. The following subsection outlines the key drivers for a new Secondary Municipal Plan. A new South Core Plan will provide a detailed framework for new development, including public realm and built form design guidelines, direction for access and circulation, land use distribution, heights and densities, and servicing requirements.

South Core Plan Area

The map on the facing page illustrates the South Core Secondary Municipal Plan Area (Plan Area). The Plan Area is outlined in red. The Plan Area is irregularly shaped and generally extends from George Street on the north to Albert Street on the south and from Wilmot Park on the west to University Street on the east.

About This Report

The *Community Planning Act (SNB 2017, c19)* sets out the regulatory requirement for municipalities to provide a report summarizing the present or future economic, social or physical conditions of the municipality as a basis for a Secondary Municipal Plan. This Technical Background Report provides the foundation for the South Core Secondary Plan, providing a high-level background and context for the Plan.

The Report is broken down into five main sections:

- Land Use and Built Form
- Heritage in the South Core
- Public Realm
- Transportation and Municipal Services
- Key Themes

The Appendix includes an overview of the population characteristics of the South Core including age, household size, education, income, employment. It also provides statistics on the role of active transportation and transit for South Core residents and housing type.

1.1 History of the South Core

Wolastoq: The Beautiful River (c. 10,000 BCE–Present)

Fredericton is located on the edge of the Saint John River — also known as Wolastoq/Walastokw or “the beautiful river” — which flows through New Brunswick from its headwaters in Quebec and northern Maine to the Bay of Fundy. Formed after the end of the last Ice Age, Wolastoq and its river valley are the territory of the Wolastoqiyik (Maliseet) people, whose traditional ways of life and economies were deeply connected to the river and its tributaries in the pre-contact period. Although these relationships were disrupted with the arrival of European settlers, the river remains an integral part of the Wolastoqiyik’s cultural heritage.

According to archaeological evidence and oral tradition, the Fredericton area has been occupied by Indigenous Peoples, including ancestors of the Wolastoqiyik, for at least 12,000 years. For thousands of years, the Wolastoqiyik followed a seasonal cycle of movement and resource harvesting along the river, which supported settlements, fisheries, transportation networks, and eventually agriculture. One of the most significant Wolastoqiyik villages was located at Ekwpahak, or “the end of the tide”, approximately 10 kilometres west of present-day Fredericton, an area that plays a central role in the Wolastoq creation story.

Ekwpahak was an important administrative and ceremonial centre, serving as the site of “annual gatherings, councils, and celebrations” (Nicholas, 2022, p.11). While there are no known pre-contact archaeological sites in the South Core, the presence of other pre-contact sites along the river’s edge reveals a long history of Indigenous land use and occupation in the area.



Undated archival photograph showing a group of Wolastoqiyik men building a birch bark canoe at a reserve on the north side of the Saint John River, north of Fredericton (Provincial Archives of New Brunswick, P5-381)

Acadian Settlement (late 1600s to Mid 1700s)

In the late 17th century, amid a power struggle between the English and French, the Saint John River Valley became part of the French colony of Acadia. In 1691, the governor of Acadia, Joseph Robineau de Villebon, commissioned a fort at the mouth of the Nashwaak River, which was constructed as part of a network of defences against British attacks. Known as Fort St. Joseph or Fort Nashwaak, it was located on the north side of the Saint John River, across from present-day Waterloo Row.

Although the fort was abandoned by 1700, an Acadian settlement known as Pointe Sainte-Anne (St. Anne's Point) was established on the site of present-day Fredericton in the 1730s. In 1759, as part of the Saint John River Campaign and expulsion of the Acadians, the village of Pointe Sainte-Anne was destroyed by the British, and many Acadians were massacred; the nearby village of Ekwpahak was also burned around this time. Following the Saint John River Campaign, the area came under British control.



1758 painting by Thomas Davies titled A View of the Plundering and Burning of the City of Grimross, depicting the British attack on the Acadian settlement of Grimross at present-day Gagetown, during the Saint John River Campaign (National Gallery of Canada)

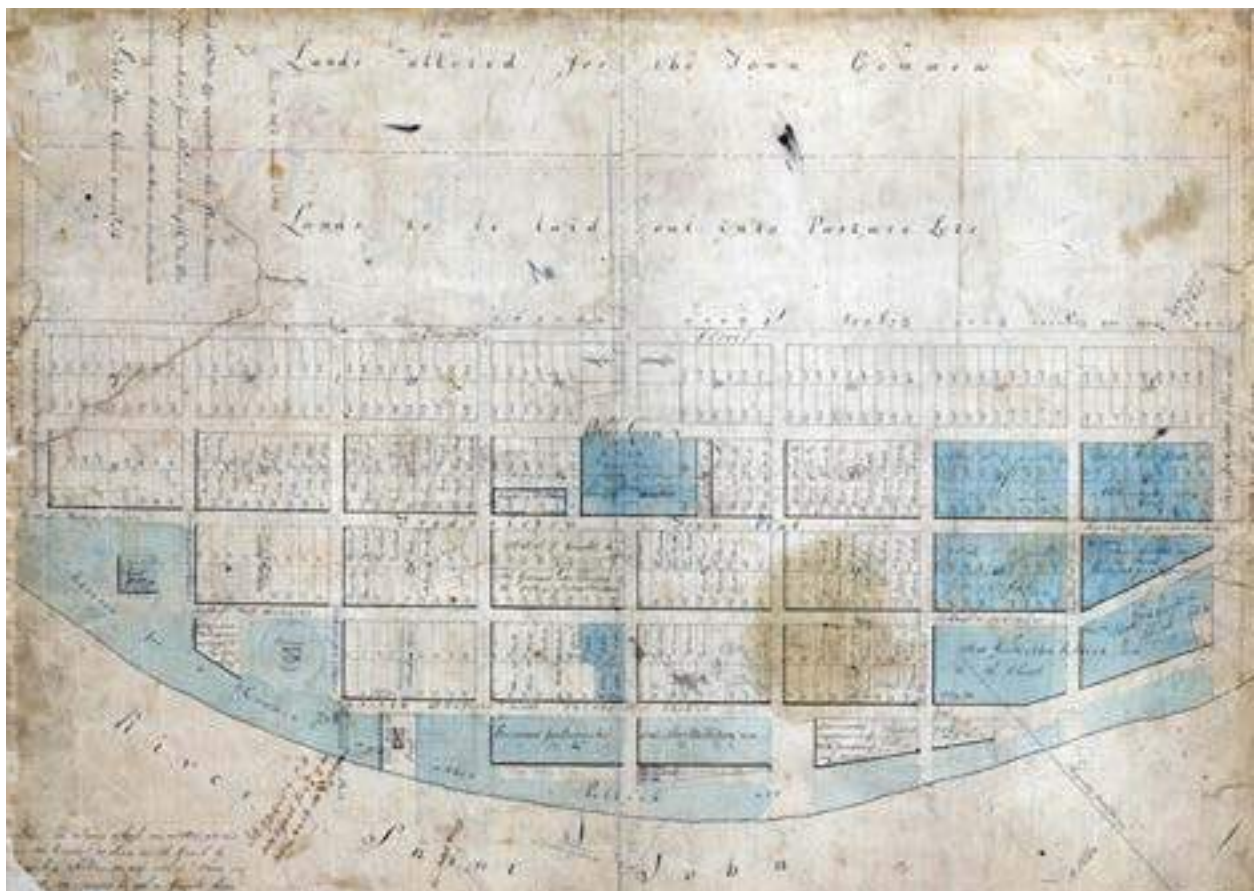
Loyalist Settlement and Development of the Town Plat (Late 1700s–Early 1900s)

The modern history of Fredericton began in 1783 with the arrival of thousands of Loyalists at the end of the American Revolutionary War, which led to the formation of the colony of New Brunswick the following year. The colony's first governor, Colonel Thomas Carleton, selected St. Anne's Point as the site of its new capital in 1785 due to its protected inland location, renaming the settlement "Fredericktown" in honour of the King's son. Despite the 1763 Royal Proclamation, which stated that Indigenous Peoples held title to their land until it was ceded by a treaty, the area was never formally ceded by the Wolastoqiyik, who were displaced by the newly arrived Loyalist settlers and eventually left with small reserves.

In 1786, the Town Plat was surveyed by Captain Dugald Campbell, establishing a grid pattern for the new provincial capital, with lands set aside for government, military, and public uses. The Town Plat originally extended between the river in the north and Charlotte Street in the south; the area

south of Charlotte Street, including most of the South Core, was laid out into pasture lots and a town common. Of the existing streets in the South Core, only George and Charlotte Streets were surveyed in 1786, along with one-block sections of the intersecting north-south streets: Church, St. John, Regent, Carleton, York, Westmorland, and Northumberland.

This part of the Town Plat saw very little development during the first half of the 19th century, as it was considered the "back of town"; however, there are examples of early Loyalist and Georgian-style houses on George Street, including the Colonel Isaac Allen Cottage at 868 George Street (c. 1800), and the Rectory at 734 George Street (c. 1829). Charlotte Street, on the other hand, was not formally laid out until 1848, despite being part of the 1786 survey. During the second half of the 19th century, the Town Plat was expanded to the south as new streets were surveyed incrementally, including Needham



1788 survey of Fredericton's Town Plat, showing the lands set aside for government, military, and public uses

Street in the 1860s and 1880s, Saunders Street (extension) in 1894, and Aberdeen Street in 1895.

The proliferation of new streets in the 1890s was tied to a building boom in that decade, which resulted from a relatively large increase in Fredericton's population, creating a demand for affordable building lots. The area around Queen Square, which was established in 1896, experienced a notable increase in building activity as streets were extended in the park's vicinity.

In response to the burgeoning population, new churches and schools were also constructed throughout the neighbourhood during the latter part of the 19th century and early part of the 20th century, typically using brick and/or stone as opposed to wood, which was the predominant material for residential structures. The prevalence of wood as a construction material can be attributed to the influence of architectural traditions from New England, as well as the availability and affordability of wood in the region.

Throughout much of the 19th and early 20th centuries, the most common residential building type and style was the detached house in the Classical Revival style (front gable plan), which was well suited to the neighbourhood's narrow urban lots. Similar to earlier Loyalist architecture, this style was imported from the United States and emerged out of the Greek Revival movement, which greatly influenced American architectural styles in the first half of the 19th century. The style was interpreted in various ways by local builders, such as William Scarr, who built approximately 10

houses on Saunders and Westmorland Streets between 1895 and 1901, combining elements of the Classical Revival and Queen Anne Revival styles.

Concurrent with the expansion of the Town Plat, the southern section of the South Core emerged as a railway and industrial district during the late 19th century, beginning with the arrival of the railway at York Street south of Aberdeen Street in the late 1860s. The railway spurred industrial development on adjacent lands, particularly around the turn of the century, when the Hart Boot and Shoe Company, John Palmer Company, and Chestnut Canoe Company all built large factories near the rail yards. It is likely that Needham Street was developed in connection with the railway, given its date of survey (1860s), proximity to the rail yards, and naming after Mayor William Needham, who was instrumental in determining the site of the station.



Undated panorama view of Fredericton, likely from the late 19th or early 20th century, looking north from the hill above Dundonald Street (Provincial Archives of New Brunswick, P5-15-C).

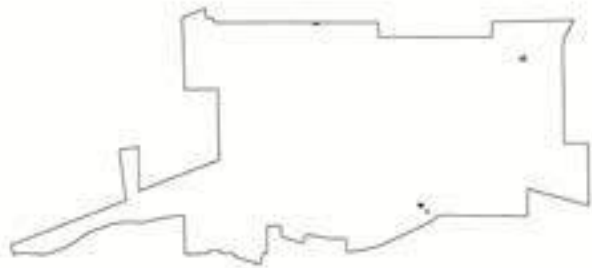


c. 1870s photograph showing an engine of the Fredericton Branch Railway at the York Street crossing, with the Exhibition Building in the background (Provincial Archives of New Brunswick, P5-373)

From Edge to Core (Mid-1900s–Present)

For more than a century, the area that would become the South Core was regarded as the “back of town”, forming the southern edge of the city. Even with the survey of new streets in the first decades of the 20th century, including Beaverbrook Street in 1935, the area beyond the South Core remained largely rural in character. In the post-war period, as new automobile-oriented suburbs were established on the outskirts of Fredericton, the South Core transitioned from being the “back of town” to an inner-city neighbourhood.

As families relocated to the suburbs, many of the houses in the residential section of the South Core were converted from single-family homes into multi-unit apartments, often with rear additions and other alterations. Small-scale infill developments, such as walk-up apartments, were also constructed during this period through the consolidation of smaller lots. Beginning in the latter part of the 20th century, larger low-rise apartment buildings were constructed on brownfield sites in the former railway lands, following the removal of the railway lines and de-industrialization of the district.



1825



1850



1875



1900



1925



1950



1975



2000

Figure-ground evolution of the South Core between 1825 and 2000 (ERA, 2024; City of Fredericton Open Data).

1.2 Drivers of Change

The South Core has been evolving for hundreds of years. Today, the area is diverse in its character, uses and built form. Today, population growth, housing affordability and escalating costs of living are driving change in the South Core necessitating the preparation of an updated Secondary Municipal Plan.

Population Growth

In 2017 the City of Fredericton released a Growth Strategy that projected a population growth of approximately 92,000 people by 2041, up from 60,000 in 2016. Since then, new projections show that there is likely to be over 114,000 people in Fredericton by 2051. The City's Growth Strategy aims for a quarter of the city's population growth to be settled in the Urban Core, a larger planning area that includes the South Core. Under the Municipal Plan, areas within the South Core Secondary Plan Plan Area (the Plan Area) are designated as either Established Neighbourhood or South Core. Both designations allow for some intensification primarily through infill and encourage densification along major transportation corridors. Development is currently occurring within the Plan Area and more will be needed to meet the city's goal of accommodating a quarter of new residents within the existing Urban Core.

Migration and immigration are predicted to be the largest drivers of population growth within the city. The city's growing number of institutional and commercial jobs are attractive reasons to move to Fredericton. Much of the institutional and commercial job growth is likely to occur within the post-secondary education campuses and the downtown core which are adjacently located to the Plan Area. People often prefer to live in proximity to where they work to reduce time spent commuting, as such, housing within the South Core will continue to be desirable for many.

Adjacent to the Plan Area is the New Brunswick Provincial Exhibition with a secondary plan that describes its future. The exhibition grounds will continue to operate, however, it will also contain new residential and commercial developments, new parkland, and potentially a new school.

Development on the exhibition grounds will also affect development in the Plan Area as it will provide recreational and employment opportunities for residents. It may also influence the density of future projects along the border of the proposed Plan Area emulating the density envisioned for the exhibition grounds.

Housing Affordability

The rental vacancy rate in Fredericton as of October 2022 was 2.1% (CMHC, 2023). Because of the low vacancy rate and the rapid increase in population there is a large demand for housing in Fredericton and competition for housing is high. Between 2021 to 2022, Fredericton had the highest increase in average rent among the studied municipalities in New Brunswick at a 9% increase (CMHC, 2023). There are few vacant properties within the Plan Area meaning that most new development within the area will take place through infill. Infill development can take multiple forms, including:

- through the addition of additional dwelling units on an existing lot such as a garden suite, basement apartment, or in-law suite,
- the construction of a new dwelling on a vacant lot within an established neighbourhood,
- replacement of the existing dwelling on a lot with a new building containing more dwelling units.

The proposed Plan Area encompasses much of the Town Plat, which is a unique and important neighbourhood in the city due to its history and distinctive urban form. New developments and redevelopments in the area will need to be sensitive to the surrounding built form and use similar buildings materials and design elements to complement the existing built form and preserve the character of the neighbourhood. Future plans for the city include a number of Zoning By-law Amendments that will increase the amount of affordable housing in the city. These amendments will have direct impacts on the proposed study as they seek to increase densities and encourage

infill development. The amendments will increase the number of units permitted by right on many lots within the proposed Plan Area in addition to easing zoning requirements for additional units. With the new amendments it will become easier for property owners to increase the number of dwelling units on their lot through basement suites, garden suites and in-law suites. Parking requirements are also to be adjusted, which will increase the potential for additional housing within the proposed Plan Area. Approximately 75% of residents within the Plan Area are renters, increasing the number of units available within this area will offer residents more choice in accommodations while also housing new residents

Escalating Living Costs

Living costs across the country have been increasing year over year, which have been exacerbated by the COVID-19 pandemic and the impacts have deepened further since. Disruptions to global supply chains due to public health restrictions contributed to increased costs of many goods, both at the consumer and production levels. Additionally, the pandemic and post-pandemic periods saw unprecedented population growth, especially in Atlantic Canada, which resulted in the housing supply not being able to meet demand. These changes at the global and national level had profound impacts, especially on individuals' living costs.

Residents in Fredericton have not been immune to these changes. The living wage is different from the minimum wage (\$15.30 as of March 2024), as it is a calculation of the hourly wage that a worker should earn to support a household of four with two working adults to meet their basic needs, and to live comfortably and with dignity. In 2023, the living wage in Fredericton increased to \$24.50, up \$1.05 from 2022, with housing being the highest expense in a household's budget (27%), followed by food (19%) and childcare (17%).

The increased costs of living can also be seen in the inflation rate, which measures the change in costs for basic goods and services. At its highest, the inflation rate in Canada, reached 8.1% in

June 2022. Although inflation has been declining since, this might not be completely reflected in the prices that people pay day-to-day. To prevent the inflation rate from increasing further, the Bank of Canada gradually increased its key interest rate from 1% in June 2022 to its current rate of 5% (as of March 2024). This means that it has become more expensive to borrow money, resulting in higher mortgages and higher financing costs for construction, with the costs being bear by both homeowners and renters.

Even though demand for construction is high, the residential sector has been seeing a slower pace of growth than expected as it has faced challenges across the country due to a shortage in skilled labour, rising material prices and high interest rates. This subsequently leads to increases in construction prices and housing becoming less affordable. Intensification is a way to create housing that is more affordable, especially as the overall costs associated with building a single-detached house is higher per unit than building a multi-unit dwelling. The City's Growth Strategy identified the South Core as an area for further intensification due to its proximity to Fredericton's downtown.



2 Land Use and Built Form

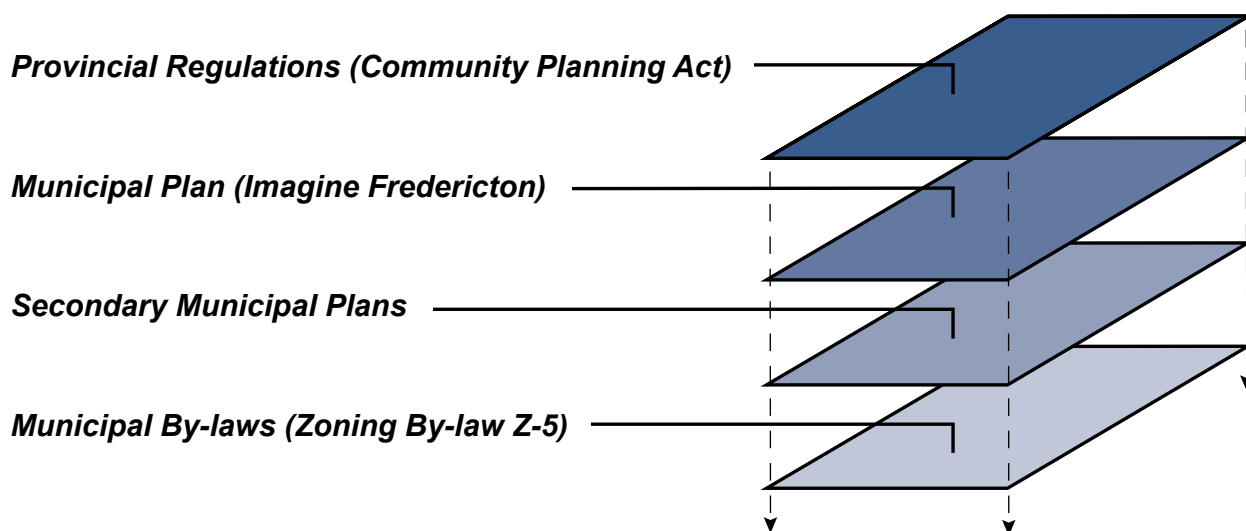
This chapter outlines the land use and built form in the South Core. It begins with a review of planning policy that guides growth and development relating to the South Core Secondary Municipal Plan Area, and then describes the land use, built form and the public realm in the South Core.

2.1 Policy Context

Land use, growth and development in Fredericton are guided and implemented through a series of provincial and municipal land use planning documents. Together, these documents ensure that Fredericton continues to grow as a city where residents can live, work and play, and develops in a manner that is socially and environmentally sustainable.

The documents exist in a nested hierarchy, as shown below. This means that the By-laws at the base must respond and align to the regulations and policies at the top. If changes are made at a higher level, amendments to lower level documents are required.

As communities continue to change, planning documents are reviewed periodically to ensure that they still meet the needs of the current and future populations.



City of Fredericton Policy Framework

Provincial Regulations

Community Planning Act

The *Community Planning Act* is the provincial legislation that enables the powers and authority that local governments have to regulate land use and enable development within their boundaries. The Act also outlines the required contents of all planning documents. More recently, the Province adopted the regulation that Municipal Plans must comply with statements of public interest.

Statements of Public Interest

The *Statements of Public Interest* is a provincial regulation that outlines the interests and priorities for land use planning in New Brunswick, setting the planning framework and the minimum planning standards for land use. All land use plans and decisions must comply with the five *Statements of Public Interest* as introduced in October 2023:

1. **Settlement Patterns** – Promote settlement patterns that contribute to the well-being of the residents of the province, minimize impacts on the environment, and support vibrant rural and urban economies.
2. **Agriculture** – Promote the agriculture, fishery and aquaculture sectors and the production of food in the province.
3. **Climate Change** - Engage in processes of climate change mitigation and climate change adaptation.
4. **Flood and Natural Hazard Areas** - Manage development in flood and natural hazard areas to increase health and safety and limit social, environmental and economic costs to the province, local governments, and residents.
5. **Natural Resources** - Protect natural resource development areas and environmentally sensitive areas for present and future generations while fostering a more consistent and predictable regulatory environment.

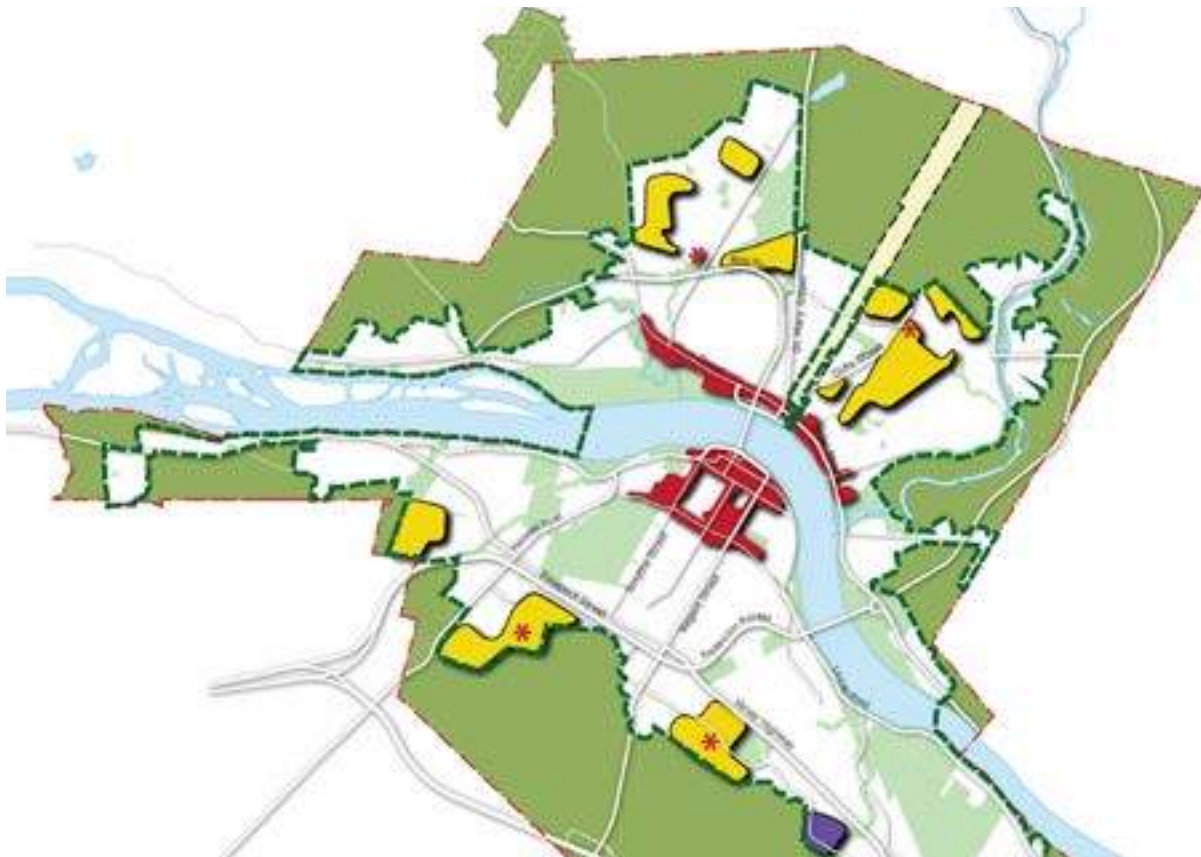
Fredericton Growth Strategy

The Fredericton Growth Strategy (2017) is a non-statutory document with the overarching goal of ensuring that Fredericton grows in a way that is environmentally and economically sustainable for the next 25 years. The Growth Strategy identifies areas in the city for growth (see map on this page) and provides a high level description of how neighbourhoods should be designed. It served as the basis for Imagine Fredericton: The Municipal Plan. The Strategy identifies a growth boundary based on existing and planned municipal services and infrastructure for the next 25 years, with areas within the boundary being able to accommodate future growth and development.

The Strategy sees the city grow in a more compact manner, with a focus on building complete communities, where residents can have services and amenities in proximity. Five distinct areas were identified to accommodate growth: the Urban Core, Brookside, Northeast, High Point Bishop-Hanwell and Uptown.

A significant portion of the Plan Area for the South Core Secondary Municipal Plan is located within the Urban Core (red area in the map below).

Approximately one-quarter of population growth, roughly 8,000 new residents, and much of the forecasted employment growth is intended to occur in the Urban Core, which includes the South Core. Population growth will be accommodated primarily through infill development and intensification, including the introduction of more varied forms of housing, higher densities, mixed land uses, and taller buildings in appropriate locations. The South Core has the potential to accommodate new residential growth. It has many main streets along which intensification could occur, proximity to major employment nodes (e.g. downtown, University of New Brunswick), and numerous large sites that may serve as nodes for taller buildings.



Growth Strategy Map from the Fredericton Growth Strategy

Municipal Plan

The Municipal Plan: Imagine Fredericton (adopted January 27, 2020) provides the broad planning policy framework to manage growth and provides policy guidance on issues related to land use and development in the city. To guide development, the Municipal Plan identifies land use designations that form the basis for the Zoning By-law and all other related development controls (top map on the facing page). The policies in each land use designation outline the desired land use pattern and all planning decisions within the city must align with this Plan.

The Municipal Plan identified two land use designations in the South Core: South Core and Established Neighbourhoods (see bottom map on the facing page). The South Core designation includes both larger sites for potential redevelopment and small sites for modest intensification along principle corridors. Achieving significant intensification in the South Core will be critical to maximizing downtown vitality. Generally, lands within the Established Neighbourhoods designation are not expected to accommodate this same level of intensification. However, these areas still need to evolve and accommodate new development through complementary and compatible forms. More significant development can still occur along primary corridors and the edges of neighbourhoods.

The Land Use Designations from the Imagine Fredericton Municipal Plan provide the foundation for the South Core Secondary Plan. The South Core land use designation is focused primarily along four types of transportation corridors:

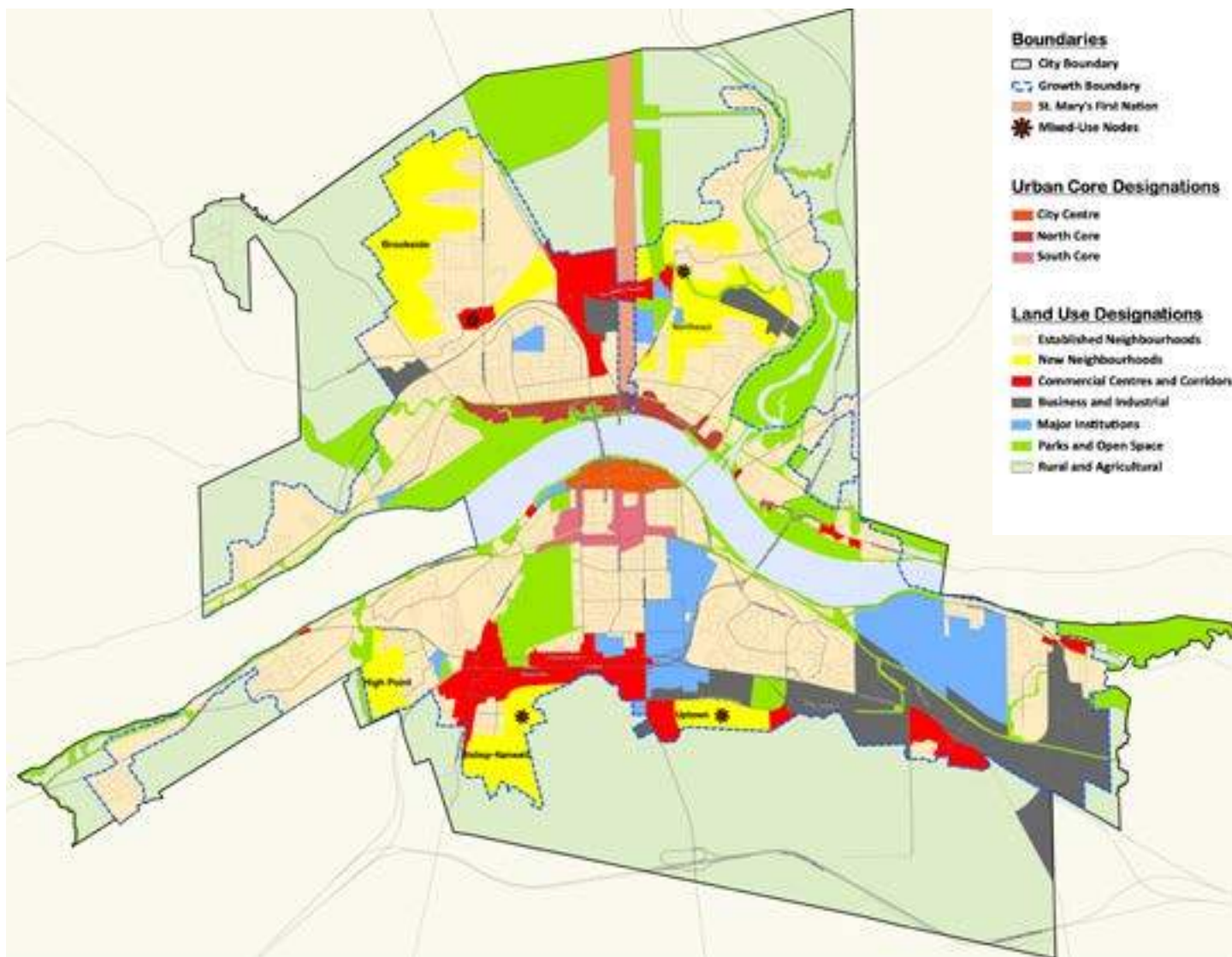
1. Waggoners Lane/Dundonald Street/Beaverbrook Street;
2. Smythe Street;
3. York Street; and
4. Regent Street.

Areas of the Established Neighbourhood land use designation are nestled within the South Core which represent the mature residential community that has existed for generations.

The Municipal Plan establishes that more people will live downtown or within walking or cycling distance, supporting businesses and creating street life, established neighbourhoods will be enhanced with new types of sensitively integrated housing, new development will meet high standards of urban design and architecture, and there will be viable choices for moving around the city, including by public transit, cycling, and walking, in addition to the automobile. With respect to the Plan Area, it contains the land use designations South Core, Established Neighbourhoods, and Parks and Open Space.

In addition the Municipal Plan directs that:

- streetscapes will be designed or improved to feel safe and comfortable to pedestrians and to encourage walking;
- the city's public realm will be designed and maintained so that the main pedestrian thoroughfares and other well-used pedestrian connections are usable and safe year-round;
- the city's tree canopy will increase, and there will be more tree-lined streets;
- the built environment will be designed to encourage outdoor winter activities; and,
- main streets in the Urban Core will be redesigned over time to become multi-modal complete streets, with a 'pedestrian first' principle.



Schedule 2 Land Use Map from the Fredericton Municipal Plan



Detail of Schedule 2 Land Use Map from the Fredericton Municipal Plan with the South Core Secondary Plan Area (dashed black)

Secondary Municipal Plans

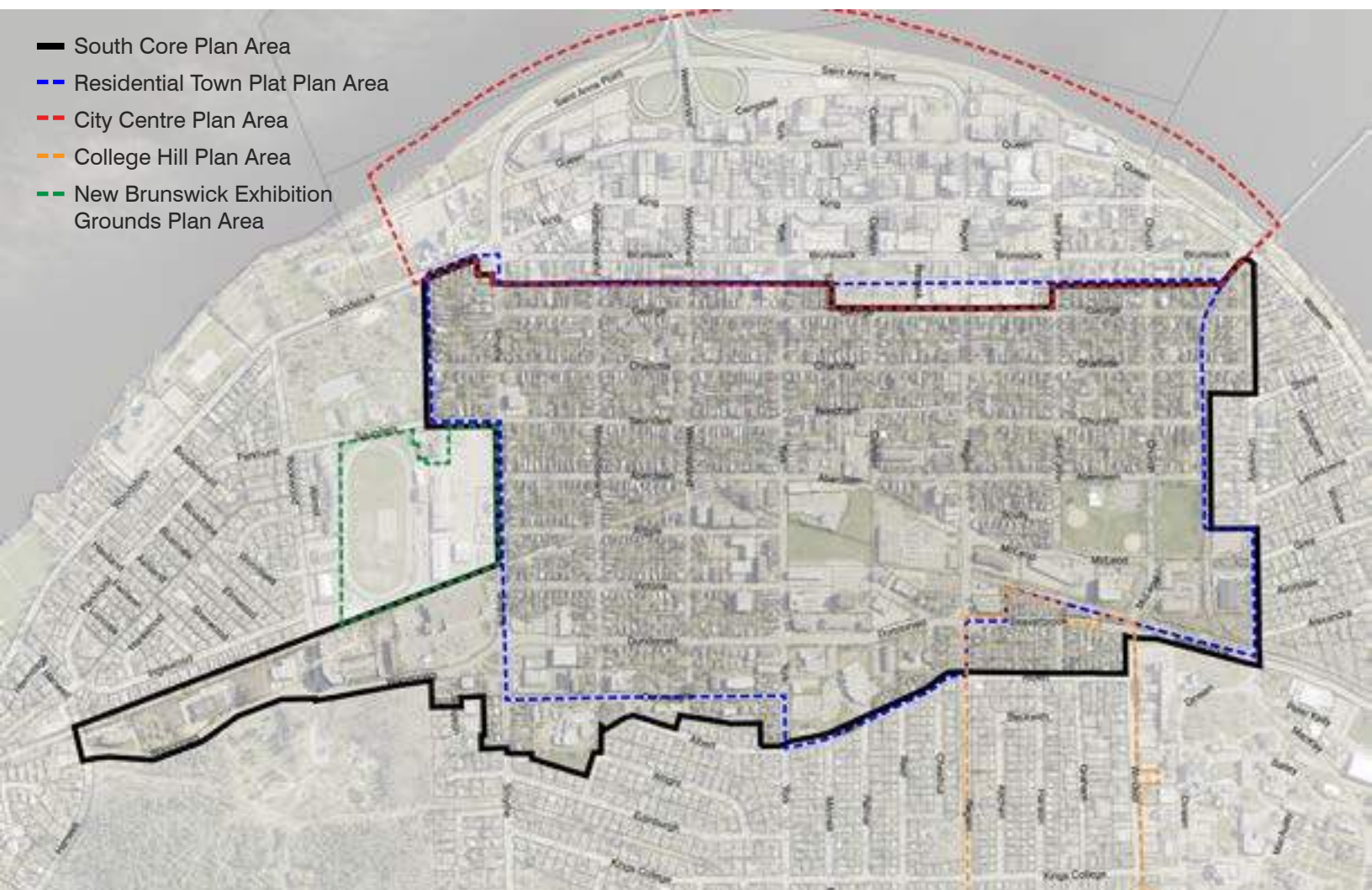
A Secondary Municipal Plan is a more detailed land use plan for a specific area within a Municipality. Secondary Municipal Plans address issues or needs that a Municipal Plan might not and can provide a further level of detail for policies specific to the area. For instance, this may include building design guidelines that are only applicable to a specific neighbourhood and not to the rest of the Municipality.

There are six Secondary Municipal Plans in effect:

- Residential Town Plat Secondary Municipal Plan
- New Brunswick Exhibition Grounds Secondary Municipal Plan

- College Hill Secondary Municipal Plan
- City Centre Secondary Municipal Plan
- Union Street Area Secondary Municipal Plan
- Main Street Secondary Municipal Plan

The South Core Secondary Municipal Plan Area includes the entirety of the planning area of the current Residential Town Plat Secondary Plan as well as a portion of the College Hill Secondary Plan. The map below illustrates the boundaries the various Secondary Municipal Plans that abut or extend into the South Core Plan Area. Each are briefly described in this chapter.



Map of the boundaries of the existing Secondary Municipal Plan areas compared with the South Core Municipal Plan Area

Residential Town Plat Secondary Municipal Plan

The Residential Town Plat Secondary Municipal Plan has an objective of ensuring that development in the Plan Area is compatible with the existing 19th and early 20th century housing stock, while still creating a diverse and vibrant residential neighbourhood that provides a range of housing types. Additionally, the Plan seeks to preserve the historically-significant street grid that characterizes the Town Plat. The Plan recognizes the importance of the system of parks, pathways and open spaces in the area, and highlights the need to create a trail linkage through the former railway yards in what is known today as the Cross Town Trail.

First written in 1999, the Residential Town Plat Secondary Plan has been reviewed several times, with the most recent revision in 2020. It has achieved success through the ongoing development of the Railyard Lands and the modest infill throughout the area. This Plan will be repealed and replaced by the South Core Secondary Municipal Plan.

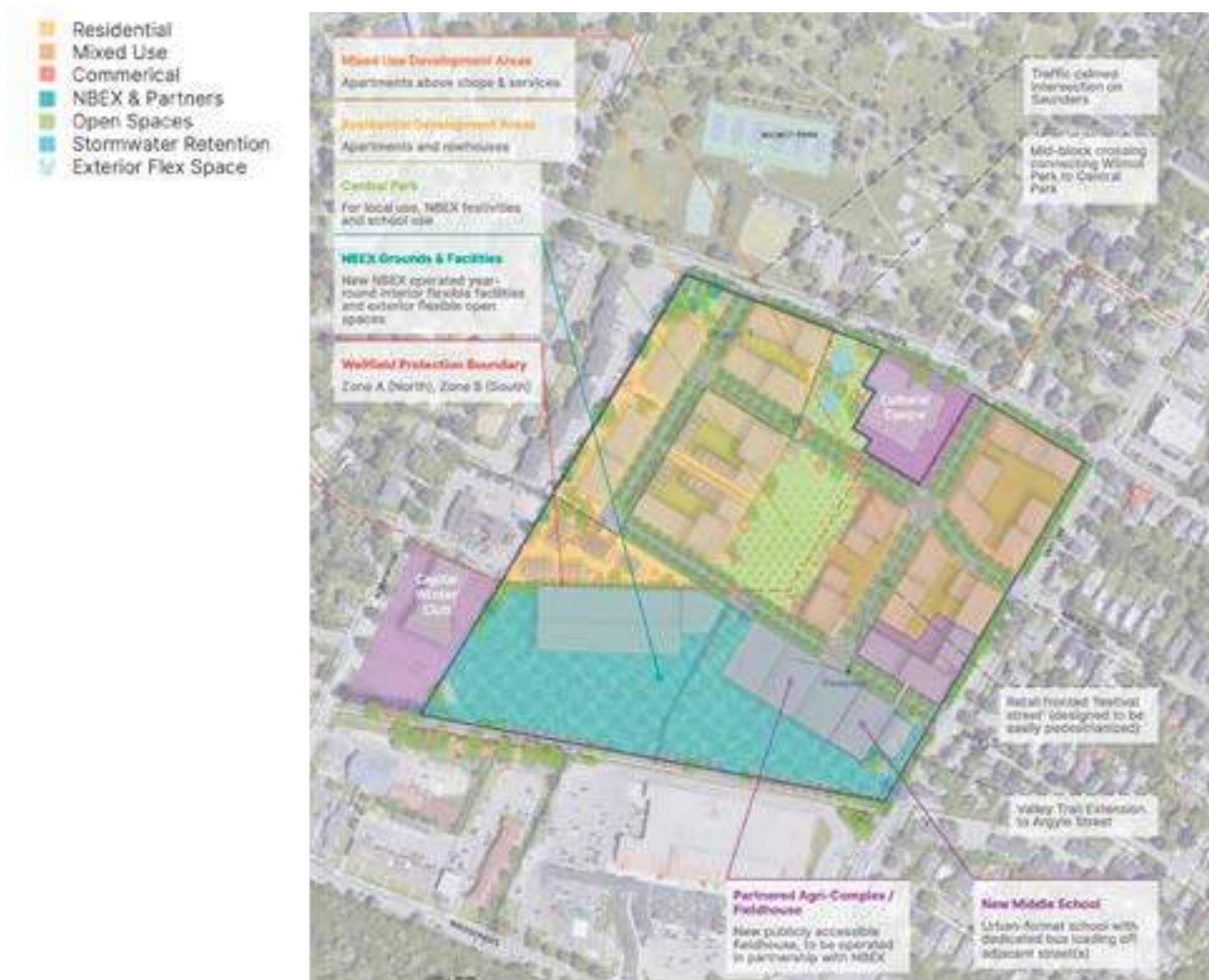


Schedule 1 - Land Use from the Residential Town Plat Secondary Municipal Plan

New Brunswick Exhibition Grounds Secondary Municipal Plan

The New Brunswick Exhibition (NBEX) Grounds Municipal Plan provides the vision, framework, and policies for the development of the NBEX grounds, while maintaining its important cultural history and legacy. The Plan addresses, amongst other themes, reconciliation, sustainable growth by adding density to the area, addressing affordable housing through partnerships, creating and linking open spaces. This plan provides three alternative development scenarios that will be driven by major investment decisions, including the establishment of a new school to serve the urban core.

The South Core Plan will not alter the work that was already completed by the NBEX Plan. Suitable built form and use transitions will be considered, ensuring that the areas directly adjacent to the NBEX site will develop compatibly with any of the potential development scenarios.



Scenario A: Comprehensive Development from the NBEX Secondary Municipal Plan

College Hill Secondary Municipal Plan

The College Hill Secondary Municipal Plan has the key objective of maintaining the residential character of the neighbourhood adjacent to the University of New Brunswick. The Plan was written in 1994, in response to the community's desire for maintaining single-detached houses as the main housing form. Therefore, only a small portion of the area permits multi-residential buildings. The restrictions have given way to illegal additional suites amidst a growing post-secondary population and a shortage of housing. Updates to this Plan are being considered subsequent to the South Core Secondary Municipal Plan process.

The residential block between Albert Street and Beaverbrook Street have been included in the South Core Plan Area. This overlaps with the College Hill Plan Area, but has been done intentionally to address the south side of the Beaverbrook Street corridor and its transition further up the hill.



Schedule 1 - Land Use from the College Hill Secondary Municipal Plan

Fredericton City Centre Plan

The Fredericton City Centre Plan (2015) provides guidance to public initiatives and private development in downtown Fredericton. The Plan provides a vision for the City Centre that directs guidelines, policies, and priorities related to: land use; circulation, including traffic and parking; heritage and character areas; services and infrastructure; economic development, culture, and tourism; and, implementation.

Major/key soft sites (large underutilized properties or groups of properties in key locations) and correlation with public ownership (City, Province, Federal Government) were identified. Built form character areas and heritage resources are identified, characterized, and mapped. High-level urban design guidelines are provided for each character area. This is complemented by a public realm framework, including open space plan with existing and potential open spaces, a streetscape hierarchy and proposed streets, signalized

intersections, mid-block pedestrian crossings, and representative cross-sections. The City Centre Plan is included as it abuts the north edge of the South Core's Plan Area and it will be important to ensure that both Plans work in harmony.



Map 9: Built Form Character Areas from the Fredericton City Centre Plan

Zoning By-Law

The Zoning-Bylaw is the set of land use tools and development controls that enables the implementation of the Municipal Plan and each Secondary Municipal Plan. The Zoning By-law identifies a zone for each parcel of land in the city and sets the rules and standards for that particular zone. Where the Municipal Plan provides the policy direction on how an area should be developed, the Zoning By-law sets the rules on what and how something can be developed in a particular zone.

Following policy direction in the Municipal Plan and in the Residential Town Plat Secondary Municipal Plan, Zoning By-law Z-5, which came into effect in 2013, includes four Residential Town

Plat zones (TP-2, TP-3, TP-4 and TP-6). These zones outline the standards for the design of new buildings in the Town Plat, which have the objective of integrating new developments with the character of the Town Plat.



Map showing Zoning By-law Z-5 zones within the South Core Secondary Municipal Plan Area

2.2 Land Use and Built Form Today

With the foundation of the planning policy context described in the previous chapter, this chapter summarizes the land use and built form (created by buildings, streets and blocks) of the South Core.

The South Core Plan Area encompasses much of the historic Town Plat creating a unique character area within the city. The Town Plat is characterized by its historic 19th and early 20th century housing that is distinctive in its high peaked roofs, buildings located close to the sidewalk, narrow lots, and often colourful wooden siding.

Many of the houses give the appearance of being single occupancy buildings, while containing multiple units making the Town Plat and the South Core one of the most densely populated neighbourhoods in the city.

The South Core is primarily residential in land use. Most of the buildings in the Plan Area are low- to medium-density residential with an increasing number of high-density residential along collector and arterial roads. There is a combination of institutional, recreational, and commercial entities.

The Plan Area has two main commercial nodes, each anchored around a grocery store. The first is along Regent Street between Aberdeen Street to Beaverbrook Street (Sobey's) and the second is at the intersection of Smythe Street, Waggoners Lane and Dundonald Street (Atlantic Superstore).

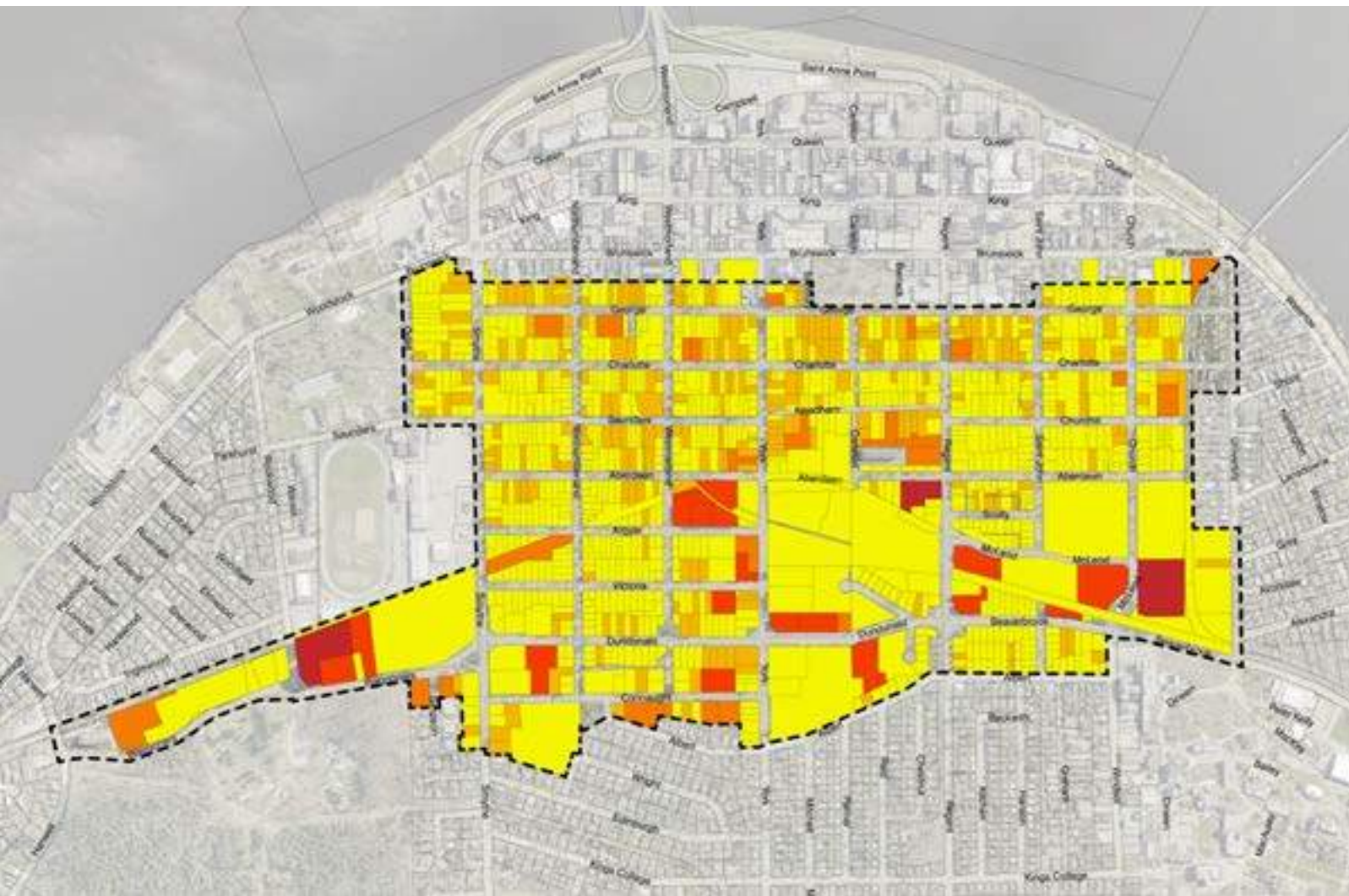
The maps at the end of this section illustrate the key aspects of the South Core's land use and built form.



Example of the character of the existing built form in the South Core

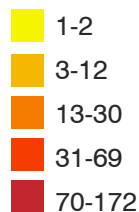


City of Fredericton



Map of Existing Density in the South Core Plan Area

Units



The above map shows the density in the South Core Plan Area as a measure of the number of units on each property. Higher density uses are located close to the Cross Town Trail and generally south of Aberdeen. Most of the Plan Area has a density of 1-12 units per lot (yellow to orange on the map above)



Map of Existing Commercial Uses in the South Core Plan Area

■ Existing Commercial Uses

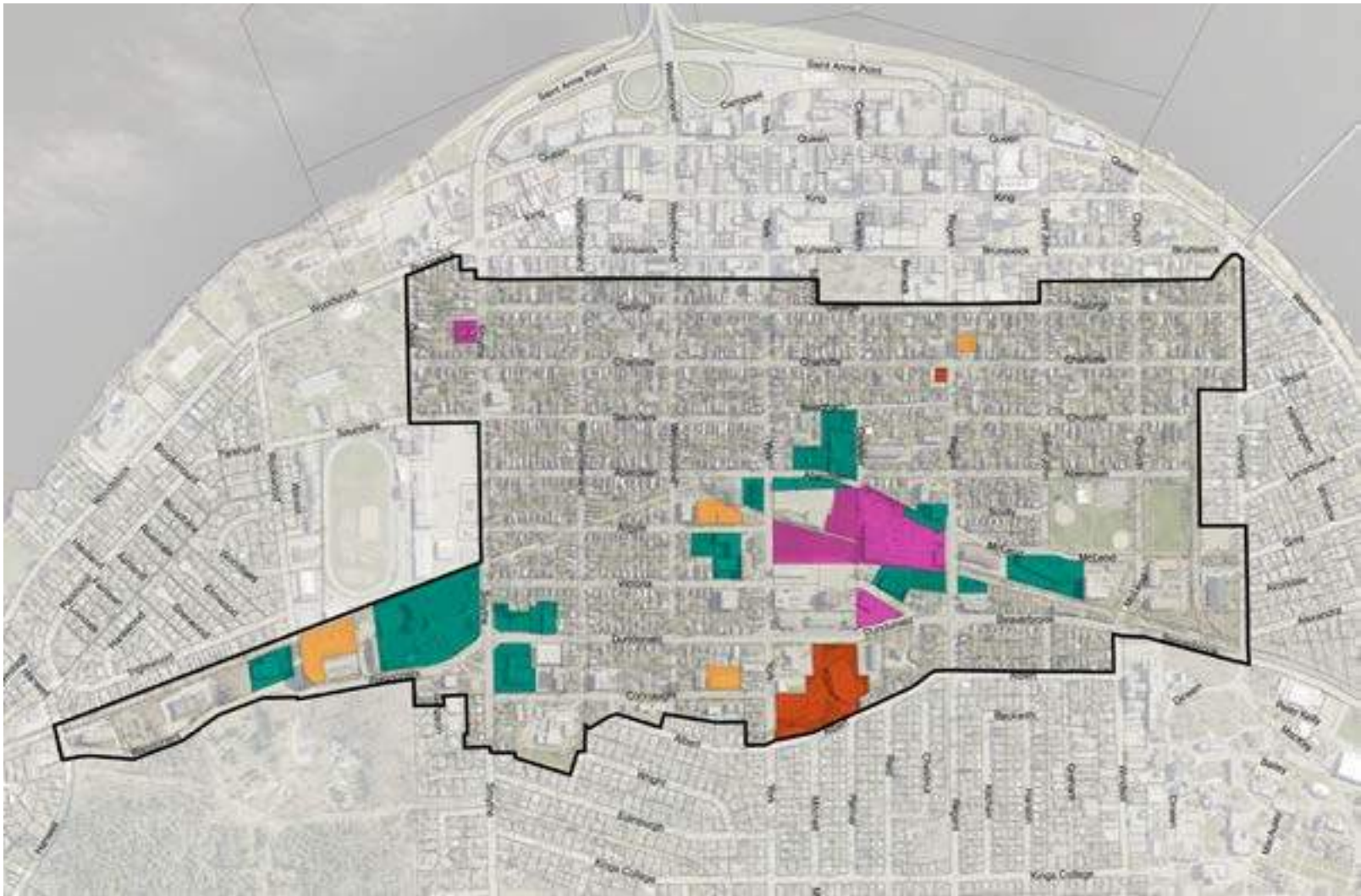
The above map shows the existing commercial uses in the South Core Plan Area. Commercial uses range from large format buildings, such as Sobeys's, to corner shops and services, often with residential uses above. The map illustrates that there are commercial uses dispersed throughout the South Core with larger commercial blocks adjacent to the Cross Town Trail.



Map of Community Facilities in the South Core Plan Area

Community services are distributed throughout the Plan Area (or in close proximity) and include schools, an intercultural centre, a fire station and places of worship. Recreational facilities include an ice rink (Lady Beaverbrook Rink), a gym (YMCA) and the amenities within the surrounding parks. The following community facilities are located in the South Core and immediately adjacent to the South Core.

- Preschools/childcare Fredericton early learning centre
- Bluebell Montessori School
- Bright Beginnings Daycare
- MacLean School for Early Childhood (Private)
- Connaught Street School (Elementary)
- New Brunswick Community College
- University of New Brunswick Maggie Jean Chestnut
- Residence/Renaissance College
- York Street Fire Station
- Lady Beaverbrook Rink
- New Brunswick Provincial Exhibition
- Capital Winter Club
- Fredericton Intercultural Centre
- YMCA
- Charlotte Street Arts Centre
- St Andrew's Presbyterian Church
- St. Charbel's Maronite Catholic Church
- St Paul's United Church
- St. Anne's Anglican Chapel of Ease
- Grace Memorial Baptist Church



Map of Development Activity in the South Core Plan Area

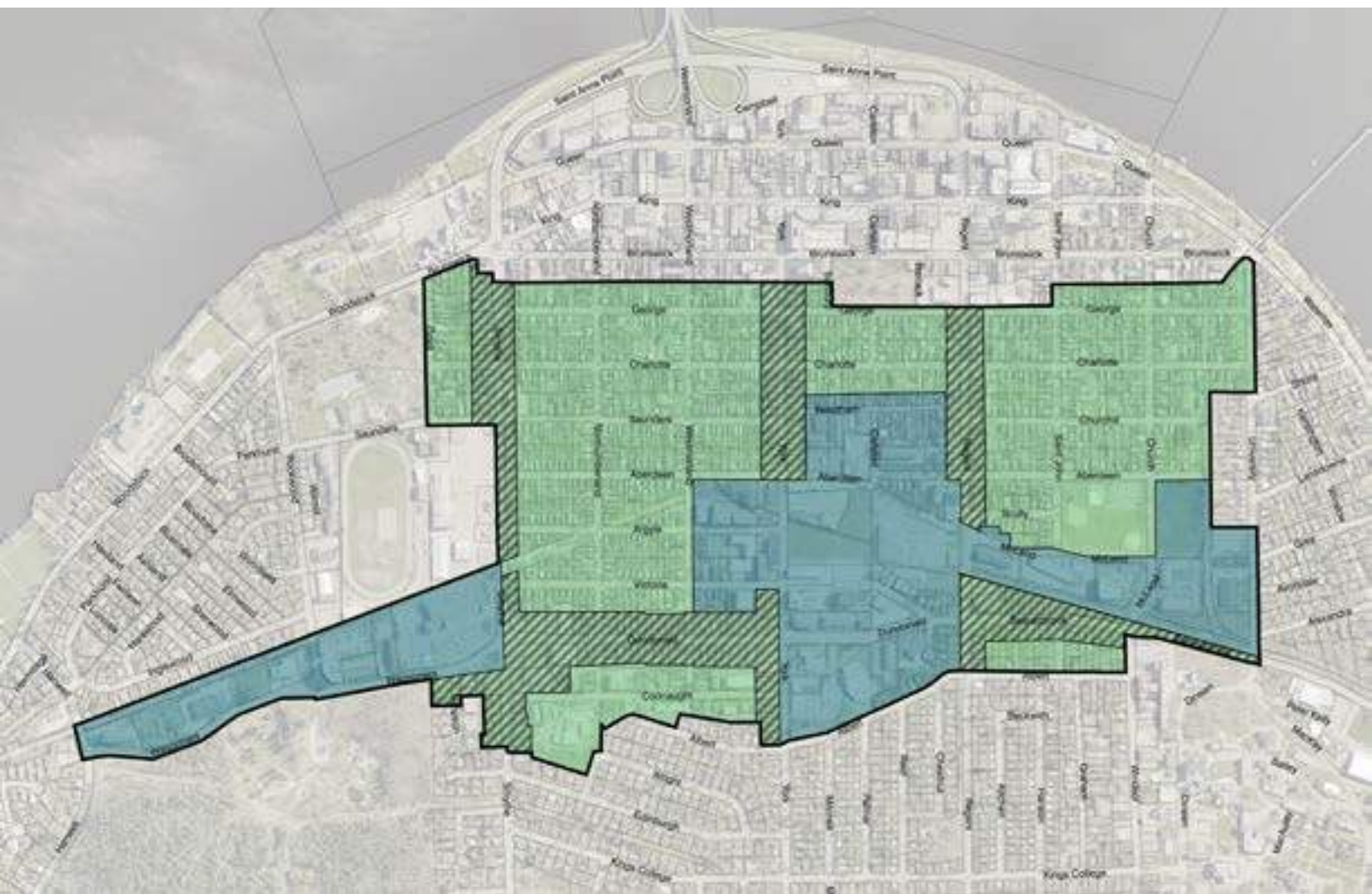
- Recent Developments
- Approved Developments
- Development Applications Imminent
- High Redevelopment Potential

The above map shows the location and status of various applications for development in the South Core Plan Area.

2.3 South Core Character Areas

The South Core can be understood as a dynamic place that has evolved together with culture and society. The historic residential and industrial functions of the South Core are still reflected in its buildings, streetscapes, and landscapes, which comprise two primary Character Areas: the Residential Character Area and the Rail-Adjacent

Character Area. These Character Areas will be refined through the remaining phases of the work on the Secondary Municipal Plan.



Preliminary Map of Proposed Character Areas in the South Core

- Residential
- Avenues Sub-Area
- Rail Adjacent

Residential Character Area

The Residential Character Area is the larger of the two Character Areas in the South Core, extending from George Street in the north to Connaught Street in the south, on either side of the former railway lands. The northern portion, encompassing George and Charlotte Streets, was laid out in 1786 as part of the original Town Plat survey, which established a regular grid pattern for the new provincial capital.

This pattern of development created a relatively consistent grid pattern, with long narrow building lots fronting the east-west streets and shallow lots fronting the north-south streets. The grid is anchored to the east by Queen's Square Park, established c. 1896, and bordered to the north and west by the Old Burial Ground and Wilmot Park, respectively.

Within the Residential Character Area, the most common building typology is the detached house, which is expressed in a variety of architectural styles, including Classical Revival, Queen Anne Revival, American Foursquare, Second Empire, Craftsman, and other vernacular styles. The area around Queen's Square, especially around George and Church Streets, features an ensemble of grand late-19th and early-20th-century houses, with more modest housing concentrated further east and south. The southernmost residential zone along Connaught Street was developed in the post-war period, diverging from the predominant residential character.

Like other parts of the Maritimes, the houses in South Core are largely constructed from wood, due to the influence of traditional building methods from New England, and the relative abundance and affordability of timber in New Brunswick. The Residential Character Area also includes a variety of **civic and institutional building typologies**, including churches and schools which, in contrast to the housing stock, are typically constructed from brick and/or stone and exhibit more ornamental flare in their architectural expression.

A number of these civic assets have been converted to accommodate new residential, office, and cultural uses since the 2000s. This trend

of adaptive reuse is mirrored in the residential fabric, where many detached houses have been converted from single-family homes into multi-unit apartments (known colloquially as “**accordion houses**” or “**telescope houses**”).

Small-scale infill developments that reflect the consolidation of multiple lots for neighbourhood intensification, usually in the form of **walk-up apartments** or **townhouses**, can also be found throughout the Residential Character Area. These buildings were built during many phases of the neighbourhood's evolution and reflect the popular building typologies and architectural styles of their time; for example, Modernist walk-up apartment buildings built during the 1960s and 70s or Post-Modern townhouses built in the late 20th century and beyond.

One of the area's defining features is its mature tree canopy, a network of tree-lined boulevards and rear-yard trees which provide a green setting around the built fabric. Due to the minimal setback of many houses from the street, front-yard trees are less common, although they do exist in some locations.

The Residential Character Area has an Avenues Sub-Area. Smythe, York, Regent, and Dundonald Streets are distinguished from the rest of the area by their greater intensity of uses. The Avenues feature wider streets designed for automobile traffic and variation in building typologies, with civic/institutional buildings, infill developments, and detached houses located side by side. The tree canopy is less regular along the four Avenues. In this way, they differ from the prevailing character of the Residential Character Area.

Key Features (Residential)

- Grid pattern (slightly irregular);
- Long narrow building lots fronting the east-west streets;
- Shallow building lots fronting the north-south streets;
- Little variation in building typologies, with the detached house as the predominant building type (many converted into multi-unit apartments);
- Mix of architectural styles (mostly traditional);
- Intact mature tree canopy; and,
- Narrower streets.



Typical streetscape in the Residential Character Area

Key Features (Avenues)

- Shallow building lots with a range of lot sizes;
- Greater variation in building typologies, including infill, civic/institutional buildings, and detached houses;
- Larger buildings;
- Mix of architectural styles (traditional and contemporary);
- Fragmented tree canopy; and,
- Wider streets.



Typical streetscape in the Residential Character Area

Rail-Adjacent Character Area

The Rail-Adjacent Character Area is the smaller of the two primary Character Areas in the South Core, comprised of two former railway corridors — currently the Cross Town, Lincoln, and Valley Trails — and the former railway yards and adjacent industrial lands.

Originally a rural landscape on the edge of Fredericton, this area was transformed with the arrival of the railway in 1868, which terminated at a depot on York Street south of Needham Street. During the late 19th and early 20th centuries, an industrial district emerged in proximity to the rail yards, with siding connecting factories to the main line. For many decades, this district formed the edge of the Town Plat, before being encircled by residential developments as Fredericton expanded over the course of the 20th century.

In contrast to the Residential Character Area, this area is characterized by its coarse-grained urban fabric, with an irregular block pattern, large lots, and large building footprints interspersed among surface parking lots and brownfield sites. The remnant railway and industrial landscape is the area's defining feature. It comprises the trail network which replaced the railway lines, the collection of late-19th and early-20th-century industrial buildings, and the former Canadian Pacific Railway Station at York Street, which is the focal point of the area. Most of the remnant **brick and beam** industrial buildings are located near the station, including the landmark Hartt Boot and Shoe Factory, a three-storey building with a prominent central tower built c. 1898 on the west side of York Street.

Within the Rail-Adjacent Character Area, there are several residential zones which were developed contemporaneously with the railway, containing modest workers' housing sited alongside former industrial sites. One example is Needham Street, an east-west street laid out haphazardly in the 1860s in connection with the railway. In the post-railway era, many of the brownfield sites were redeveloped with low- and mid-rise apartments, large commercial buildings, and new civic/ institutional buildings, which have contributed to a layered urban landscape.

Key Features

- Interrupted grid pattern and irregular blocks;
- Trail network following the former railway lines;
- Large lots and building footprints;
- Widest variation in building typologies, including industrial buildings, low- and mid-rise apartments, and commercial buildings;
- Mix of vernacular and contemporary styles;
- Unbuilt areas (surface parking lots, brownfield sites); and,
- Wider streets.



Heritage industrial building in the Rail-Adjacent Character Area



Redevelopment example in the Rail-Adjacent Character Area

2.4 Building Typology

Building typology examines the typical form, massing, siting, materiality, articulation, detailing and construction methods of buildings. In this way, typology can clarify why certain types of buildings were designed – or have evolved – in particular ways and can be a highly effective tool for developing guidelines to support compatible future evolution within an area.

As an evolved neighbourhood with two distinct Character Areas, the South Core has many different building types. The predominant historic forms found throughout the South Core are civic and institutional buildings, like schools and churches; modest vernacular houses that have expanded over time (colloquially known as “Accordion Houses” or “telescope houses”); rail side brick-and-beam factories; small- to medium-scale post-war apartment blocks; and more contemporary mews and townhouse developments.

Civic and institutional buildings tend to be among the earlier buildings, dating to the late 19th century, along with the original houses of the Residential Town Plat, many of which have become “Accordion Houses” through alteration. Historic rail side factories, which developed during the late 19th and early 20th centuries, have been adapted to residential and office uses. More contemporary forms include post-war modernist apartment blocks and mews or townhouse developments designed in the late 20th century post-modern architectural tradition that echoes the vernacular style of the original South Core houses. The single unifying factor is that each building type is a product of its time.

Completely new or contemporary buildings have largely been excluded from the analysis, as there is a high degree of variability in their form and articulation. Mews and townhouse developments represent the exception. They have been included because they exhibit a highly distinctive character and represent a highly context-specific adaptation of residential form within the South Core.

Civic/Institutional (Late 19th Century)

Civic and institutional buildings take many forms within the Residential Character Area; their single common attribute is their ability to provide supportive infrastructure for the community. Often these buildings are constructed of durable materials, such as brick or stone, in contrast to the predominantly wood construction of house-form buildings in the South Core. Whether they are churches, schools, or community centres, these buildings provide places for meaningful connection for residents of South Core. As the nature of society changes, so too does the role of these buildings; for example, the designated 1893 York Street School building at 193 York Street, which has been converted to loft apartments. While they are candidates for adaptive reuse, they often require sensitive or bespoke approaches to redevelopment.

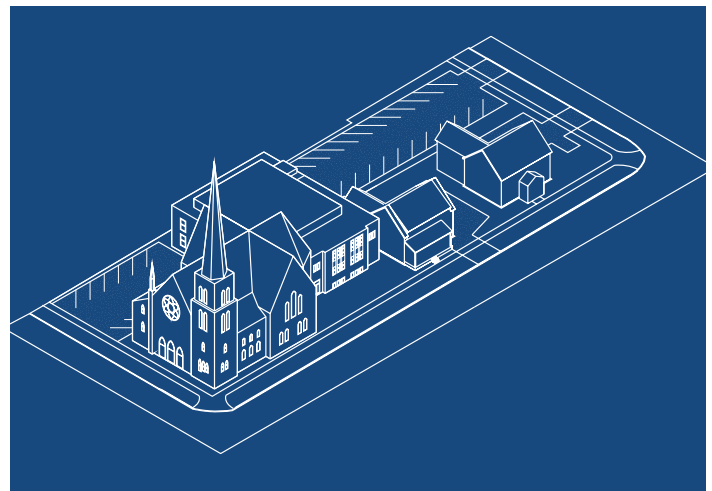


Diagram of a typical Civic/Institutional building

“Accordion House” (Late 19th Century, adapted)

A common adaptation of the predominant typology within the Residential Character Area, the “accordion house,” or “telescope house” as it is colloquially known, represents the evolution of traditional wood-frame house-form buildings to accommodate larger families and/or multiple tenancies. These houses usually reflect loyalist vernacular architecture but are found throughout the South Core in a variety of architectural styles, including Classical Revival and Queen Anne. “Accordion houses” reflect the many architectural styles present in the Residential Character Area with additions – commonly built into the rear yard – and long, narrow driveways stretching toward the rear property line. The interior-block tree canopy and green vs. grey infrastructure is altered on properties that contain “accordion houses”.



Diagram of a typical “Accordion House”

Brick and Beam Factory (Late 19th Century/Early 20th Century, adapted)

Brick and beam factory buildings are a prominent historic rail side typology within the Rail Adjacent Character Area. Designed to maximize floor area and access to natural light for large-scale operations, brick and beam factories tend to have very large and relatively narrow, horizontally-oriented floorplates and regularly spaced windows on all sides. The high proportion of fenestration to brick on the buildings, which are longer than they are wide, ensures that natural light can penetrate the building and factory floor from both sides. Many of these factories have been converted to residential or commercial use. The loose-fit interior spaces and brick and beam construction makes them excellent candidates for adaptive reuse.

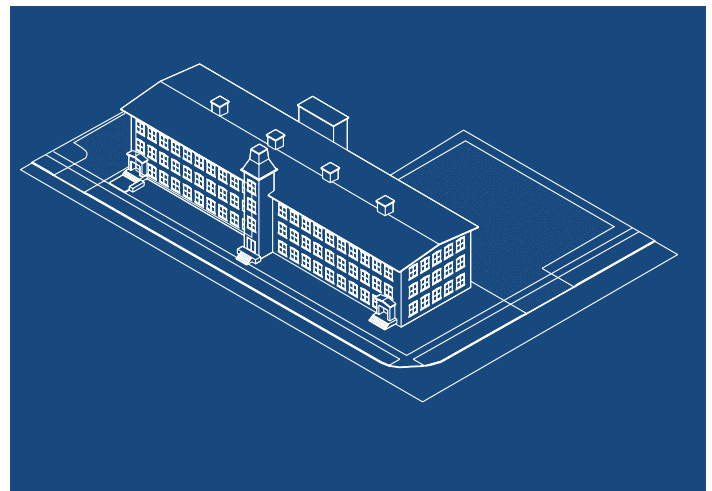


Diagram of a typical Brick and Beam Factory building

Walk-up Apartment Block (Post-WWII)

Along with the accordion house, the walk-up apartment block is another residential typology that has evolved within the Residential Character Area and the Rail Adjacent Character Area to accommodate intensification. Formed by lot consolidation and low-rise redevelopment, these blocks often alter the relationship of built form to public realm: their entrances are set back more deeply from the sidewalk or street, and they are surrounded by open green space. Many of the blocks were developed in the post-war period and reflect the architectural sensibilities of the time; however, others are more contemporary, making the overall stylistic character of these blocks somewhat mixed.

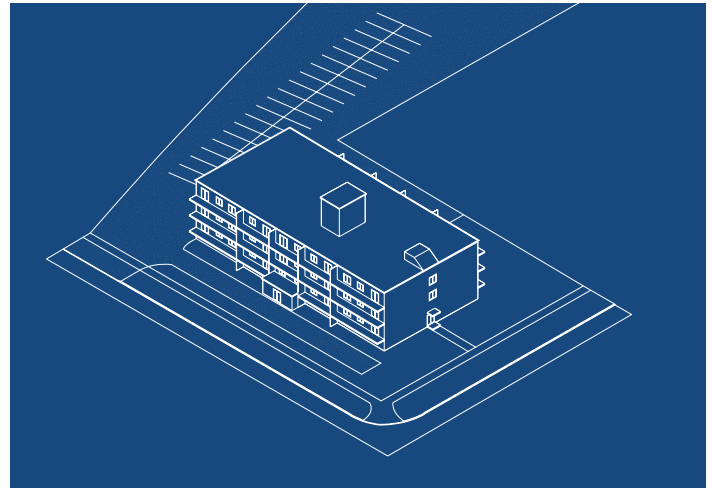


Diagram of a typical Apartment building

Townhouse/Mews Development (Late 20th Century)

Townhouses oriented around a central mews or parking area are yet another form of intensification present in the Residential Character Area. Like apartment blocks, these developments were formed by consolidating residential lots and adding a low-rise residential typology to accommodate multiple families within an area that would have historically accommodated fewer units. The long and narrow lots within the South Core and elsewhere in Fredericton create the ideal condition for this type of development. Like post-war apartment blocks and “accordion houses”, these developments disrupt the tree canopy and historic relationship between green to grey infrastructure by replacing open green space with surface parking to accommodate more residents. They are a more contemporary adaptation of built form in South Core but are often aligned with the vernacular architectural style of the Residential Character Area.

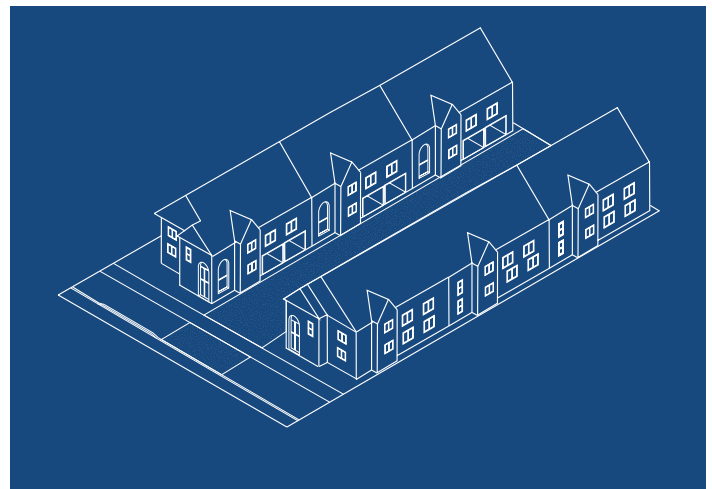
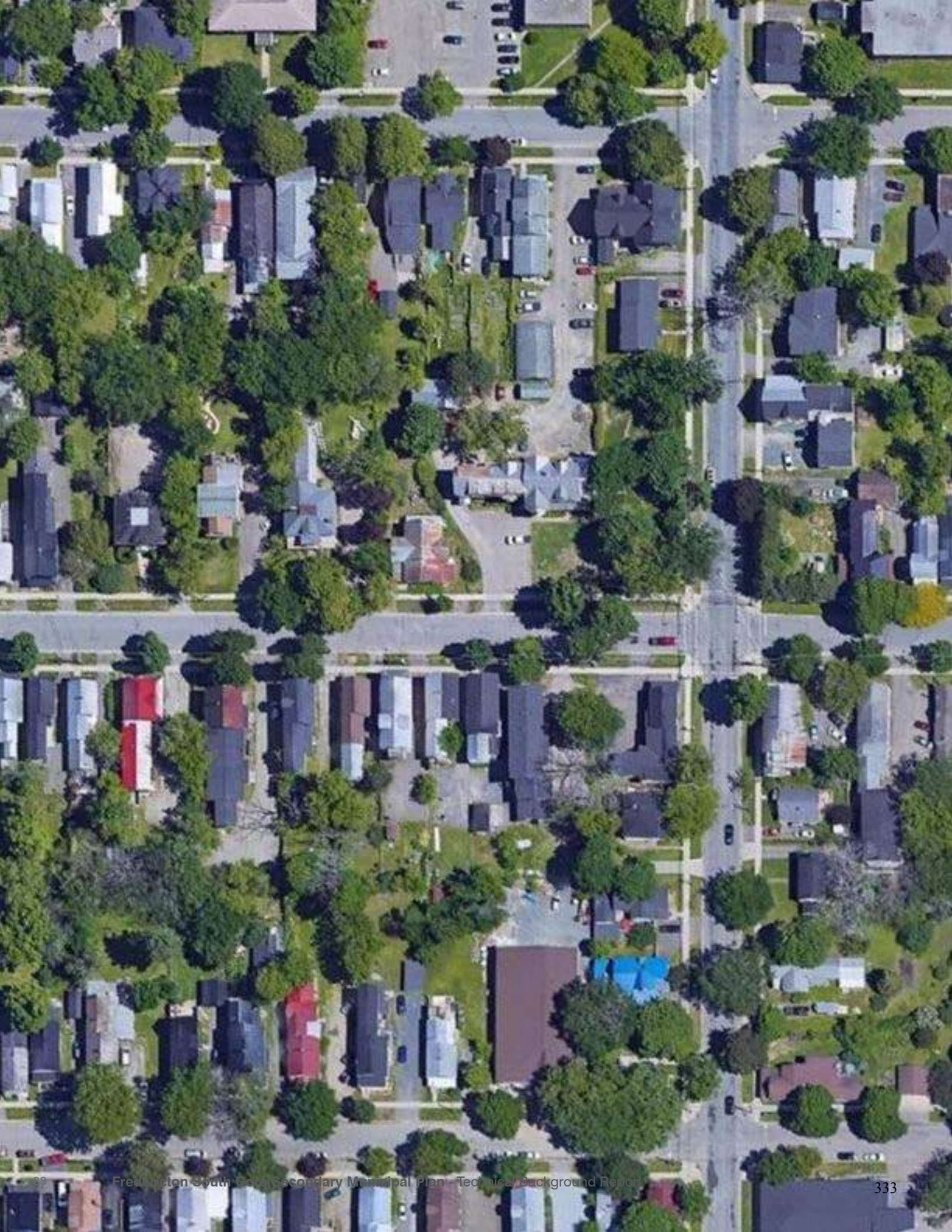


Diagram of a typical Mews/Townhouse building

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3 Heritage

3.1 Conservation of Cultural Resources

Conservation of cultural heritage plays a critical role in supporting and maintaining local identity, community character, and civic pride in Fredericton. It is one of many – sometimes competing – objectives to be balanced in municipal planning. The South Core forms part of the early settlement area of Fredericton and retains many buildings that carry significance in relation to that history. Its character has evolved with time, its buildings reflecting waves of residential intensification that resulted from Fredericton's growth as a centre of commerce, trade, and government activity. Good planning requires that this character be conserved as the neighbourhood continues to evolve to meet future needs.

From Heritage Preservation to Contemporary Conservation

Early forms of conservation emerged in North America as “preservationists” rallied against the demolition of architecturally significant buildings during the 1960s and 70s. Conservation of heritage and archaeological resources in New Brunswick was regulated at the time by the *Historic Sites Protection Act* (1973), which established the general framework for conservation, and the *Municipal Heritage Preservation Act* (1978), which enabled municipalities within the province to enact by-laws for the protection of historic sites. These statutes formed the basis of the *Provincial Heritage Conservation Act* (“Heritage Conservation Act”; 2009), the legislation that currently governs conservation in Fredericton.

Since the early preservationist movement, definitions of heritage have expanded beyond architecturally significant historic buildings and archaeological sites. Natural features, designed landscapes, and intangible cultural practices are all considered to carry cultural heritage value in contemporary heritage discourse. Our understanding of whose heritage we aim to conserve has also expanded. Places that hold significance for Indigenous and marginalized peoples are increasingly recognized as heritage resources and the stories they tell are becoming

part of the broader public consciousness. The “new heritage” aims to understand both history and the present-day attribution of value through many lenses.

Evidence of this shift in thinking can be found within Fredericton's periodic municipal heritage program reviews. A Technical Report on History and Heritage, prepared in 2003 as part of the Municipal Plan (2006) Review, identified concerns regarding unsympathetic alterations to heritage resources, demolition and decay of historic buildings, and lack of high-quality design in new construction (2003; Issues in Heritage Planning, p.42). It also signalled a growing appreciation for the City's layered and multi-faceted heritage, including pre-contact histories, post-war architecture, contemporary streetscapes, general neighbourhood character, and urban form. Recommendations from the more recent Heritage Program Services Review focused strongly on ensuring that Fredericton's heritage program is accessible and inclusive, highlighting the need to support an “accurate presentation of everyone's story” (2023; Introduction, p.1).

As part of broader conversations about culture, environment, and community evolution, contemporary heritage conservation is achieved through more than just enabling heritage legislation. Municipal planning documents, for example official municipal plans, secondary municipal plans, or arts and culture plans, frequently incorporate measures to conserve cultural heritage. Cultural heritage conservation has also become part of many municipal frameworks for Truth and Reconciliation with First Nations Peoples. The policies contained in these documents often lie outside of the legislative framework for heritage conservation, while sharing its primary objective.

Municipal regulatory tools for heritage management include prescriptive development standards (by zone), easements or covenants, transfer of development rights, density bonusing, zoning provisions, design review, design guidelines, alternative development standards

and property acquisition. Incentive programs, including tax incentives and grant programs, can also empower property owners to engage with heritage conservation and encourage community resource stewardship. Public awareness campaigns, including interpretation programs, can be highly effective tools to communicate lesser-known histories to new audiences.

Provincial Heritage Conservation Act (2009)

The *Heritage Conservation Act* forms the legislative basis for heritage conservation and the management of archaeological sites in New Brunswick. It works in tandem with the provincial *Community Planning Act*, which regulates land use and density and guides the design of new buildings outside of areas protected by heritage legislation.

Within New Brunswick municipalities, resources that are deemed to be locally significant for historical or architectural reasons can be designated as Municipal Heritage Conservation Areas under the *Heritage Conservation Act*. Designation protects significant resources through Heritage Preservation By-Laws that govern how they are conserved. Municipal Heritage Conservation Areas can include individual or multiple properties, buildings, or structures; archaeological and palaeontological sites; cultural landscapes; and natural heritage features. The *Heritage Conservation Act* also allows municipalities to recognize local historic places by adding them to an honorific register with the consent of the property owner.

The creation of Heritage Conservation Areas and By-Laws is a regulated process that involves input from an appointed municipal Heritage Board, formal public review, and approval by municipal Council. Heritage Conservation By-Laws are part of the municipal planning framework and co-exist with Zoning By-Laws that regulate land use and built form.

Conservation Areas within the South Core

The City designated its first heritage resources in 1984 under the predecessor legislation to the *Heritage Conservation Act*. The designated heritage resources comprised 60 properties bounded by Queen, St. John, Church and Brunswick Streets that became known as the St. Anne's Point Heritage Preservation Area. Heritage Preservation By-law L-4 governs the management of all heritage resources within the St. Anne's Point Heritage Preservation Area.

The St. Anne's Point Heritage Preservation Area was expanded in 1997 to include additional properties in the area immediately adjacent to the eastern extent of the South Core, extending to the St. John River, and contiguous with the original St. Anne's Point Heritage Preservation Area boundary. The additional properties were designated as part of a collaborative conservation effort by the municipality and a local neighbourhood steering committee. Several properties within the South Core boundary were protected as part of the establishment and early extension of the St. Anne's Point Heritage Preservation Area, with concentrations of buildings along George, Charlotte and Church Streets, and University Avenue.

Individual property designations outside the 1984/1997 St. Anne's Point Preservation Area boundary began in 2005 with the designation of 289 Westmorland Street in the South Core. Other individual properties have been designated since 2005, including five properties around Queen's Square, which were designated with owner consent in 2009, following an unsuccessful attempt to protect that neighbourhood under Heritage Preservation By-Law L-4.

The Queen's Square neighbourhood, situated almost entirely within the South Core boundary, was studied and inventoried through the Queen's Square Study in 2008. The study area comprised approximately ten blocks in the east Town Plat bounded by Regent, George, Church and McLeod Streets and about 250 buildings. Like

much of the Residential Character Area within the South Core (see Section 2.4), Queen's Square is an assemblage of predominantly residential dwellings comprised of single-detached houses and small apartment buildings (Queen's Square Study, 2008; p. 8 and Appendix '1').

Properties within the Queen's Square Study area were evaluated and graded from A to D, with A-graded properties being considered the highest priority for designation. The Queen's Square Study recommended that Council designate a group of 67 high-priority properties contiguous with the western boundary of the St. Anne's Point Preservation Area, along with 24 additional high-priority properties. Council declined to pursue designation and many of the properties remain identified heritage resources with no formal protection under Heritage Preservation By-law L-4 (see Map of Identified Heritage Resources in Section 2.3).

Designated South Core properties situated outside of the immediate St. Anne's Point Preservation Area include houses at 289 Westmorland Street, 279 Regent Street, 334 St. John Street, 261 St. John Street, 240 St. John Street, and 770 George Street; and the 1893 York Street School building at 193 York Street, which has been converted to loft apartments.

Municipal Planning Framework

The vision for Fredericton, as articulated in the current Municipal Plan, provides that significant cultural heritage resources will be "protected, interpreted, and celebrated" to support culturally rich and diverse communities (2020; Section 1.3(3)(i), p. 10). Within this context, the Municipal Plan heritage policies expressly acknowledge the context of the evolving city and the pressure that new development places on the conservation of heritage resources (Section 3.5.1, p. 49). The City is consequently required to maintain a Heritage Conservation By-Law, in accordance with the *Heritage Conservation Act*, and is mandated by the Municipal Plan to "seek and encourage" conservation of heritage resources in a manner that reflects their value to the community (ibid.).

Fredericton's Secondary Municipal Plans flow from the overarching objectives and direction of the Municipal Plan to regulate planning within distinct areas of the city. Two Secondary Municipal Plan areas overlap with the South Core boundary: the Residential Town Plat and College Hill. The College Hill Secondary Municipal Plan contains no heritage policies, and no identified heritage resources are located within the area where the College Hill Secondary Municipal Plan and proposed South Core Secondary Plan boundaries overlap (Beaverbrook to Albert; Windsor to Regent Streets). The Residential Town Plat Secondary Municipal Plan (1998; updated 2022) overlaps substantially with the South Core Plan Area and includes the historic Residential and Rail-Adjacent Character Areas described in Section 2.4 of this Technical Background Report.

The Residential Town Plat Secondary Municipal Plan expressly acknowledges the value carried by the social and architectural heritage of the Town Plat, and its potential contribution to the future development of the city (Section 1(2), p. 3). The buildings of the Town Plat, in all their diverse forms, are also recognized as an important source of housing in Fredericton; adapting the existing building stock to accommodate intensification or other forms of change is recommended as an alternative to full demolition and new construction (Section 1(8), p. 3).

Section 7 of the Residential Town Plat Secondary Municipal Plan provides for Heritage and Design Review in accordance with the high-level objective to ensure new development respects the historic built form and architectural identity of the area (Section 7, p. 8). Section 7 articulates policies intended to achieve that objective including, for example, a requirement that Council recognize the unique residential character within the South Core and adjust lot standards and building setbacks to reflect historic settlement patterns (Section 3(2), p. 4). The South Core Secondary Municipal Plan is intended to replace the Residential Town Plat Secondary Plan.

Conservation of the “New Heritage” in Fredericton

A 2023 third-party review of Fredericton’s Heritage Program calls for City departments and divisions including Heritage and Urban Design, the Community Inclusion Office, First Nations Relations, and Recreation, Tourism and Community Development to collaborate with Treaty and Aboriginal Rights Holders, cultural groups and organizations, Fredericton Heritage Trust, and others. The purpose of deep collaboration is to support the ongoing and perpetual evolution of the heritage program to reflect contemporary best practices in heritage conservation.

Among the specific medium-term recommendations of the review are the creation of a new Heritage Conservation By-Law, including the creation of a new non-regulatory (honorific) level of recognition for identified heritage resources, and consideration of affording that new honorific heritage status to areas of the West Town Plat as part of the South Core Secondary Municipal Plan process. As of the date of this Technical Background Report, a new Heritage Conservation By-Law has not been created.

3.2 Identified Heritage Resources

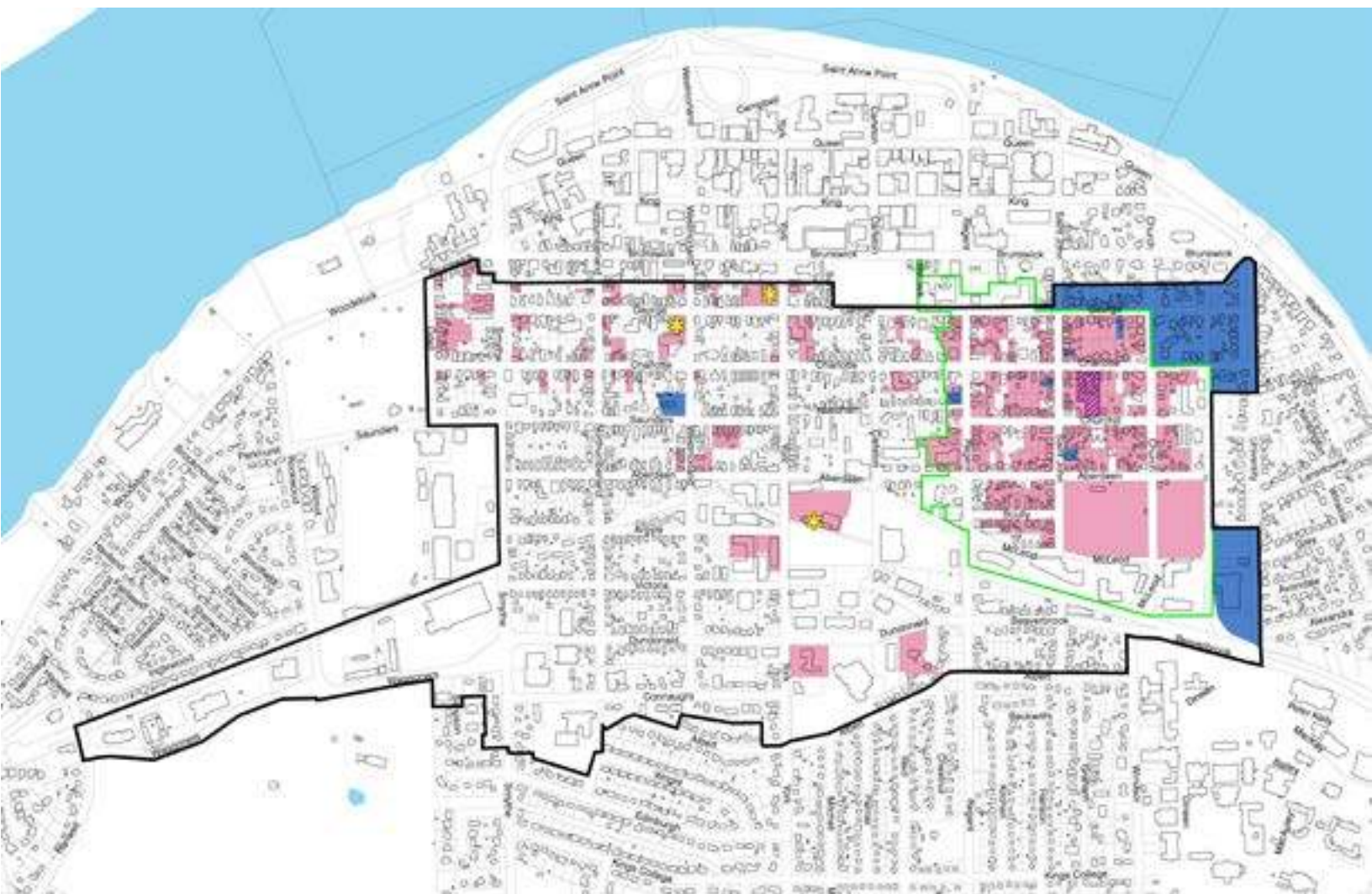
Several hundred properties in Fredericton have been identified as significant heritage resources, but only certain properties are protected under Heritage Preservation By-law L-4. The remaining properties are either:

- recognized in a non-regulatory manner (on the Local Historic Places Register or otherwise flagged through municipal heritage inventories); or,
- subject to Provincial jurisdiction under the *Heritage Conservation Act* as provincially significant, rather than locally significant, properties.

Properties Designated under Heritage Preservation By-Law L-4 (blue on the map opposite) are recognized as locally significant Heritage Conservation Areas in accordance with the *Heritage Conservation Act*. Interventions on these properties must be reviewed and approved in accordance with the provisions of Heritage Preservation By-Law L-4.

Provincial Heritage Places (purple hatch on the map opposite) are deemed to be provincially significant and are protected under the *Heritage Conservation Act*. Proposed interventions are subject to Ministerial approval.

Other Heritage Resources (pink on the map opposite) have been identified on the Local Register of Historic Places in accordance with the *Heritage Conservation Act* or flagged through municipal heritage inventories. Starred properties are designated as **National Historic Sites**. Interventions on these properties are not subject to the provisions of Heritage Preservation By-Law L-4.



Map of Identified Heritage Resources in the South Core

- Properties designated under Preservation By-law L-4/St. Anne's Point Heritage Preservation Area
- Properties on the Provincial Heritage Places register
- Other known heritage resources (identified through City studies or local, provincial or national registers of historic places)
- ★ National Historic Site/Heritage Railway Station
- Queen's Square Neighbourhood Heritage Evaluation Area



4 Public Realm

The public realm is comprised of publicly owned land such as streets, parks and other open spaces that are shared spaces. It knits buildings and their private outdoor spaces together and is influenced by buildings, landscape, and activity all working together to create a sense of place. The public realm of the South Core Plan Area is distinct within the city, largely due to the interface with buildings.

Streets

The streets of the residential neighbourhoods of the South Core are distinct within the context of the City, largely due to the tight interface between the buildings and the street. In many areas the buildings are set within a few metres of the sidewalk with sodded boulevards of over 2.5 m separating the vehicular travel lanes from the pedestrians. All of the streets have sidewalks on both sides (see map on the next page). Lined with hundreds of mature trees, the boulevards provide the soil volumes needed to support the dense

canopy that shades the streets in summer and presents a beautiful cathedral like spatial frame that is an important character identifier of the neighbourhood (see map on the next page).

Two-way streets are the norm in the South Core with a minority being one-way. Some residential streets allow on street parking during the day, but most do not allow overnight parking. To prevent drivers from speeding through residential areas traffic calming circles have been strategically placed at specific intersections.



Example of the character of the streets, a component of the public realm in the South Core

Parks

The South Core neighbourhood is anchored by four important parks, Odell Park to the southwest and Wilmot Park to the northwest, Queen's Square to the northeast and the South Riverfront Trail and The Green. While Odell Park, Wilmot Park and the South Riverfront Trail are not within the South Core they are valuable amenities for those who live within the neighbourhood. Together, Odell Park and Wilmot Park make up approximately 164.5 hectares of parkland (12.2% of the City's total parkland area). The Odell Park Management Plan and A Park Improvements Plan for Wilmot Park guide the continued improvements and management of each park. Amenities within the two parks include cross country ski trails, racquet sport courts, ball diamonds, lawn bowling, a splash pad, and open green space.

The most significant park within the South Core is Queen's Square, located between Aberdeen Street and McLeod Avenue. The park contains two baseball diamonds, an outdoor pool, climbing structures, outdoor courts, and an outdoor skating rink. It is intensely programmed with limited passive green space. A new master plan will be developed in the future to direct capital investment. Directly adjacent to Queen's Square is a track & rugby field operated by the University of New Brunswick.

Three smaller parks within the South Core include: the Rabbit Town Park, near the intersection of Northumberland Street and Argyle Street adjacent to the Cross Town Trail; the Smythe Street Green near the intersection of Smythe and George Street; and the University Avenue Green, near the intersection of University Avenue and George Street. These small parks do not have the capacity to provide active recreational opportunities due to their limited size. However, they offer valuable green spaces for the local community.

The Old Burial Ground is another important open space on the edge of the South Core that serves as a pedestrian and cycling connection between Brunswick and George Streets. The historic cemetery is also a serene strolling area for residents and visitors.

Trails

Trails are described in more detail in the following chapter. A key connecting active transportation route is the Cross Town Trail through the centre of the South Core tracing the former railway line. It is a popular walking and cycling route through the neighbourhood for people of all ages and abilities. In some sections the trail has park-like qualities, lined with vegetation and mature trees. In other sections it connects through commercial and residential areas. The final phases of the trail will be completed in the near future.

Private Realm

In the residential areas the private side yards and rear yards of the South Core comprise a landscape zone of gardens and driveways that provides space for mature trees and vegetation, acting as a "sponge" that absorbs storm water and provides habitat for urban wildlife. These areas contribute significantly to the character of the South Core as a treed neighbourhood and garden district. There is a trend to pave more of the private green space to accommodate additional parking required for increasing density. This will have a cumulative negative impact on the character of the South Core over time.

In the areas adjacent to the railway, the private lands are dominated by surface parking with limited opportunities for greening and pedestrian infrastructure. Recently, new residential and mixed use developments in these areas have introduced both greening and pedestrian amenities.



Map of existing sidewalks in the South Core Plan Area

The above map locates the existing sidewalks in the South Core (outlined with the black dashed line) and the immediate area. Sidewalks are mapped in yellow lines and exist on both sides of the streets through the South Core.



Map of locating existing trees in the public realm in the South Core Plan Area

The above map locates the existing street trees in the South Core (outlined with the black dashed line) and the immediate area.



Map of Parks and Community Sports Facilities in the South Core Plan Area

- Anchor Parks
- Other Parks
- Community Sports Facilities
- Public Rights-of-Way

The above map locates the existing parks in the South Core (outlined with the black dashed line) and the immediate area. Locations with community sports facilities include the YMCA and the Lady Beaverbrook Arena. The public street rights-of-way are also indicated.



5 Transportation and Municipal Services

The previous chapter outlined the land use and built form in the South Core. This chapter outlines:

- the current state of the city's active transportation, public transit, and street networks within the South Core;
- utilities in the South Core: water, sanitary sewer, stormwater, flood mitigation and electricity; and,
- the City's Climate Adaptation Plan, Community Energy and Emissions Plan and Corporate Energy and Emissions Plan.

5.1 Mobility

Active transportation

Active transportation is facilitated through the city's multi-use trail network, sidewalks, and bike lanes. The grid street pattern and provision of sidewalks on nearly all streets within the South Core creates the conditions for an exceptionally walkable and bikeable community. All sidewalks and trails in the South Core are plowed and maintained in the winter by the City.

One website – Walkscore.com – calculates the area as a 'Walker's Paradise' (i.e. 90+) and 'Very Bikeable' (i.e. 80+) through their scoring systems. The area also scores highly on the People for Bikes rating system.

Cycling Facilities

In addition to the multi-use trail system, there are connections for cyclists in the South Core via bike lanes and bike routes. Bike lanes feature signage and lines/symbols painted on the travelled portion of the street, designed for one-way cyclist traffic. Bike routes are identified with signage only and are typically found on streets with higher traffic volumes where the existing cross section cannot accommodate bike lanes.

The following facilities for cyclists exist within the South Core:

- York Street incorporates dedicated bike lanes from George Street to Priestman Street;
- Smythe Street is identified as a bike route; and,
- Church Street is identified as a bike route and has a shared-street treatment between Aberdeen Street and McLeod Avenue through a vehicle access closure.

Additionally, a project has been identified to connect Carleton Street in the City Centre via a trail system to the Cross Town Trail at Aberdeen & Carleton. This would also integrate with the

Downtown Cycle Track, a significant protected bike-lane project planned for the City Centre. It focuses on a one-way loop on Brunswick Street and Queen Street with north south connections made at Carleton Street, the Lincoln Trail and Bill Thorpe Walking Bridge, Northumberland Street and Victoria Circle. Conceptual design is advancing and will be shared in 2024.

Multi Use Trails

Extending and expanding the city's trail system was highlighted as a goal for a complete transportation system within the Municipal Plan. In addition to new trails and trail extensions (some of which are mentioned below), the City is continuously enhancing and improving the existing trail network (refer to map on facing page). Recent and upcoming highlights include:

- Paving several sections of trail in recent years to allow for more accessible year-round access for residents;
- refining the trail level of service standards (a typical approach is that all paved trails are maintained and plowed year-round);
- modernizing the City's Parklands Bylaw (L-19) to more accurately reflect the shared-use nature of how residents are using the trail system; and
- installing lights on a section of the Lincoln Trail (between University Avenue and the Bill Thorpe Walking Bridge) this year.

The South Core has great connectivity to the City's trail network via the following trails:

- Valley Trail, runs parallel to Waggoner's Lane and provides a primary connection west of the Plan Area. This trail also forms part of the Trans Canada Trail, linking the city's trail network with Canada's national trail network. It connects to the South Riverfront Trail in the City Centre via a new boulevard trail (completed in 2022) along Rookwood Avenue and through Wilmot Park.
- Lincoln Trail borders the east of the South Core and provides a connection east of the Plan Area as well as the northside trail network via the Bill Thorpe Walking Bridge. This bridge is a critical transportation and recreation facility for the city that typically averages over 600,000 crossings per year.
- Cross Town Trail intersects the South Core and will provide a continuous east-west active transportation corridor in the Urban Core, connecting the Valley Trail and the Lincoln Trail. The Cross Town Trail provides a partial connection from Valley Trail (where it intersects the NBEX grounds) to Westmorland Street and from Regent Street to the trail on the north side of Beaverbrook Street. The City has approved a plan to upgrade multiple aspects of the trail system to fill in the missing sections (totalling approximately 250 metres). Upgrades include: a bi-directional protected active transportation facility and adjacent sidewalk on the 100 block of Argyle Street and connections from Westmorland Street to York Street that will connect with the multi-use trail on Aberdeen Street as well as the trail network within the future development site on York Street. Preliminary work on the new Cross Town Trail connections began in 2022 with conceptual plans and a public engagement process. The construction phase(s) will begin in 2024.



Map of Active Transportation in the South Core

- Cross Town Trail
- Lincoln Trail
- Valley Trail
- Trans Canada Trail
- Other Trails
- ... Potential Trail Connections
- Dedicated On-Street Bike Lanes
- ... Signed On-Street Bike Route

The above map shows the location of existing and potential trails in the South Core (outlined in the dashed black line). It also locates the dedicated and signed on-street bike routes.

Street Network

The South Core Plan Area includes a variety of street types as illustrated on the map on the facing page. The following provides an overview of the streets within the South Core based on a review completed in 2016. The City is currently undertaking the Transportation Master Study, which will assess street classification in the South Core and, as the city grows, some streets may be upgraded and classifications may change.

Major Arterial

Regent Street is classified as the area's only 'Major Arterial' and is a part of the Provincial-Municipal Highway System (Route #101). Regent Street is a critically important north-south connection that has been identified as operating 'over capacity' based on the current standard threshold levels. Efforts are underway to upgrade the street; replacing and upsizing buried infrastructure to prepare for anticipated impacts of climate change and growth while also upgrading the pedestrian facilities, crosswalks, and roadway. To address capacity issues, in the future, lane configurations will be adjusted to provide two continuous north bound lanes from Aberdeen to St Anne's Point Boulevard (with access to the Westmorland Street Bridge). Pedestrian and signal upgrades will be advanced at Charlotte Street. Major infrastructure construction is anticipated to be completed in 2024.

Minor Arterial

Smythe Street is classified as a "minor arterial" within the city's transportation network. It serves as an important north-south connector street and carries a significant amount of city traffic through the Plan Area. Its southern end connects with the Route 8 Highway and Bishop Drive via the Smythe Street Roundabout. Its Northern end connects with St. Annes Point Drive and the Westmorland Street Bridge via the Victoria Circle Roundabout. The Victoria Circle roundabout was constructed to improve safety for pedestrian and vehicular travel through the Smythe, Brunswick & King

Street intersection while also serving as a gateway to downtown. In 2024, Smythe Street between Victoria Street and Argyle Street will be narrowed to facilitate an upgrade to the Cross Town Trail. Smythe Street is a three-lane cross section with sidewalks on both sides of the street.

Waggoners Lane / Dundonald Street / Beaverbrook Street are also classified as 'Minor Arterials'. There were major improvements made to the Waggoners Lane in 2021 under the Waggoners – Rookwood – Odell Park Improvement Project. The major feature of this project was the addition of a roundabout at the Waggoners Lane and Rookwood Avenue intersection. The roundabout is designed to make the intersection safer for both pedestrians and drivers by reducing vehicle speeds, improving sight lines, creating shorter crosswalks, and by reducing the risk of right-angled collisions. Other improvements included updating water, sanitary, and sewer infrastructure, extending the multi-use trail to connect Odell Park and the Valley Trail, and upgrading transit stops to make them fully accessible.

Major Collector

York Street is the only 'Major Collector' within the Plan Area and provides another north-south connection for residents. This route connects north to City Hall and downtown, and south to Priestman Street. It has on street bike lanes and two lanes of vehicle travel.

Minor Collector

University Avenue is located along the eastern boundary of the Plan Area. This route provides a north-south connection to the Universities and destinations to the south, and to the downtown and destinations to the north. It has two lanes of vehicle travel and curb side parking.



Map of Existing Street Hierarchy in the South Core

- Major Arterial
- Minor Arterial
- Major Collector
- Minor Collector
- Local

The above map identifies the various categories of roads through the South Core (outlined in the black dashed line) and the adjacent area.

Public Transit Network

Most of the City's transit routes provide service to the South Core (see map on facing page) within a reasonable walking distance. However, it is important to note two minor gaps along some of the minor arterial street: Dundonald Street between Smythe Street and Regent Street, and Smythe Street between Dundonald Street and King Street. In 2019, the City prepared the Fredericton Transit Strategic Plan (Fredericton On the Move) that presents short-, medium- and long-term recommendations in all areas of transit operations. This includes alternative route recommendations that would help remedy existing gaps and increase frequency on key corridors. It would also remove service along some streets to reduce overlaps in the service area. Furthermore, as part of this strategy, transit service standards were presented to Council in 2023 that will guide future route decisions for staff. The Sunday pilot program follows a different routing model from standard service.

The South Core is primarily serviced by three north-south transit routes that run through the Plan Area: 14N/15S along York Street; 116/216 along Regent Street; and 16N/17S along University Avenue. These routes offer a standard level of stop frequency relative to the overall transit system, with 30-minute stop intervals during peak periods and 1-hour stop intervals outside peak periods.

For east-west travel within the South Core, both the 12N/13S along Waggoners Lane; and 10N/11S along Beaverbrook Street service the area. The 10N/11S route does provide a 30-minute stop interval during all daytime hours, which is the highest level of frequency currently within the system.



Map of Transit in the South Core

Route	
—	10N CARLISLE
—	11S PROSPECT
—	12N BROOKSIDE MALL
—	13S PROSPECT
—	14N BARKERS POINT
—	15S HANWELL
—	16N MARYSVILLE
—	17S REGENT
—	116 KINGS PLACE
—	216 CORBETT CENTRE
—	18 SILVERWOOD
—	20 LINCOLN
●	Bus Stop (ADA Compliant)
●	Bus Stop (Accessible)
●	Bus Stop (Not Accessible)
○	Scheduled Bus Stop

The above map locates the transit routes through the South Core (outlined in the black dashed line) and the adjacent area.

5.2 Utilities

Water

The South Core contains infrastructure that is critical to the City's overall water system (see map on the facing page). The wells supplying water, the water treatment plants, and the transmission system that distribute this water are all in or near the South Core Plan Area. Like many segments of the city, the water infrastructure varies in age. Water mains are in the process of being replaced as the city undergoes its regular infrastructure renewal projects. Additionally, there is an ongoing unidirectional flushing program to maintain water quality in the system.

The South Core Plan Area falls within zones A & B of the New Brunswick Wellfield Protected Area Designation Order - Clean Water Act. Two wellfield zones are A1, which includes a portion of the NBEX site, and A2, which includes the Queen's Square and surrounding residential properties. The Zone A designation is for the land immediately surrounding the wells up to a 100 day travel time for any surface contamination to reach the well. The rest of the South Core falls within zone B which is less restrictive. This program is under the authority of the New Brunswick Department of Environment and Local Government (DELG) and places restrictions on land use (such as what can be stored on properties) to help protect the City's drinking water from potential contamination. Zone 'A' is closest to the wellhead and is considered the most environmentally sensitive. Zone B surrounds zone A and is further from the well head. As it relates to new or renovated development (residential, commercial, industrial, or institutional), most activity is permitted as long as it meets the guidelines set out in the regulation. For development in zone A, there are some restrictions in terms of non-residential land use as well as the installation of sanitary sewer services for future development and any developer should be aware that exemptions may be required for non-conforming activities. The City maintains a good working relationship with DELG and development within the Wellfield has continued to progress since Fredericton's Wellfield Protected Area was designated in 2005.

Sanitary Sewer

The City operates a fully separated sanitary sewer system where all wastewater/sewage is taken to facilities for treatment. The main sewage treatment plant is located on Barker Street which treats approximately 90% of the City's sanitary, including the South Core. This facility is entering into year 13 of a \$16.5 million upgrade. Additionally, an annual capital infrastructure program (approximately \$7 million) has been initiated to replace aging components of the linear collection system as well as lift station repairs and replacements.

The City is currently undertaking to develop a sanitary sewer collection system hydraulic model to parallel the model completed for the drinking water system. A functional model is anticipated near year end 2024. The purpose of the model is to identify existing and 2041 areas of the sewage collection system impacted by:

- current infrastructure conditions;
- proposed development areas;
- continued population growth; and,
- pinch points susceptible to overflow.

The map on the facing page locates the sanitary service lines in the South Core.



Map of Utilities in the South Core

Utilities	Wellfield Protection Areas
— Storm	 Zone A
— Water	 Zone B
— Sanitary	 Zone C

The above map locates storm, water and sanitary service lines through the South Core (outlined in the black dashed line) and the adjacent area. It also shows the Wellfield Protection Area zones in the South Core.

The City's sanitary sewer system principal components comprise:

- 400 km of collection main
- 18,000 water and sewer customers
- 34 sewage lift stations

Stormwater

The city is known to have high year-round precipitation, averaging approximately 1,100 mm annually. With the amount of hard surfaces in the South Core (e.g. buildings, streets, parking lots, etc.), as well as the impacts from the elevated topography to the south, managing stormwater runoff is critical. This is done primarily through underground stormwater pipes on the north-south streets, occasionally capturing the runoff from larger developments along the east-west streets (the map on the previous page locates stormwater pipes in the South Core).

Addressing the runoff from the higher elevated neighbours to the south, a majority of this stormwater is diverted prior to entering the South Core or shortly thereafter. Specifically, runoff from properties south of Albert Street and west of York Street is diverted towards Odell Park and through a piped system where it connects with Phyllis Creek and carried subsequently to the river. Runoff from properties south of Beaverbrook Street and east of York Street is diverted east along the Cross Town and Lincoln Trail into a piped system leading to a sedimentation pond adjacent to Waterloo Row and then out to the river. This portion of the stormwater system notably receives runoff from the University of New Brunswick and south hill and is sized appropriately for this increased capacity.

Flood Mitigation

Historically, the South Core has not experienced the same types of impacts from river flooding compared to the City Centre and the Waterloo Row neighbourhood to the east. Flood extents typically do not reach areas south of George Street or west of the Lincoln Trail. However, higher river levels can flood the storm system in the South Core, which inundate the area. Flood waters tend to back up through the stormwater system and cause issues below grade.

Given the likelihood of more intense floods through the impacts of climate change, it will be important to continue monitoring existing flood mitigation measures. This includes the existing provision in Zoning By-law Z-5 that requires all residential dwellings to be located above a geodetic elevation of 9.0 metres. Most of the South Core is above this elevation threshold, specifically areas that are expected to receive a higher density of new residential development. For new & existing dwellings in the South Core, drain tile connections to the storm sewer have been restricted due to the backwater conditions from river flooding.

Additional mitigation measures are not anticipated to be required for the lifecycle of the South Core Secondary Municipal Plan. The City has also engaged with the community in recent years - most recently in 2018/19 - to keep residents informed and provide the tools to help reduce risks associated with river flooding.



Map of Flooding Extents in the South Core

- Combined Lower Saint John River Flood Extent
- 1973 Flood Extents

The above map locates the combined Lower Saint John River flood extent and the 1973 flood extents in relation to the South Core (outlined in the black dashed line).

Electricity

The City of Fredericton's electric utility provider is NB Power, a crown corporation that is responsible for most of the province's generation, transmission, and distribution of electricity. NB Power operates a substation at 437 Aberdeen Street, which is located within the Plan Area and powered by the Mactaquac Plant and Terminal. The substation has had a recent upgrade, which allowed for the divestiture of the Carleton Street substation adjacent to the library. This is the only property in the South Core zoned for 'Infrastructure', and in turn, development on directly adjacent properties has been limited.

As noted in the province's Clean Energy Report (Powering our Economy and the World with Clean Energy: Our Path Forward to 2035), demand for energy is increasing due to economic development, electrification, and population growth. The number of severe weather events is also increasing, which has resulted in widespread power outages in the region. These events highlight the importance of energy distribution in the city. In 2019, the province announced the Fredericton South Reliability Project, highlighting this vulnerability to power outages due to transmission capacity issues. New transmission lines to the Rainsford Lane substation have received an Environmental Impact Assessment certificate but have yet to be constructed.

Most of the electric utility infrastructure in the South Core is above ground, which plays a role in the overall aesthetic of the public realm. While there are stated goals to in the Municipal Plan to encourage NB Power to implement a more 'urban' approach to the installation of infrastructure and to collaborate with utility providers to place services underground in the Urban Core, transitioning existing above-ground services to underground is very resource-intensive. Furthermore, they become more difficult to replace or upgrade. However, once underground, the infrastructure is more protected from extreme weather events, the quality of transmission and distribution is improved, and the streetscape appearance is enhanced.

An Electric Vehicle Strategy for the city is currently being drafted to provide guidance on public charging availability and support household charging. The higher number of converted dwellings paired with the current limited off-street parking availability in the South Core will likely provide some challenges if the trend of electric vehicle ownership increases as expected. Energy demand will increase with the expectation that household charging will be the predominant form of charging.



Aberdeen Street Substation

5.3 Climate Change

The consequences of climate change affects the environment and infrastructure that residents depend on. Seasonal temperatures and precipitation patterns are fluctuating at an increased rate causing a greater number of extreme weather events, raising the potential for flooding, heat waves, drought, and winter rain, and forcing people to adapt or risk more hazardous outcomes in the future.

The City has released the Climate Adaptation Plan in 2019 and two companion documents in 2021 to address Climate Change and greenhouse gas emissions: the Community Energy and Emissions Plan and the Corporate Energy and Emissions Plan. Each document outlines key initiatives to reduce the amount of electricity and natural gas, and all aspects relevant to the South Core Secondary Municipal Plan process are detailed below. Together they form a strategy to both “minimize future changes to the climate and help us prepare for the inevitable changes that are to come”.

Goals and initiatives are being tracked through an online dashboard linked through the City’s website.

Climate Adaptation Plan

Integrating climate change adaptation considerations into City plans is one of the principle ‘cross cutting’ actions from this document. It is important for this lens to be applied to the subsequent planning process. For example, using other actions to help illustrate this point:

- natural and constructed shade can be incorporated to improve community health and safety;
- expand the Active Transportation Network to mitigate disruptions due to climate change impacts; and
- enhance green space, ecosystem corridors and tree canopy, particularly as it relates to stormwater management.

Implementation is anticipated until ~2025 although many actions are related to ongoing, multi-year initiatives, and as of Summer 2024, 61 of 68 actions have been taken of which 4 have been completed.

Community Energy and Emissions Plan

The Community Energy and Emissions Plan focuses on addressing community-based emissions in the efforts of achieving net zero by 2050 in waste, transportation, buildings, and land use. Relevant actions include:

- increase transit utilization and traffic flow;
- encourage the development of near-net zero buildings; and,
- increase bicycle parking facilities and Active Transportation Network connections with new residential development.

To date, 30 of 58 actions have been taken of which 4 have been completed.

Corporate Energy and Emissions Plan

The Corporate Energy and Emissions Plan presents actions that relate to corporate emissions (e.g. fleet, city-owned buildings, etc.) and therefore are not as directly relevant to the South Core Secondary Municipal Plan. Decarbonization of high emitting facilities like Lady Beaver Brook Arena and York Fire Station are among the buildings identified for a 50% reduction in greenhouse gas emissions. The time-year horizon for this document is 2022 to 2030 and to date, 35 of 45 actions have been taken of which 12 have been completed.



6 Key Themes

Introduction

The team's review of the present and future economic, social and physical conditions of the South Core Plan Area and the consideration of input received through the various forums for engagement have helped to shape four lenses through which of the South Core's is being examined.

During the next phase of the work, the team will develop draft public realm and urban design guidelines and planning policy to direct the growth and continued development in the South Core Secondary Municipal Plan Area. Over the next few months the team will organize the draft guidelines and planning policy under four lenses::

- Character of the South Core;
- Building in the South Core;
- Greening the South Core; and,
- Moving Around the South Core.

Each lens or theme is described in this chapter.

6.1 Character in the South Core

Two primary Character Areas have been identified based on their historic residential and industrial functions and where those characteristics are still reflected in their buildings, streetscapes, and landscapes. The intent of the plan is to ensure that change in the South Core is calibrated based on the existing and emerging character of the neighbourhood, whether in the Residential or Rail-Adjacent Areas.

The **Residential Character Area** has a relatively consistent grid pattern, with long narrow building lots fronting the east-west streets and shallow lots fronting the north-south streets. The most common building typology is the detached house, which is expressed in a variety of architectural styles. More recently, many of these houses have been converted from single-family homes into multi-unit apartments. One of the area's defining features is its mature tree canopy, a network of tree-lined boulevards and rear-yard trees which provide a green setting around the built fabric. The four Avenues — Smythe, York, Regent, and Dundonald Streets — are distinguished from the rest of the area by their greater intensity of uses, wider streets designed for automobile traffic and variation in building typologies, with civic/institutional buildings, infill developments, and detached houses located side by side. In the Residential Character Area change will occur incrementally and surgically, ensuring that the valued elements of the neighbourhood are maintained. The mature trees and garden areas will be protected as will the scale of buildings. On the Avenues more significant change is anticipated as main street style mixed-use buildings are introduced to support the growing number of people living in the South Core.

The **Rail-Adjacent Character Area** is comprised of two former railway corridors — currently the Cross Town, Lincoln, and Valley Trails — and the former railway yards and adjacent industrial lands. In contrast to the Residential Character Area, this area is characterized by its coarse-grained urban fabric, with an irregular block pattern, large lots, and large building footprints interspersed among surface parking lots and brownfield sites. The remnant railway and industrial landscape is the area's defining feature. It comprises the trail network which replaced the railway lines, the collection of late-19th and early-20th-century industrial buildings, and the former Canadian Pacific Railway Station at York Street, which is the focal point of the area. In the post-railway era, many of the brownfield sites were redeveloped with low- and mid-rise apartments, large commercial buildings, and new civic/institutional buildings, which have contributed to a layered urban landscape. The most significant change is anticipated in the Rail-Adjacent Character Area. These former industrial lands that currently provide for single use large format retail and commercial uses will transition to a medium density mixed use neighbourhood. The transformation will give priority to pedestrians, cyclists and transit users while accommodating other forms of transportation. New streets will be introduced creating a fine grain network for ease of access, taking cues from the Residential Character Area.



Preliminary Map of Proposed Character Areas in the South Core (to be further refined in the final plan)

- Residential
- Avenues Sub-Area
- Rail-Adjacent

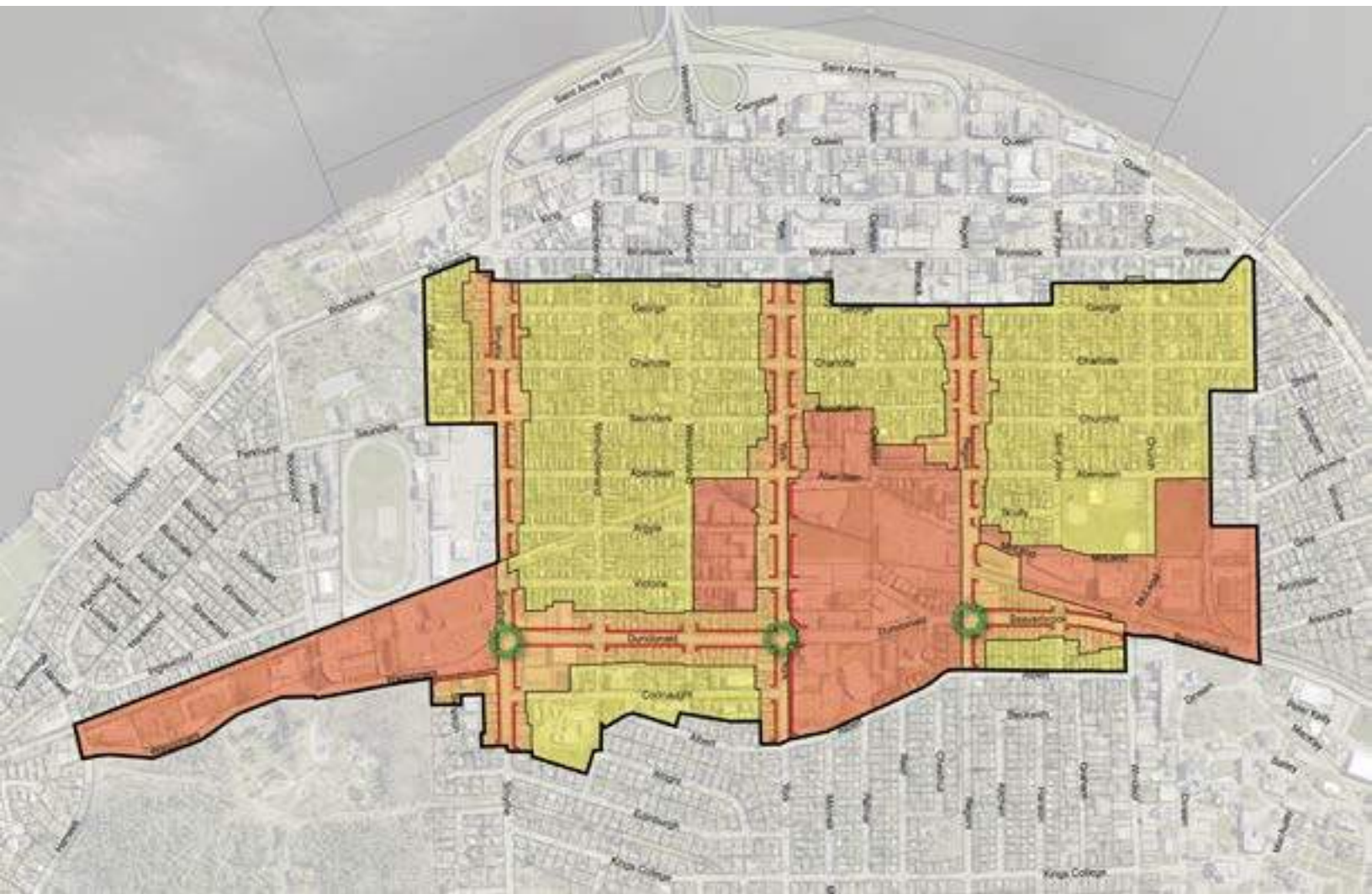
6.2 Building in the South Core

Growth in the South Core will be accommodated in a variety of building types and forms. These will vary depending on the location and context of the development, however they will generally be aligned according to the two main South Core character areas identified in Chapter 2 based on their historic residential and industrial functions – the Residential Character Area and the Rail-Adjacent Character Area. These Character Areas have been adapted to form three Infill and Intensification Areas: the Neighbourhood Infill Area, the Avenues Intensification Area, and the Primary Intensification Area. From small-scale, gentle residential infill in the Neighbourhood Infill Area to larger-scale, mixed-use development in the Primary Intensification Area, the Avenues Intensification Area offers the opportunity for modest forms of intensification while providing a transition between the two.

In the **Neighbourhood Infill Area**, the defining attributes of the housing forms and styles, as well as their relationship to the public realm and landscaped areas shall be considered when shaping new buildings and additions. The key themes for development in these areas is to promote buildings that are compatible in scale and expression within their context, to avoid overwhelming existing buildings, to minimize the impacts of parking on lots and to preserve and amplify the tree-lined streets and lushly vegetated yards that define the area.

The **Avenues Intensification Area** not only provides a transition from the Neighbourhood Infill Area to the Primary Intensification Area, but also serves to frame the major streets in the South Core. The Avenues provide the opportunity for creating storefronts along the streets and housing above the stores. The key theme for development in the Avenues Intensification Area is to promote street-oriented, pedestrian-scaled buildings that animate and support a vibrant streetscape.

The **Primary Intensification Area** offers the greatest opportunity to accommodate significant density in the South Core. The area is generally buffered from the Neighbourhood Infill Area by the Avenues Intensification Area and is thus envisioned to consist of building forms that are of greater scale and massing than found in the other areas of the South Core. The Primary Intensification Area also offers the opportunity to extend the existing grid of public streets through the central portion of the Plan area, and to create new public spaces and trails as part of the public realm network. Development in the Primary Intensification Area is anticipated with buildings that reinforce the streets, public spaces and trails.



Map of Proposed Built Form Framework in the South Core

- Neighbourhood Infill Area
- Avenues Intensification Area
- Primary Intensification Area
- Built Form Frontage
- Gateway Landmark

6.3 Greening in the South Core

With the projected increase in the number of people living in the South Core there will be more pressure on the public realm to provide amenities and infrastructure that support the needs and desires of the existing and future residents. Critically important will be protecting and enhancing the existing character elements that contribute to the neighbourhood's sense of place including the boulevards and mature trees that line the streets, the treed back yards and discrete side yard driveways. A new street hierarchy is envisioned that introduces Primary Civic Boulevards, including Regent, York, Smythe, and Dundonald / Beaverbrook Streets. Green Streets are a secondary category emphasizing walking and cycling off the main thoroughfares including Northumberland, Westmoreland, Carlton, St John, Church, University, Charlotte and Aberdeen. All other streets are considered Neighbourhood Boulevards that preserve and enhance the existing character elements.

For the Rail Adjacent Lands Character Area and Avenues Character Area where more significant built form changes are anticipated, the greening approach will take cues from the beautiful streets of the South Core but will add new public realm typologies that align with increased densities and new built form typologies. Those will potentially include publicly accessible mid-block connections, publicly accessible private open space, new public parks and private outdoor amenity areas for residents.

The anticipated growth will impact the existing parks, including Rabbit Town Park, the Smythe Green and University Green, that will need to provide more varied amenities for residents, ensuring access for all ages and abilities. The anticipated master plan for Queen's Square will assess the value and condition of the existing park amenities and infrastructure and provide and new refreshed vision for the park responding to the changing context of the South Core. The plan will also provide design direction for Church Street, as it crosses through the park. The Cross Town Trail will become an even more critical pedestrian and cycling connection through the South Core linking intensifying areas in the Rail Adjacent Lands Character Area to the established neighbourhoods and beyond to the surrounding neighbourhoods like Waterloo Row and Sunshine Gardens.



- Anchor Parks (outside Plan Area)
 - Anchor Parks (inside Plan Area)
 - Other Parks (outside Plan Area)
 - Other Parks (inside Plan Area)
 - Community Sports Facilities
 - Urban Forest
 - Sponge Areas
 - Civic/Institutional Buildings
 - Primary Civic Boulevards
 - Green Streets
 - Neighbourhood Boulevards
 - Extensions/Connections

6.4 Moving Around in the South Core

A hierarchy of streets provide vehicular, cycling and walking access to and through the South Core. The streets within the Residential Character Area are walkable and pedestrian friendly with sodded, planted or paved boulevards separating the sidewalks from vehicular traffic. Most of the streets in these areas provide shared cycling and vehicular travel lanes. Within the Rail adjacent Lands the streets are less pedestrian and cycling friendly and reflect the former industrial character of the area. The intent of the plan is to transform those streets to a more balanced pedestrian and cycling friendly model.

The framework plan establishes a street hierarchy based on the character areas that responds to the way people move around the South Core. The highest order streets are the Avenues, with the first order including Regent, Smythe and Dundonald / Beaverbrook Streets and second order York and Westmorland Streets. These streets are intended to provide for higher volumes of traffic at higher speeds with more urbanized cross sections.

The second order are the Green Streets including Northumberland, Carlton, St John, Church, University Charlotte and Aberdeen Streets. These streets are intended to provide for local traffic at lower speeds with shared cycling, emphasizing walkability. The large canopy trees that line these streets that are a signature character element and create natural visual friction that is effective in slowing traffic. The intent is for vehicular traffic to move slowly along these streets creating a safer and more comfortable environment for cyclists and pedestrians.

The remaining streets are categorized as Local Streets with lower volumes of vehicular traffic, favouring cyclists and pedestrians. Vehicular traffic will be encouraged to move slowly along these streets. On both Green Streets and Local Streets curbside parking will be explored to increase the parking capacity of the South Core.

The typical road width varies between 8.5 and 9.5 metres. There is limited parking provided on the streets within the South Core, although most streets are wide enough to accommodate one side of parallel parking while maintaining two lanes of travel. The Cross Town Trail provides pedestrian and cycling access east-west through South Core.



Map of Emerging Directions for Moving Around the South Core

- | | | |
|---|---|---|
| ■ Avenue | — Trails | - - Transit Routes |
| ■ Green Street | | ● Bus Stop (ADA Compliant) |
| ■ Local Street | | ● Bus Stop (Accessible) |
| ○ Signalized Intersection | | ● Bus Stop (Not Accessible) |
| X Signalized Crosswalk | | ○ Scheduled Bus Stop |
| | | ● Overnight On-Street Parking |



APPENDIX Community Profile

Data Source

The analyses performed in this section used ArcGIS Community Analyst, where data compiled by Environics-Analytics is used to create more specialized demographic datasets and information for non-Census years by creating estimates. The data compiled by Environics-Analytics includes census data from Statistics Canada.

The boundaries of the Plan Area do not align with the boundaries of the existing census geographies. Census geographies are the different levels of geographic areas that Statistics Canada has defined in order to study the population. Not all census categories are available for all levels of geographies. For instance, housing information is not available at all levels of census geographies. Thus, using Community Analyst allows a more complete picture of the planning area, using 2023 figures as the base year.

Furthermore, with respect to census geographies, it is important to acknowledge that census boundaries do not always align with municipal boundaries. In this instance, the boundaries of the Fredericton Census Subdivision (CSD) encompass the municipal boundaries before the Local Governance Reform in January 2023 and does not include the area that makes up St. Mary's First Nation urban reserve.

Population Characteristics

The City of Fredericton has been growing at an unprecedented pace, and 9.45% of the city's population have chosen the South Core as their home (Figure 1).

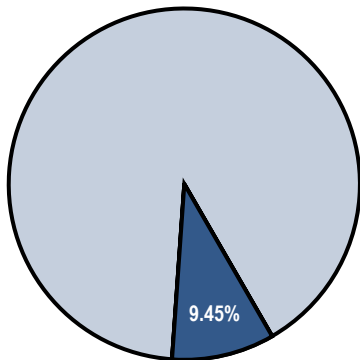


Figure 1. Population percentage in the South Core

The variety of services and amenities within the neighbourhood, as well as the proximity to the City Centre and major employers have contributed to the area's growth. Since the Residential Town Plat was implemented in 1999, the population in the South Core has grown by approximately 20%, to 6,308 residents in 2023. In the past 5 years alone, the population in the South Core has grown by more than 7%. The city's most recent population projection has surpassed the Growth Strategy's number, as the new projection now sees the population grow to approximately 108,000 by 2041 and 114,000 people by 2051, with employment identified as the key driver for this growth. Accordingly, a key component of the South Core Secondary Municipal Plan will be managing significant population growth in the Plan Area while balancing the existing built form that is characteristic of the area.

The following section provides an overview of the people who live in the South Core and how the Plan Area compares in the context of the City of Fredericton.

A Younger Population

The Plan Area has a relatively young population when compared to the city as a whole. The median age in the Plan Area is 34.2 years of age, whereas the median age in Fredericton is 38.8 years, which is still relatively young when compared to the median age of the province (46.3 years of age). This is not unusual for areas closer to a City Centre, which tend to have a younger population, more compact housing forms and, thus, higher population densities.

Figures 2 and 3 show population pyramids for the Plan Area and for Fredericton. Population pyramids are visual representation of the structure of the population, separated by age and sex. The age of the population is grouped in 5-year groups or cohorts.

The population pyramid of the Plan Area shows that the largest cohorts are aged 20-39 years. While other age cohorts have remained relatively consistent since 2018 and are relatively proportional to figures of the city, the proportion of the population aged 20-39 years in the Plan Area (49%) is almost double that of the city. More specifically, the Plan Area has seen a 4.5% increase in the population aged 25-39 years since 2018. This increase can be due to the younger cohorts in 2018 having continued to reside in the South Core, or it can be due to having an increased number of younger adults choosing to move to the area. While this can be explained in part by the South Core's proximity to the downtown area, the availability of rental housing in the area may also play a role, as the population aged 20-39 years is also more likely to rent than to own their housing.

While the population above 65 years of age is relatively smaller than the cohorts aged 20-39 years, policy guidelines for residential uses in the existing Residential Town Plat Secondary Plan highlight the importance of having housing units that also address the needs of seniors. This is especially as older adults decide to downsize and move to neighbourhoods closer to services and amenities, like the South Core.

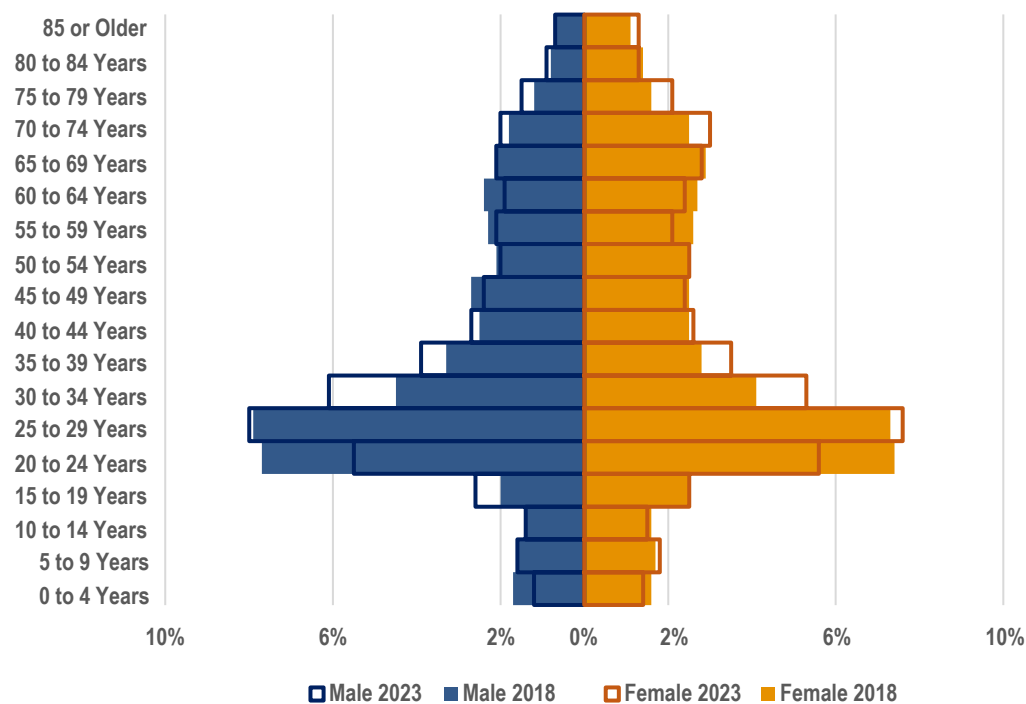


Figure 2. Population pyramid of the Plan Area

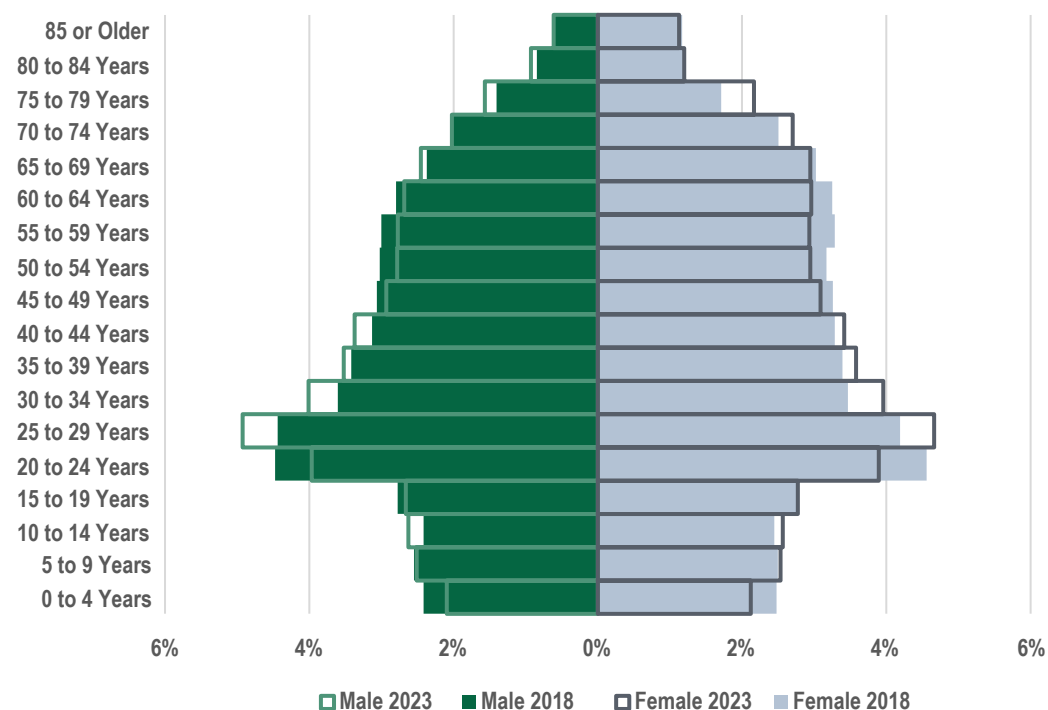


Figure 3. Population pyramid of the City of Fredericton

New Residents

Many new residents to Fredericton have chosen to live in the South Core. Indeed, 62.96% of residents in the Plan Area have moved there in the past 5 years from within the city, the province, the country, or internationally. In comparison, 46.72% of residents in Fredericton have indicated moving in the past 5 years. The higher proportion of rental dwellings in the Plan Area and a younger population driven to the city by employment opportunities in the city are important contributing factors to a growing South Core.

Additionally, new residents have also had an impact in the diversity of the neighbourhood. In the Plan Area, 8.33% identify as a visible minority, compared to 11.43% in the rest of the city. While the figure in the Plan Area is smaller, it is higher than the provincial figure at 4.04%.

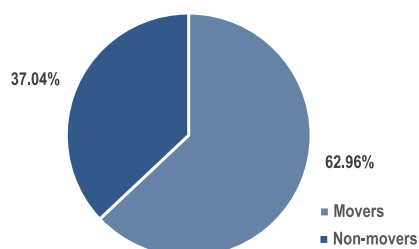


Figure 5. Plan Area Migration

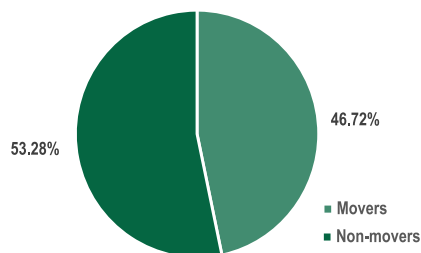


Figure 6. City Migration

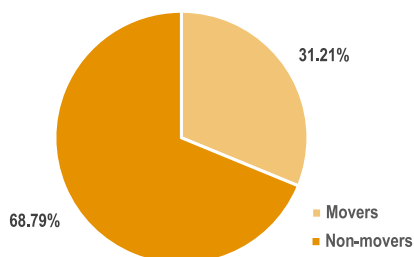


Figure 4. Provincial Migration

Smaller Households

The average household size in the Plan Area is 1.9 persons, which is smaller than that of the city's average household size of 2.2 persons. As shown in Figures 8 and 9, the Plan Area is characterized by having a higher proportion of 1- and 2-person households when compared to the rest of the city, with these smaller households constituting almost 80% of households in the Plan Area. When compared to 2018, there has been a slight increase in the number of 1- and 2-person households in the Plan Area.

While the household size can be influenced by individuals' choice to have smaller families, other factors can also impact this number, such as the type and cost of housing available in the Plan Area, as well as the types of services and amenities available.

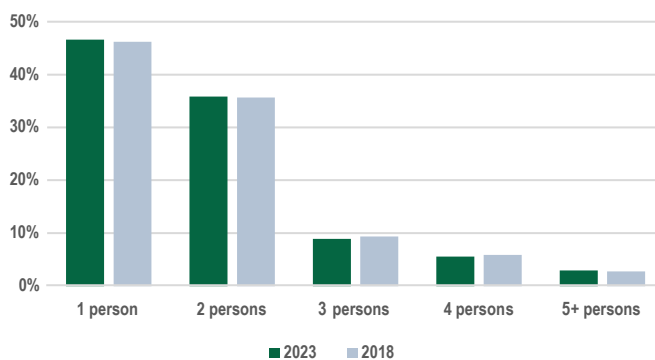


Figure 5. Household size in the Plan Area

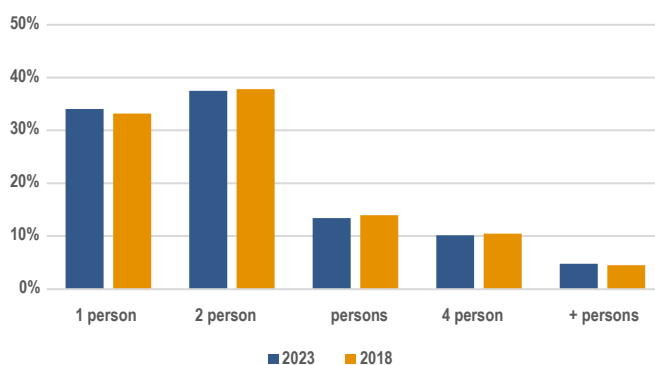


Figure 6. Household size in the City of Fredericton

Education, Income and Labour

The population in the Plan Area is relatively well-educated when compared to the city. As seen in Figures 7 and 8, around 51% of the population in the Plan Area have a University degree, compared to 40% in the city. This can be attributed to professionals moving to the city to work, as well as the proximity of the Plan Area to the various post-secondary institutions in the city.

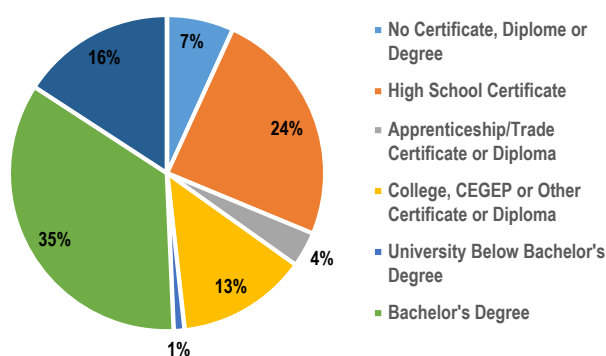


Figure 7. Household size in the Plan Area

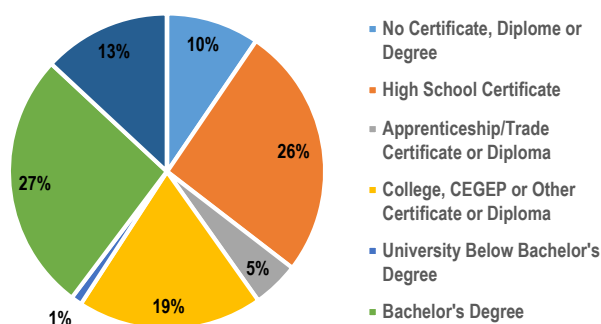


Figure 8. Household size in the City of Fredericton

While a higher education level is usually related to higher income levels and employment rates, this is not necessarily the trend in the Plan Area, in part due to its younger adult population. As seen in Table 1, the median household income in the Plan Area is \$69,891.70, which is lower than the City and Province's median incomes. Having a larger young adult population means that the household composition may be smaller, and that there may be residents who are earlier in their careers, and thus, earn less when compared to those who are further advanced in their careers. Figure 9 shows the distribution of income in the Plan Area compared to the city.

	2018	2023
Plan Area	\$ 56,133.94	\$ 69,891.70
Fredericton (CSD)	\$ 67,219.47	\$ 80,264.71
New Brunswick	\$ 65,534.69	\$ 74,517.32

Table 1. Median income (2023 dollars)

Similarly, although the employment rate in the Plan Area is lower than that of the city, its participation rate is higher. This means that there is a higher percentage of people in the Plan Area who are in the labour force, which includes those who are of working age and who might be employed or unemployed. The higher participation rate can also be explained by its relatively younger adult population and a relatively smaller proportion of residents who are not of working age, are retired, or not looking for work.

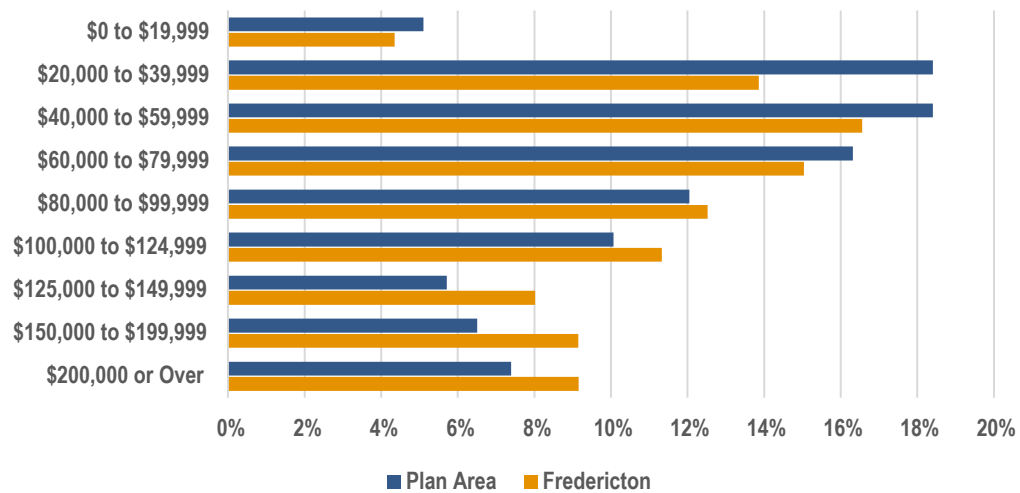


Figure 9. Income level comparison

	Plan Area	Fredericton
Participation Rate	65.9%	62.10%
Employment Rate	90.4%	92.4%
Unemployment Rate	9.6%	7.6%
Not in Labour Force	34.1%	37.9%

Table 2. Employment status

Commuting to Work

Active transportation and public transit play a key role for residents in the Plan Area. The proximity and connectivity of the Plan Area to the City Centre and to major employers have led to a higher proportion of residents who prefer methods of transportation other than driving a car. This is especially important as employment has been a key driver for population growth in Fredericton.

Figure 10 compares the modes of transportation for residents commuting to work in the Plan Area and the City of Fredericton. More than 30% of residents in the Plan Area commute to work by walking, biking or public transit, compared to just 16% for the city overall. Walking is particularly notable, as around 25% of residents in the Plan Area have indicated walking to work, which is

almost triple that of the City of Fredericton (9%). Although driving to work remains the most popular mode of transportation, this proportion is much lower in the Plan Area with 54.3% of residents driving, compared to the city as a whole (75.5%).

The decline of private vehicle use for daily mobility also has an implication on the cost of living in the city, as vehicle ownership presents an additional cost to residents. With the Municipal Plan’s “pedestrian first” principle for the Urban Core and the Plan Area’s central location, the South Core can continue to be characterized as a walkable, complete and vibrant neighbourhood.

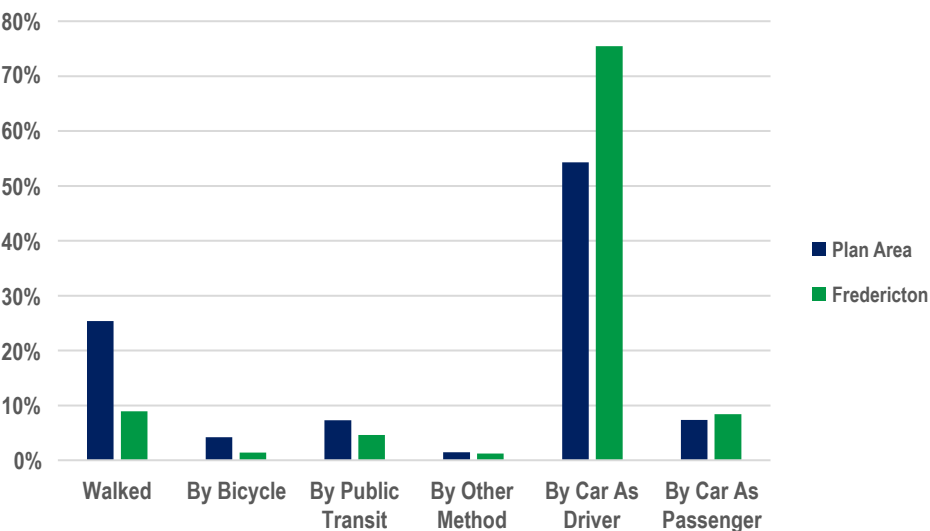


Figure 10. Work commute modes of transportation

Housing

The South Core is characterized by having a larger proportion of multi unit buildings and renters when compared to the same figure for the city. As shown on Figure 11, more than 66% of residents in the Plan Area live in multi unit buildings, with the majority living in buildings of 5 stories or less, which includes converted dwellings. Converted dwellings are located throughout the Plan Area. Converted dwellings are buildings that maintain the façade of the existing single-detached house, but the interior is divided and converted to have between 2 and 6 units. Converted dwellings have been a key way in which the Town Plat has been able to accommodate a growing population without

compromising the architectural integrity of the existing housing stock. At the same time, the Municipal Plan identifies the South Core as an important area for residential intensification due to its proximity to the City Centre, and encourages greater intensification along Smythe Street, York Street, and Regent Street in the form of low and mid-rise buildings.

Single-detached houses represent 18% of the housing in which residents in the Plan Area live in. In comparison, single-detached houses constitute 41.4% of housing type in the city, as shown in Figure 12. This demonstrates a more compact neighbourhood form that has taken shape in the South Core.

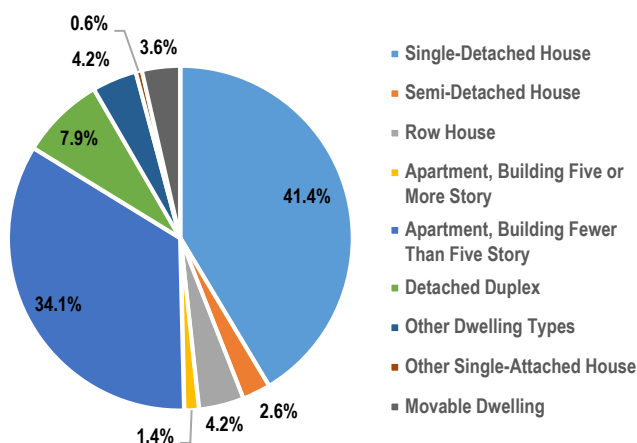


Figure 11. Plan Area Dwelling Types

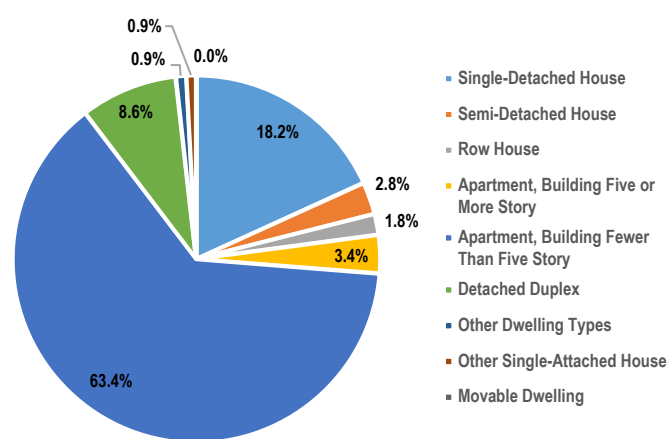


Figure 12. City Dwelling Types

With apartment buildings being the predominant housing form in the Plan Area, it is not surprising that a majority of residents are renters. Figures 13 and 14 show that 75% of residents in the Plan Area are renters, whereas homeowners only represent 25% of the population. In comparison, renters only represent 42% of the city's population, whereas homeowners represent 58%. With less ownership in the Plan Area, it means that residents might have more flexibility to move elsewhere. This aligns with having a younger population that might also have less ability to own a home due to a lower income level.

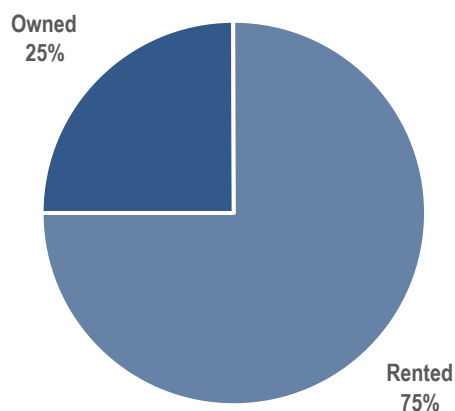


Figure 13. Plan Area Housing Ownership

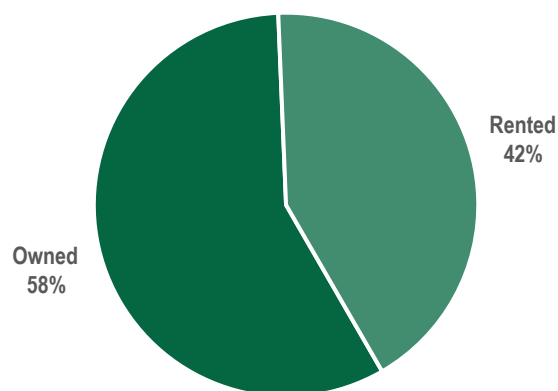


Figure 14. City Housing Ownership

With the Town Plat being one of the older areas in the city, it is no surprise to see that the housing stock is relatively older than the rest of the city. Figure 15 shows the age of the housing stock in the Plan Area and compares it to the city for context. More than 55% of the housing stock in the Plan Area was built before 1980. Only 2% of the housing in the Plan Area was built after 2021.

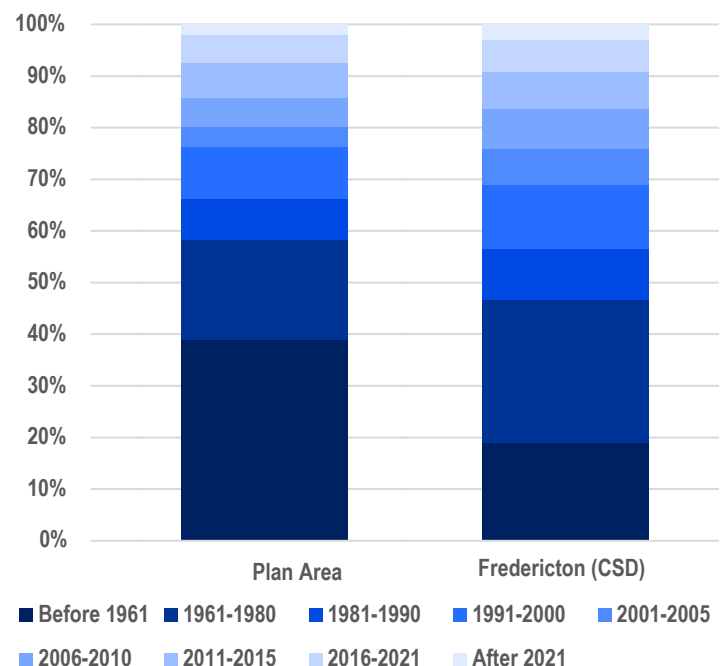


Figure 15. Dwellings by Year Built

FREDERICTON

SOUTH CORE PLAN



To: Planning Advisory Committee

From: Fredrick Van Rooyen, Senior Planner

Proposal: Tentative plan of subdivision to add land to the public right-of-way (Regent Street)
Assent of Council is required.

Property: **231 Regent Street (PID 75493858)**

OWNER: Cedar Valley Investments Limited
527 Dundonald Street, Suite 506
Fredericton, NB
E3B 1X5

APPLICANT: City of Fredericton c/o Ryan Seymour
397 Queen Street
Fredericton, NB
E3B 1B5

SITE INFORMATION:

Location: Northwest corner of Charlotte and Regent Street

Context: Mix of multi-residential and converted dwellings, office and mixed-use buildings along Regent Street and surrounding Town Plat neighbourhood

Ward No: 10

Municipal Plan: South Core

Zoning: Office Commercial (OC) Zone

Existing Land Use: Office building

Previous Applications: None

EXECUTIVE SUMMARY:

The Applicant is proposing a tentative plan of subdivision to add a 75.6 square metre parcel of land from 231 Regent Street to the public right-of-way (Regent Street). The City has previously identified a future widening of Regent Street along this location to provide a left-hand turning pocket to Charlotte Street and install a signalized intersection. The proposed subdivision adds the necessary land to the public right-of-way to accommodate the intersection upgrades and adjustments to the sidewalk location. The remnant lot complies with the minimum lot requirements in the OC Zone. Overall, staff support the application subject to terms and conditions.

APPLICATION:

Ryan Seymour, on behalf of the City of Fredericton, has made application on property located at 231 Regent Street for a tentative plan of subdivision to add a 75.6 square metre parcel of land to the public right-of-way (Regent Street).

PLANNING COMMENTS:

Proposal:

- The City of Fredericton is proposing to add a 75.6 square metre parcel of land to the Regent Street right-of-way as shown on Map III. By adding the necessary land to the public right-of-way it allows the City to make the intersection upgrades and adjust the location of the sidewalk given the future left hand turning pockets to Charlotte Street as part of the future signalization.

Discussion:

The property is zoned Office Commercial (OC) Zone in *Zoning By-law Z-5*. With the proposed tentative plan of subdivision, the remnant lot relates to the OC zone lot requirements as follows:

Standard	Required	Provided
Lot Area (min)	900m ²	1,903m ²
Lot Frontage (min)	30m	49m
Lot Coverage (max)	55%	22%
Front Yard Setback (min)	3m	5.63m

- The proposed subdivision will not result in any deficiencies to the subject property with regards to the standards in the OC zone. The Community Planning Act requires that the Planning Advisory Committee recommend the location of a proposed street to City Council before assent is given. Based on the above, staff are prepared to support this application.

Engineering & Operations:

- The City has been working on a long-term initiative to provide two through northbound lanes on Regent Street to accommodate present and future transportation demands. Part of this work includes the addition of left turn pockets on Regent Street for Charlotte Street and installation of a signalized intersection at this location. Much of this work has been completed along the corridor where land was available. As shown on the tentative plan of subdivision, the City would be acquiring an approximately 2.0 metre wide portion along the Regent Street frontage to accommodate these future upgrades.

RECOMMENDATION:

It is recommended that the application submitted by Ryan Seymour, on behalf of the City of Fredericton, on property located at 231 Regent Street for a tentative plan of subdivision to add a 75.6 square metre parcel of land to the public right-of-way (Regent Street) be forwarded to City Council with a recommendation that the location of the public street as shown on Map III attached to P.R. 69/25 be approved.

Additional Information

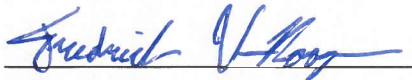
Pursuant to Section 77(1) of the Community Planning Act, the following terms and conditions will be imposed on the subdivision by the Development Officer:

- a) The final plan of subdivision be submitted substantially in accordance with Map III attached to P.R. 69/25 to the satisfaction of the Development Officer.

It is further recommended that City Council adopt the following resolution:

BE IT RESOLVED THAT the final plan of subdivision prepared by Surtek Group Ltd. entitled Cedar Valley Investments Limited Showing Regent Street (Public), 231 Regent Street, City of Fredericton, County of York, Province of New Brunswick, receive the Assent of Council pursuant to Section 88(4) of the *Community Planning Act*.

Prepared by:

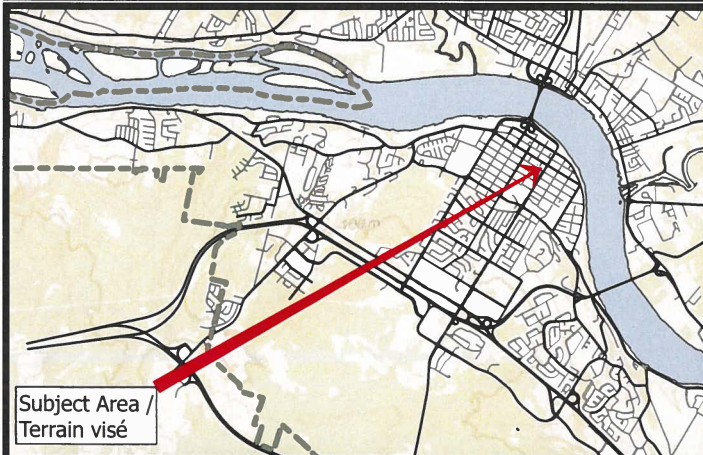
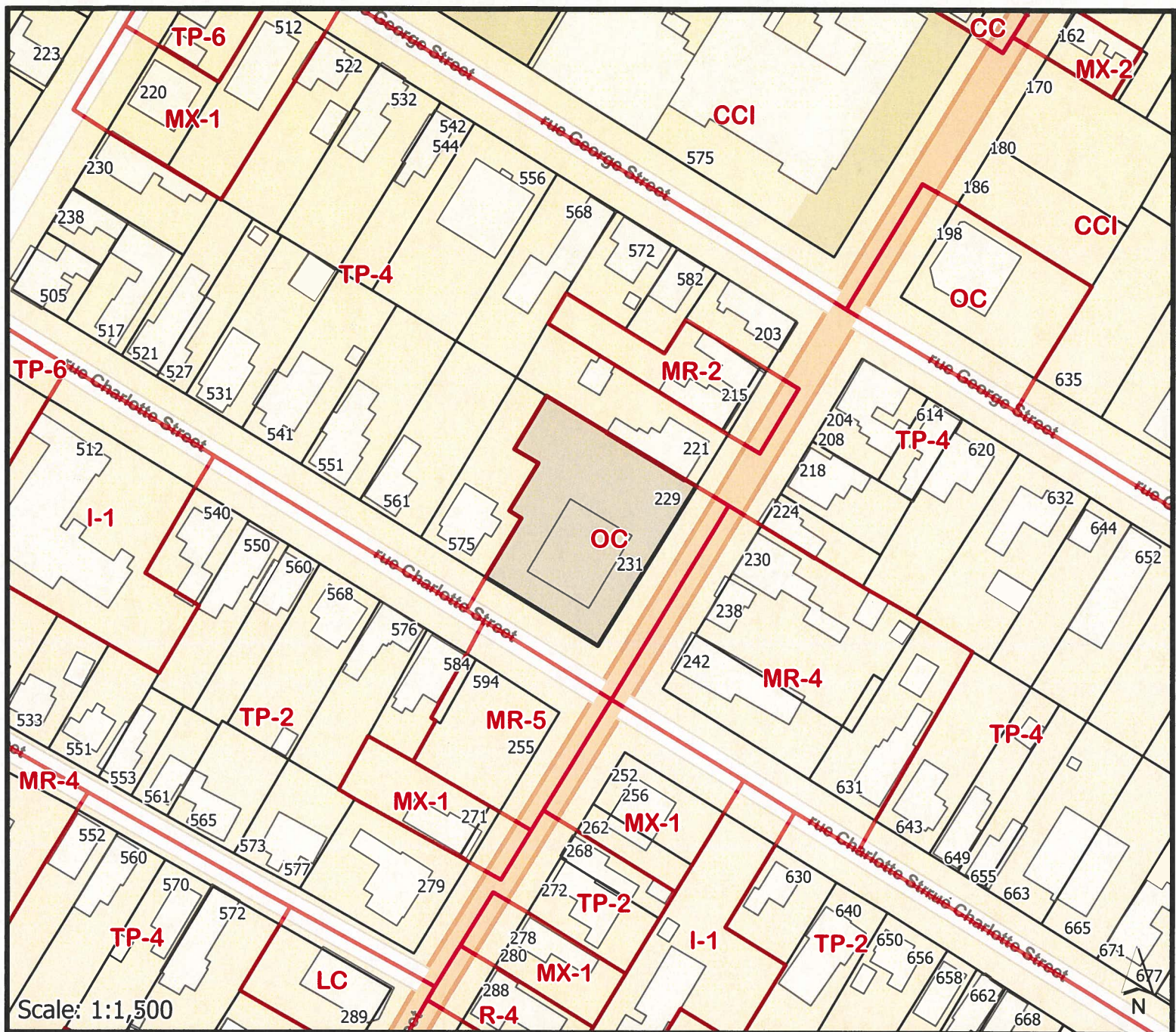
A handwritten signature in blue ink, appearing to read 'Fredrick Van Rooyen', written over a horizontal line.

Fredrick Van Rooyen, RPP, MCIP
Senior Planner, Community Planning

Approved by:

A handwritten signature in blue ink, appearing to read 'Marcello Battilana', written over a horizontal line.

Marcello Battilana, RPP, MCIP
Assistant Director, Planning & Development



 Subject Property / Propriété visée

Tentative plan of subdivision to add a 75.6 m² parcel of land to the Regent Street right-of-way from a portion of PID 75493858.

Plan de lotissement provisoire visant à ajouter une parcelle de 75,6 m² à l'emprise de la rue Regent à partir d'une portion de la propriété portant le NID 75493858.

Fredericton

Community Planning
Planification urbaine

Map \ carte # I

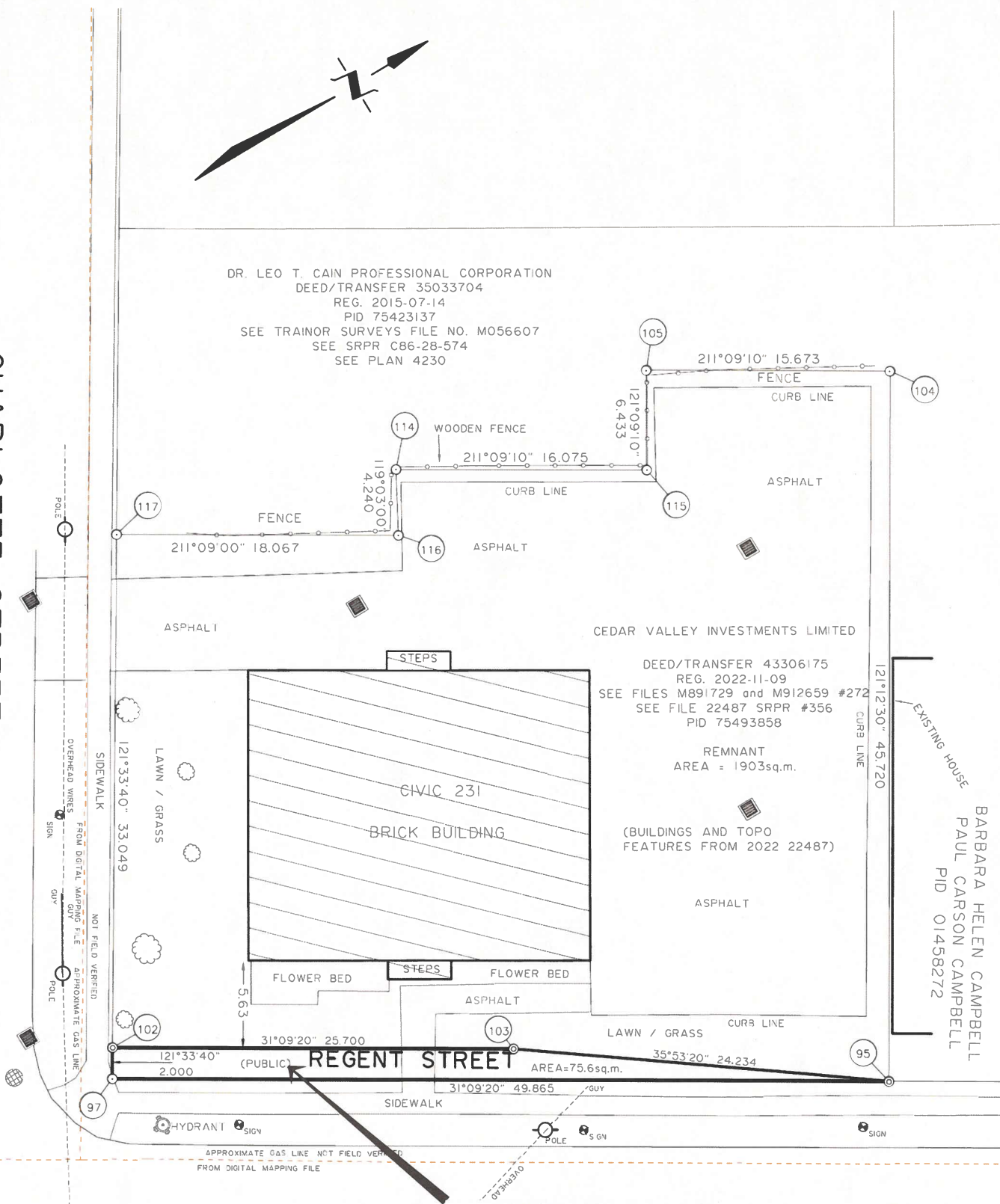
File \ fiche: PR-69-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: rue 231 Regent Street

City of Fredericton 383
(c/o Ryan Seymour)

CHARLOTTE STREET
CORNERSTONE SURVEYS - DRAWING NO. 12-C-005-B3
WIDTH VARIES (PUBLIC)



Site Plan / Plan du site

Fredericton

Community Planning
Planification urbaine

Map \ carte # II
File \ fiche: PR-69-2025
Date \ date: août \ August 20, 2025
Subject \ sujet: rue 231 Regent Street
City of Fredericton
(c/o Ryan Seymour) 384

To: Planning Advisory Committee
From: Melisa Tang Choy, Planner
Proposal: Temporary use variance to permit Group Home and Office use
Property: 348 Gibson Street (PID 75446278)

OWNER: Emily Parizeau & Barry Roberts
348 Gibson Street
Fredericton, NB E3A 4E6

APPLICANT: S.T.A.R.S. Children's Home (c/o Sue Ellen English)
301-82 Westmorland Street
Fredericton, NB E3B 3L3

SITE INFORMATION:

Location: Interior lot on the south side of Gibson Street, between Maclaren Avenue and Ashfield Street

Context: Predominantly low-rise residential neighbourhood, with Devon Lumber to south

Ward No: 6

Municipal Plan: Established Neighbourhoods

Zoning: Multi-residential Zone Two (MR-2)

Existing Land Use: Single-detached dwelling

Previous Applications: N/A

EXECUTIVE SUMMARY:

The Applicant is looking to establish a centre (S.T.A.R.S. Children's Home) that provides short-term stays and therapeutic services to children 0 to 8 years of age on the subject property. This would be a pilot program that would welcome up to 12 children who have experienced trauma, for a maximum 90-day stay. The children would be under the care of staff and receive specialized services onsite, including speech therapy, education assessments and psychological services. No expansion of the existing building footprint is being proposed, and most of the changes to the existing single-detached dwelling would be internal. A portion of the building would be converted into office/therapy space for the practitioners who provide services to the children and would be for their exclusive use. The subject property is zoned Multi-Residential Zone Two (MR-2) and thus requires a planning application to permit the proposed use.

Due to the nature of the work, the Applicant has indicated that the centre necessitates being located within a building of residential character rather than institutional. The temporary use variance would permit the Applicant to start making interior renovations to the existing building and would also provide an opportunity to test the location of the centre in close proximity to low-rise residential properties, before a more permanent change is sought through a Zone Amendment application. Attached to this report are a summary of the community engagement activities that the Applicant has carried out with neighbouring residents. The proposal would address a need for early intervention services in the community. Staff support the application subject to terms and conditions.

APPLICATION:

Sue Ellen English, on behalf of S.T.A.R.S. Children's Home, has applied for a temporary use variance to permit Group Home and Office use at 348 Gibson Street.

PLANNING COMMENTS:

Proposal:

- The Applicant is proposing a centre (S.T.A.R.S. Children's Home) that provides short-term stays and therapeutic services to children 0 to 8 years of age who have experienced trauma. The Applicant has been working with the NB Department of Social Development on this pilot program and would accommodate up to 12 children onsite, who would be under the care of staff at a ratio of 2 children per 1 staff for a maximum of 90 days. The centre would be comprised of 2 individual dwelling units which would hold up to 6 children per unit, and office/therapy space. The centre would not be available for public drop-ins.
- As shown on Map II, no additions are being proposed to the footprint of the existing single-detached dwelling, and the Applicant intends to maintain the residential character of the property. The majority of the changes to the building would be internal, except for the creation of a separate exterior entrance to the office space. One of the dwelling units would be located on the top floor of the house. The 3-car attached garage would be converted in order to accommodate the other dwelling unit. The basement level would be transformed into office/therapy space, which would be for the exclusive of practitioners who provide therapeutic services to the children. A fenced walkway would be created between the entrance to the garage and the fenced playground at the rear, in order to make it safer for children. 8 parking spaces are provided.
- The Applicant has indicated that they spent 8 months looking for a suitable location for the centre within and outside of Fredericton, that would have a large enough lot area, building footprint and that would be easily accessible for staff and families. Due to the nature of their work, the Applicant was looking for a property with residential character rather than a typical institutional structure, where children could better integrate in the community. The location of the property near public transit would allow for easier access to employees and to families.
- The Applicant intends to apply for zone amendment before the temporary use variance expires in one year. This temporary use variance would allow the Applicant to start making interior renovations to the building, begin operations, and allow for some time to see how

the use would fit in the neighbourhood. The Applicant has noted that most of the noise and traffic would take place during staff shift changes, but that there would not be a public drop-in.

- Staff would note that the floor plans and site plan have been updated since the neighbourhood notification letters were mailed. Originally, the floor plans showed one of the residential spaces in the basement of the house and the office/therapy space in the attached garage. Although the basement of the house was being used as a basement apartment, Staff could not find record of a permit for the basement apartment. The Applicant's surveyor has recently confirmed that the top floor of the basement level is below 9 geodetic metres in elevation. Zoning By-law Z-5 requires that the top of the finished floor of any habitable space is located above 9 geodetic metres in elevation. This has resulted in the office/therapy space being relocated to the basement level and the residential space to be located within the attached garage. The revised site plan now includes a fenced walkway from the garage portion to the playground at the rear of the property, in order to make it safer for children to walk next to the driveway.

Municipal Plan:

- The subject property is designated Established Neighbourhoods under the Municipal Plan. Section 2.2.1(19) recognizes that Group homes and homes for special care are uses that are considered complementary within Established Neighbourhoods, and that may be located in Established Neighbourhoods without a Municipal Plan amendment. The proposed Office use is secondary to the proposed Group Home use.
- Regarding Supportive Housing, Section 10 of the Municipal Plan encourages the provision of housing for residents with special needs that is sensitively located in appropriate neighbourhoods with easy access to transit, essential services, community facilities, and employment centres. Particular to Group Homes, the Municipal Plan outlines that,

Council shall facilitate the integration of group homes into all residential areas and shall prescribe regulations in the Zoning By-Law to:

- (a) Maintain an adequate separation distance between group homes;*
- (b) Maintain compatibility with surrounding residential uses; and,*
- (c) Ensure the adequate provision of on-site parking, landscaping, and green space.*

- The proposed use would be located within the existing single-detached dwelling and would maintain the appearance of a single-detached dwelling. The subject property is accessible by public transit and is located in close proximity to several businesses and services, including a dental clinic and a convenience store.

Zoning By-law:

- The following zoning standards are outlined for Group Homes in Zoning By-law Z-5:
 - i. maintain a minimum radius of 300 metres to another group home for those areas not within the downtown area as identified in Section 7.3(4)(a)(ii); and,
 - ii. require 1 on-site parking space per 4 beds, plus 1 on-site parking space per 2 employees on a maximum shift.
- Staff have reached out to Social Development to confirm whether the existence of group homes within a 300 metres radius of the subject property. Staff have not received a

response at the time of writing this report. The intent of this standard is to minimize potential impacts on neighbouring residential uses, and to not concentrate group homes in one place. Given that the centre would be home to children under the age of 8 and would be under the supervision of professional caregivers, Staff do not anticipate negative impacts to neighbouring properties, except during shift changes due to the increased number of cars.

- Regarding parking, a total of 10 parking spaces would be required. 6 parking spaces would be for the Group Home use (1 space per 4 beds and 1 space per 2 staff on a maximum shift). The office portion measures approximately 200 m², which would require 4 additional parking spaces. While the site plan only shows 8 parking spaces, Staff are of the opinion that the lot would be able to accommodate additional parking if required due to the lot depth and area of the subject property. Staff will continue to work with the Applicant on a final parking plan.

Public Engagement:

- The Applicant hosted a community barbecue on August 5th in order to inform the neighbouring residents about the proposal on the subject property, which 50 people attended. To promote the event, the Applicant created a public social media event and flyers were dropped off at neighbouring properties within a 30-metre radius. The Applicant has continued to meet one-on-one with neighbouring residents and property owners to address questions, including Devon Lumber. The Applicant has reported that the comments have been generally positive and has received interest from other organizations about potential future partnerships. The Applicant has made themselves available for any further questions from neighbouring residents and has continued to engage with them.
- Attached to this report are 7 letters of support for the S.T.A.R.S. Children's Home. The letters were submitted for a funding application but the Applicant has noted that they have informed the parties that the letters would be submitted for this planning application. Any parties who did not wish to append their letter to this application have had their letter removed.

Engineering & Operations:

- Any changes to the sanitary or water service are to be to the satisfaction of the Director of Engineering & Operations.

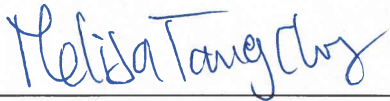
RECOMMENDATION:

It is recommended that the application submitted by Sue Ellen English, on behalf of S.T.A.R.S. Children's Home, for a temporary use variance to permit Group Home and Office use at 348 Gibson Street, be approved subject to the following terms and conditions:

- a) The site be developed generally in accordance with Map II attached to P.R. 70/25 to the satisfaction of the Development Officer;

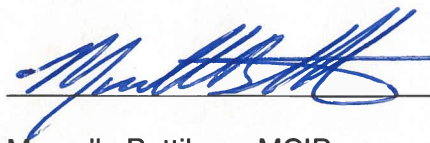
- b) Final parking and landscaping plans be provided to the satisfaction of the Development Officer prior to the issuance of a Building Permit;
- c) No habitable space be located below 9 geodetic metres in elevation;
- d) The total number of client beds be limited to 12;
- e) The Office use be limited to practitioners who provide services to the children of the Centre and shall not be use by outside practitioners, nor shall it be used for children who are not staying on the subject property;
- f) A building permit is issued for any interior renovations;
- g) Any changes to the sanitary or water service are to be to the satisfaction of the Director of Engineering & Operations.
- h) A backflow preventer is required;
- i) The temporary use variance shall expire on August 20, 2025; and,
- j) The Applicant shall obtain any applicable provincial approvals.

Prepared by:

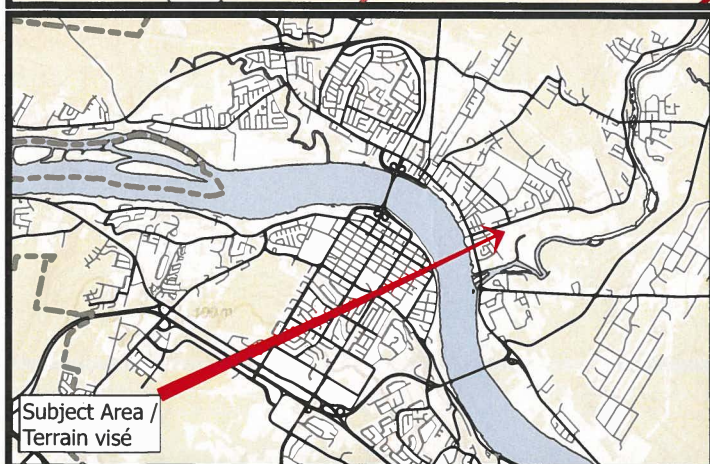
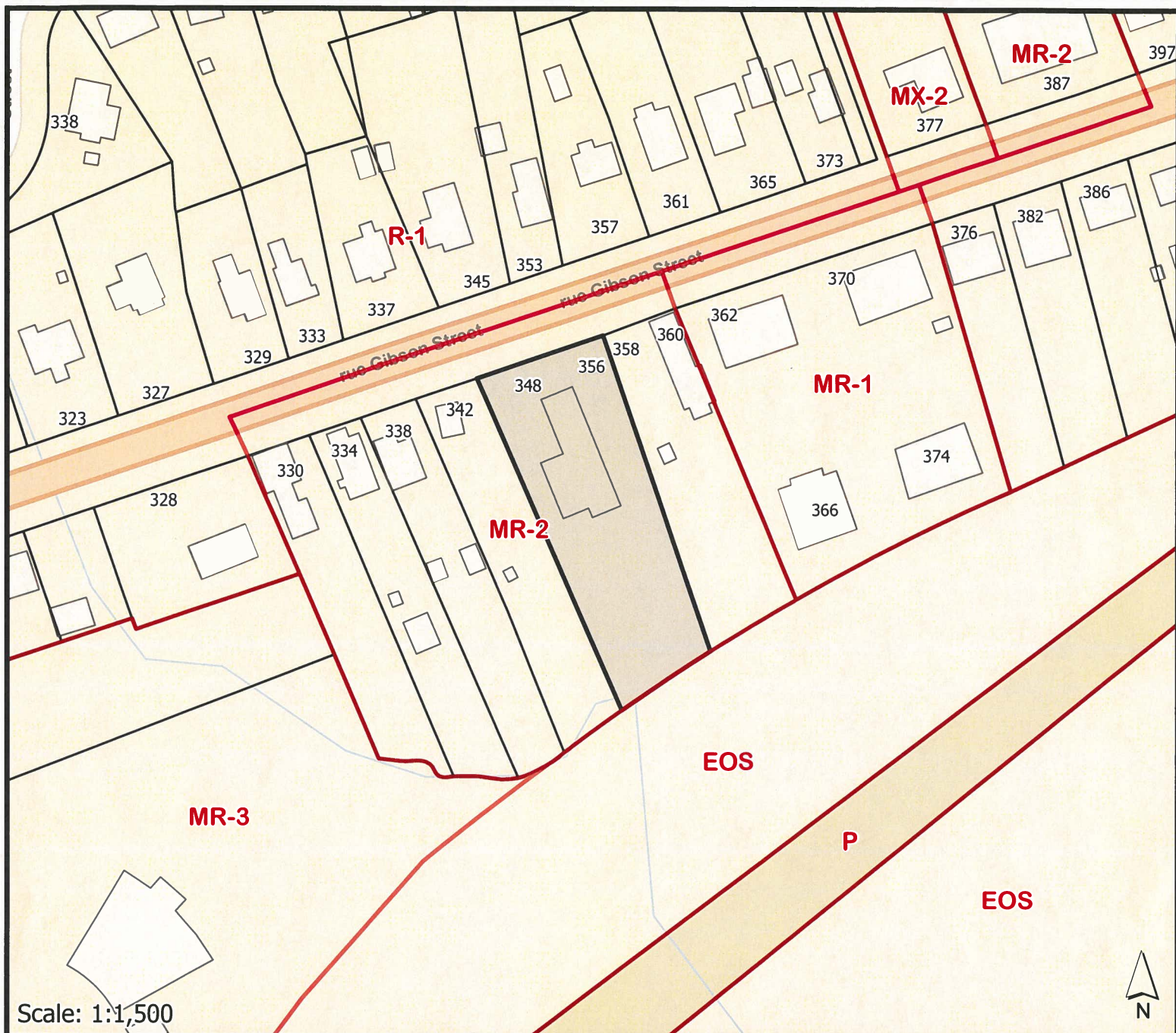


Melisa Tang Choy
Planner, Community Planning

Approved by:



Marcello Battilana, MCIP
Assistant Director, Planning & Development



 Subject Property / Propriété visée

Temporary use variance to permit Group Home and Office use in the MR-2 zone.

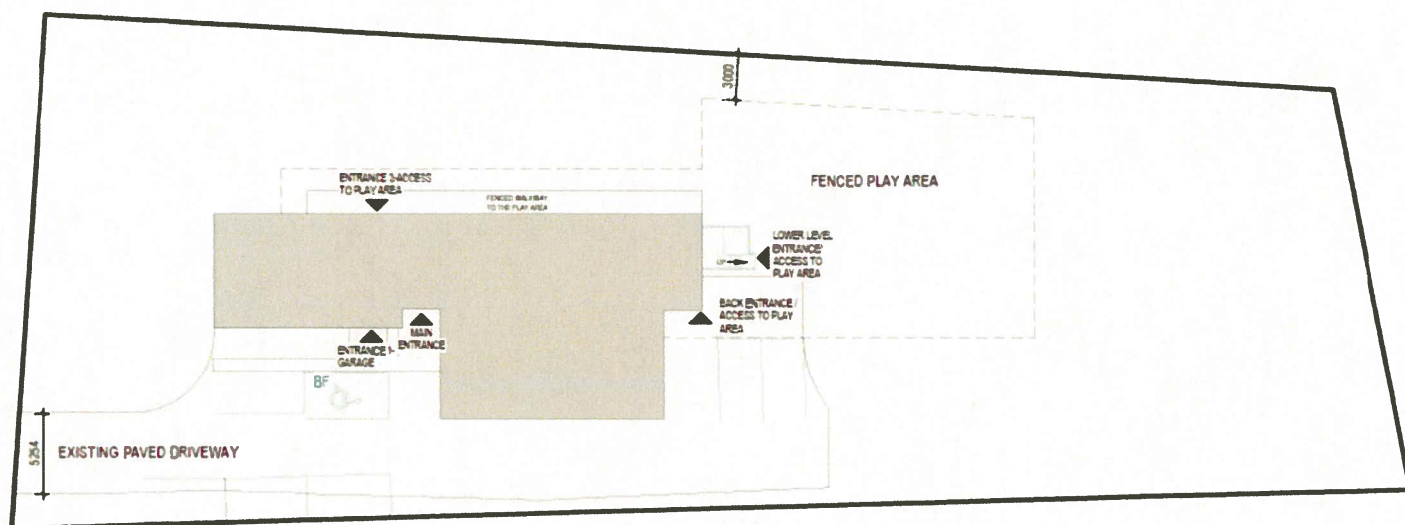
Dérogation d'usage temporaire pour permettre l'utilisation en tant que foyer de groupe et bureau dans la zone MR-2.

Fredericton

Community Planning
Planification urbaine

Map \ carte # I
File \ fiche: PR-70-2025
Date \ date: août \ August 20, 2025
Subject \ sujet: rue 348 Gibson Street
S.T.A.R.S. Children's Home
(c/o Sue Ellen English)

rue Gibson Street



Site Plan - no changes to the existing building footprint proposed. Proposed addition of a rear entrance to the in-ground level and fenced walkway and fencing for playground at the rear of the property.

Plan de site – aucun changement proposé à l’empreinte du bâtiment existant. Ajout proposé d’une entrée arrière au niveau du sous-sol et d’une clôture et allée clôturée pour l’aire de jeux à l’arrière de la propriété.

Site Plan / Plan du site

Fredericton

Community Planning
Planification urbaine

Map \ carte # II

File \ fiche: PR-70-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: rue 348 Gibson Street

S.T.A.R.S. Children's Home
(c/o Sue Ellen English)⁹²



Front - Gibson Street (West) / Façade – rue Gibson (ouest)



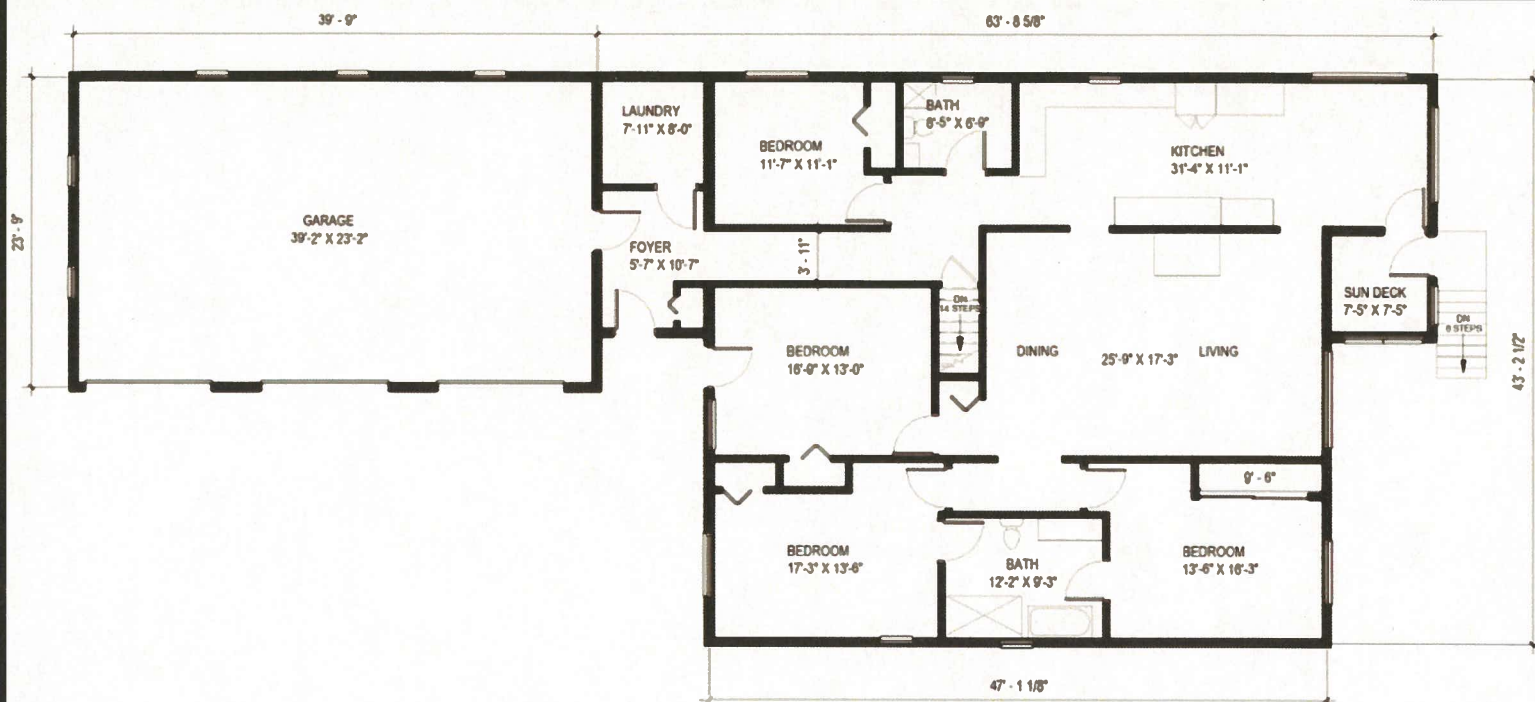
Rear (East) / Arrière (est)

Elevations / Élévations

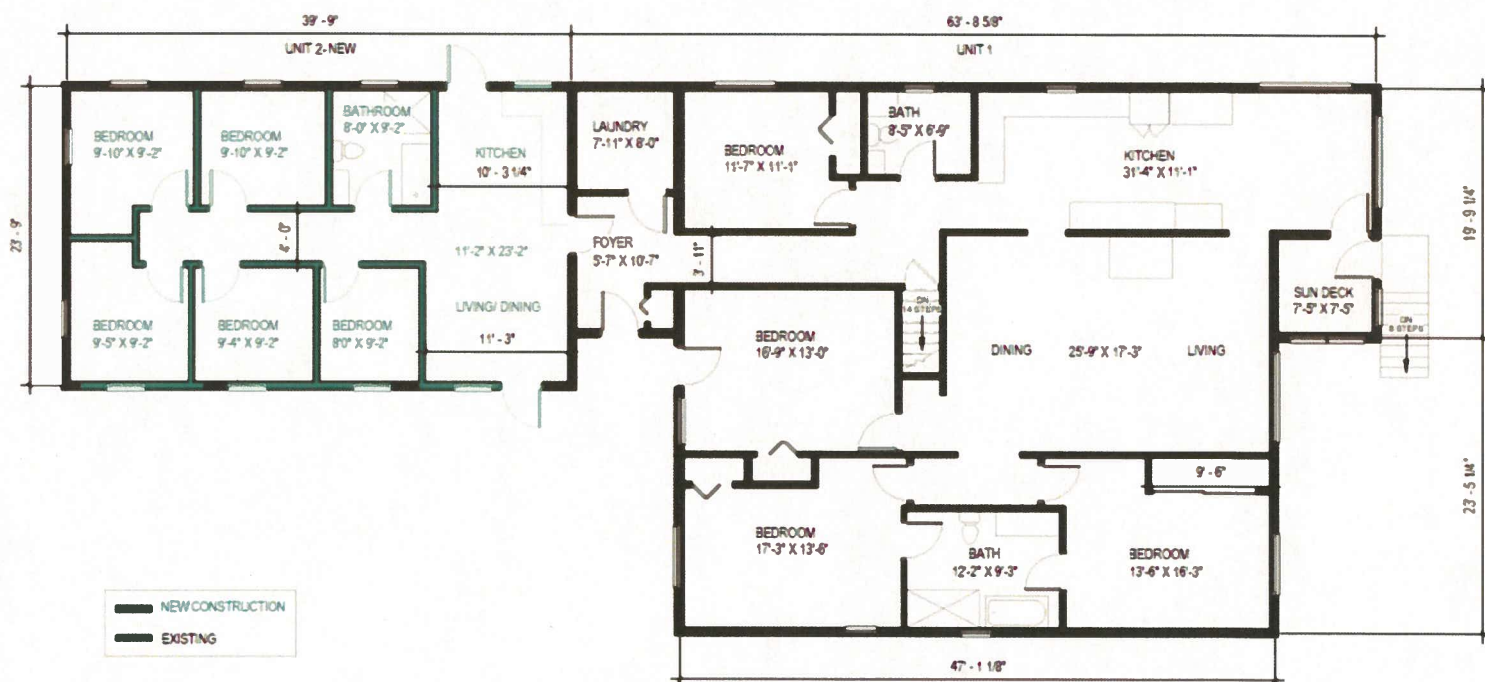
Fredericton

Community Planning
Planification urbaine

Map \ carte # III
File \ fiche: PR-70-2025
Date \ date: août \ August 20, 2025
Subject \ sujet: rue 348 Gibson Street
S.T.A.R.S. Children's Home
(c/o Sue Ellen English)³⁹³



Main Floor - Existing / Rez-de-chaussée – existant



Main Floor - Proposed / Rez-de-chaussée – proposé

Floor Plans / Plans

Fredericton

Community Planning
Planification urbaine

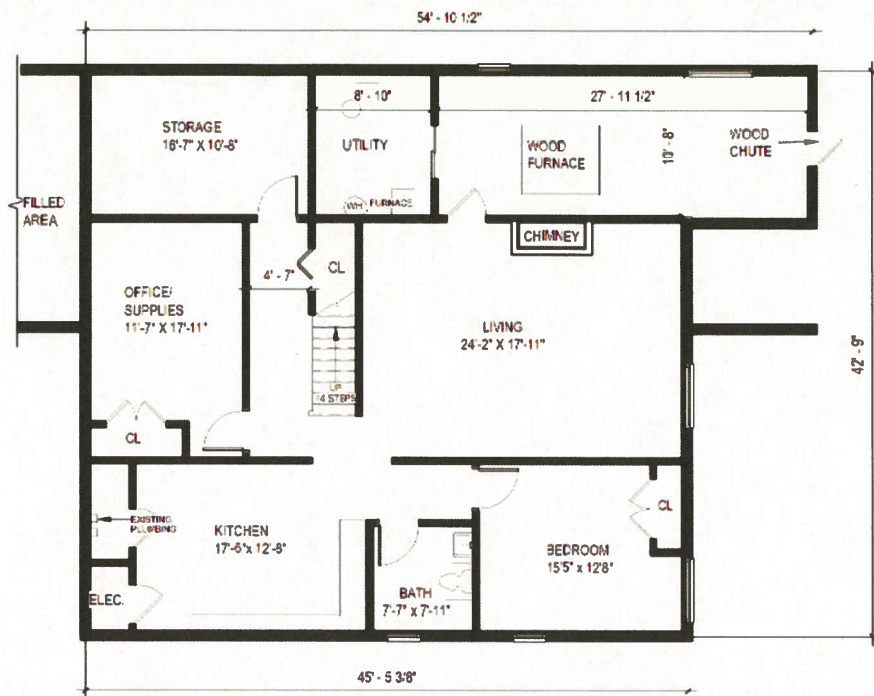
Map \ carte # IV

File \ fiche: PR-70-2025

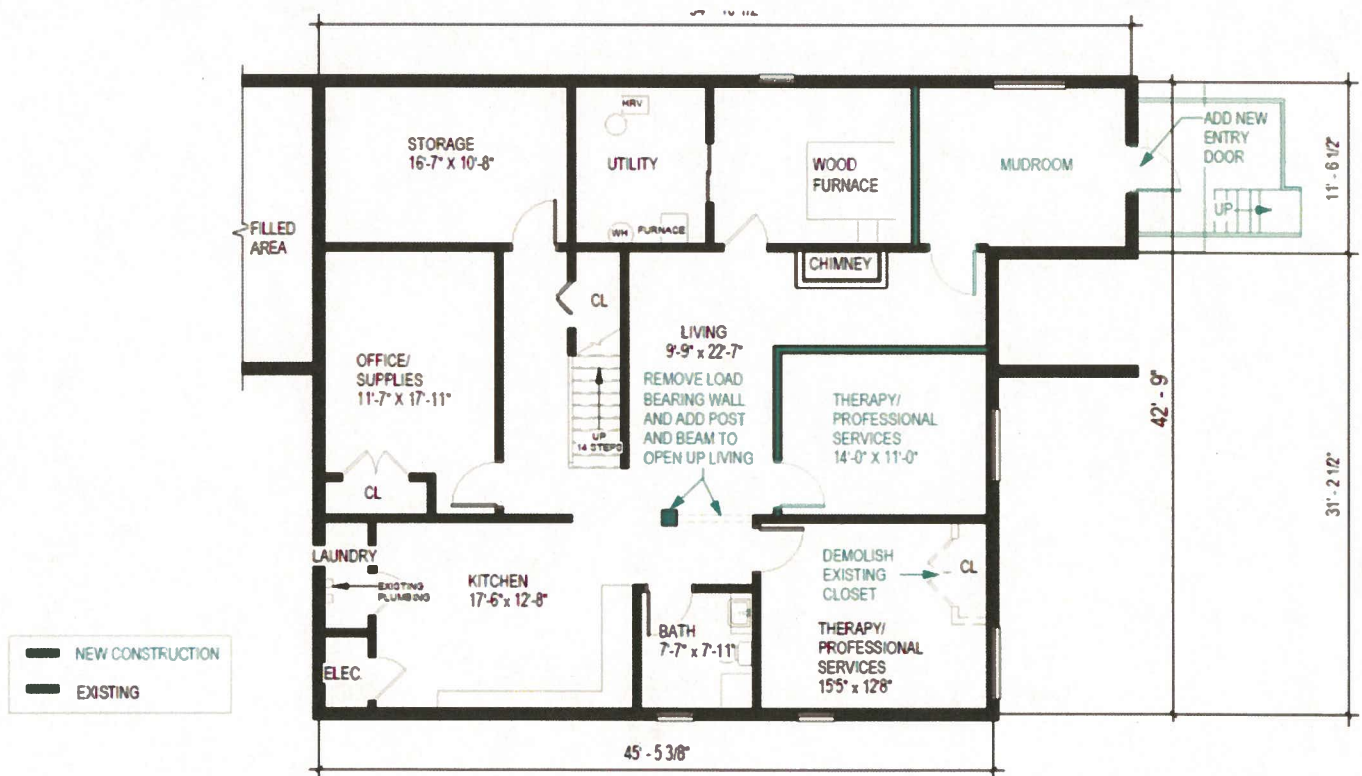
Date \ date: août \ August 20, 2025

Subject \ sujet: rue 348 Gibson Street

S.T.A.R.S. Children's Home
(c/o Sue Ellen English)⁹⁴



Basement - Existing / Sous-sol – existant



Basement - Proposed / sous-sol – proposé

Floor Plans / Plans

Fredericton

Community Planning
Planification urbaine

Map \ carte # V

File \ fiche: PR-70-2025

Date \ date: août \ August 20, 2025

Subject \sujet: rue 348 Gibson Street

S.T.A.R.S. Children's Home
(c/o Sue Ellen English)⁹⁵

TangChoy, Melisa

From: [REDACTED]
Sent: Tuesday, August 12, 2025 4:22 PM
To: TangChoy, Melisa
Cc: 'Melody Foster'; 'Jonathan Engli'; [REDACTED]
Subject: final report on community engagement
Attachments: Community BBQ.pdf; STARST SUPPORTERS FLYER (002).png; BBQ Open House Attendees.pdf; IMG_1696.jpeg; IMG_1698.jpeg; IMG_1695.jpeg; IMG_1708.jpeg; IMG_1702.jpeg; IMG_1707.jpeg

External email: Do not follow instructions, click links, open attachments, forward or respond to the email unless you recognize the sender and know the content is safe.

Good Afternoon, Melisa,

I wanted to provide you with a final update regarding 348 Gibson Street. Because of just receiving the elevations last night, we will have the new drawings to you tomorrow afternoon.

1. On Tuesday, July 22, I created a Meet n Greet BBQ Event on Facebook and made it public <https://www.facebook.com/share/12Mq2ptmGhP/?mibextid=wwXlfr>
 - a. The event was held at 348 Gibson Street on August 5 from 5 -7 pm
2. On Wednesday, July 23, I went to Devon Lumber and asked to speak with the owner. I met with Harry Gill (the president) and explained that we were moving into 348 Gibson Street and would be adjacent to his land. I explained we planned to use it for small children and gave him my contact info and a full package of information about STARS Children's Center and a written invitation to our BBQ event. I further explained we needed to get a variance permit for the use. Harry said he knew the process and would get notified, but that we would not have any problem as neighbors. He said the part of his land adjacent to 348 Gibson Street is a field with some equipment and is fenced in. I told him I would have the children's area fenced as well and that the children were young (0-8) so there would be no worry about issues such as vandalism. I told him it was important to me to come see him in person so we could move forward as good neighbors. Additionally, Harry clarified that it was his uncle who used to own 348 Gibson St.
3. On Wednesday, July 23, we dropped off poster invites (see attached) to:
 - a. Four houses to the right of 348 Gibson Street
 - b. The apartment building directly to the left of 348 Gibson Street and all 16 of the housing units in the Skigin-Elnoog Housing.
 - c. We also distributed invitations to 7 houses across the street from 348 Gibson Street.
4. Between July 23rd and the BBQ on August 5th, I continued to send out invitations to the open house via email and on social media.
5. Approximately 50 people attended the BBQ on August 5th and No Frills Grocery on Two Nations Crossing donated all the food items as well as Journey Church lent the BBQ and Pure Green Landscaping lent their chairs and the Neighbor, Ron, also lent us chairs.
6. I have attached a form with names of some of the people who attended and the comments they left:

I acknowledge that I carry out my work on the traditional unceded territory of the Wolastoqiyik, Mi'kmaq and Peskotomuhkati peoples.

ATTENTION!!

This email and any files transmitted with it are confidential and intended solely for the use of the individual or entity to whom they are addressed. If you have received this email in error, please notify the system manager. This message contains confidential information and is intended only for the individual named. If you are not the named addressee, you should not disseminate, distribute or copy this email. Please notify the sender immediately by email if you have received this email by mistake and delete this email from your system. If you are not the intended recipient, you are notified that disclosing, copying, distributing or taking any action in reliance on the contents of this information is strictly prohibited.



Community BBQ

August 5, 2025

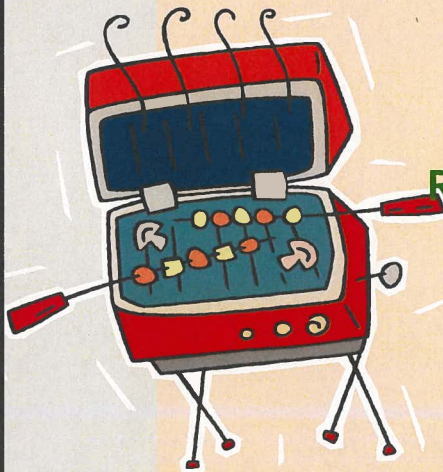
5 pm-7 pm

Join us for a meet and greet at the
future home for S.T.A.R.S.
CHILDREN'S CENTER.

We look forward to sharing our
vision and to answering your
questions.

348 Gibson Street
Fredericton, NB

RSVP to: senglish@starschildrenshome.ca



Meet N Greet BBQ
348 Gibson St
August 6th, 2025

399



S.T.A.R.S

CHILDREN'S HOME

SHARING TENDERNESS AND RAISING SUCCESS

OUR SUPPORTERS

Dr. Relja Malgorzata — Psychiatrist

Dr. Rachel Oullette — Pediatrician

Dr. Francis Bennett — Educational Leadership & Administration

— Speech and Language Pathology, Applied Behavior

Neline De Villiers — Researcher & Team Lead

Becky Jones — Speech Therapist

Matthew Maxwell — Lead Pastor, Journey Wesleyan Church

Eric Megarity — City Council Margo Sheppard — City Council

Annette Stehouwer — Dept of Education MLA Kris Austin

MLA David Coon MP Richard Bragdon Graydon

Nicholas — CM, ONB National Advisory Council on Poverty,

Canada MAWIW

Jolly Farmer Products Shift Auto Group Campbells Auto

CIBC - Daniel LaCosta Carr Brothers

ReMax — Jason Munn, Damir Kazik Royal LePage,

Lauren Reynaert Donna Gardagidner Thompson

Greener Village — Alex Boyd 92.3 New Country

96.5 Joy FM Jumping Jacks

One Up on a Child

April 23, 2025

Dr. Francis Bennett

20 Coach House Lane
New Maryland, New Brunswick
E3C1J9

Re: Endorsement for S.T.A.R.S. Children's Home - Outreach Program for Preventing Family Violence and Addressing Child Maltreatment

To Whom It May Concern,

My name is Francis Bennett, and I am a New Brunswick resident, and I have worked for the past 25-years as an educator and educational leader within the province of New Brunswick. I am writing this letter as a mechanism to strongly support the *S.T.A.R.S. Children's Home's* proposed *Outreach Program for Preventing Family Violence and Addressing Child Maltreatment*. This program utilizes the *Bringing Up Great Kids (BUGK) Methodology*, focused on reflective and mindful parenting grounded in relationships, a vital community intervention that I wholeheartedly endorse.

My extensive career in education has shown me the profound impact of caregiver-child relationships on a child's development and well-being. These relationships are fundamental to a child's capacity to thrive. I have witnessed the positive impact of secure relationships and the challenges faced by children lacking them. The *S.T.A.R.S. Children's Home Proposal* rightly acknowledges a degradation in the ability of many people to provide relational support to those who most need it. Further, in today's society, this degradation is exacerbated by factors such as the overuse of technology and the isolation perpetuated by the COVID-19 pandemic, and the associated erosion of traditional support systems, which leaves many parents unprepared to support the youth in their care.

Parenthood is a learned skill, not an innate one. The absence of sufficient role modeling contributes to increased family violence and child maltreatment, negatively impacting children's emotional, social-emotional, and cognitive development. My experience as a former school principal demonstrated the direct correlation between a child's home environment and their educational and social-emotional outcomes. Children who feel safe and secure at home are more likely to succeed academically and social/emotionally. I also saw how instability at home hinders both. Fostering strong teacher-student connections can compensate for home deficiencies and create a supportive school culture but it is only one variable in supporting the positive development of our youth.

My work developing and implementing the New Brunswick Provincial Improvement Framework for Schools further solidified my understanding of the manifold factors influencing the well being of children and greatly advanced my skills in program development and evaluation, which I offer to this project. The *S.T.A.R.S. Children's Home's* commitment to caregiver training aligns with my belief in the power of education and intervention. Their comprehensive, evidence-based approach, including evaluation of caregiver reflective capacity, is particularly important, as reflective capacity is key to sensitive and responsive care.

Therefore, I offer my support through:

- a peer review of the project;
- a review of data and reporting; and,
- assistance with the final evaluation.

I believe this project can create lasting positive change for vulnerable children and families in New Brunswick, building a stronger, healthier future.

Respectfully yours,

Dr. Francis P. Bennett

To Whom It May Concern,

My name is Francis Bennett, and I am a New Brunswick resident and a 25 year veteran educator/educational leader within the province. It is my pleasure and with a deep sense of commitment that I offer this letter of support and collaboration for the *S.T.A.R.S. Children's Home* and their proposed *Outreach Program for Preventing Family Violence and Addressing Child Maltreatment*. This program utilizes the *BUGK* (Bringing Up Great Kids) *Methodology*, which teaches reflective and mindful parenting grounded in relationships, and represents a vital intervention necessary for the positive growth of our community. nd I wholeheartedly endorse its application for funding.

My career in education, provided me with a unique and comprehensive perspective on the profound impact of caregiver-child relationships on a child's development and well-being. In my experience, the quality of these foundational relationships is not merely a social issue; it is a fundamental determinant of a child's capacity to thrive, learn, and contribute positively to society. Over the years, I have witnessed firsthand the remarkable difference that secure, nurturing relationships make in a child's life, and conversely, the significant challenges faced by children who lack such support.

The core premise of the *S.T.A.R.S. Children's Home* proposal resonates deeply with my observations and experiences garnered throughout my career. The program rightly acknowledges that in today's society, the relational density – the richness and depth of our interpersonal connections – is significantly diminished compared to previous generations.

Recent societal changes (technology) and challenges (The COVID-19 pandemic) has exacerbated these growing gaps. It has been noted that our relational efficacy and associated relational support systems have suffered due to these and other variables. This, coupled with the erosion of traditional support systems, where parents could rely on extended family and community for guidance and modeling, leaves many of today's parents without adequate preparation for the rewarding, but increasingly complex and demanding, role of raising children.

It is a flawed assumption to believe that parenthood is an innate skill. Like any other complex behaviors, it is learned and refined through observation, guidance, and ongoing support. The absence of sufficient mirroring and role modeling, which was historically provided by grandparents, extended family, and close-knit communities, directly contributes to the increased prevalence of issues like family violence and child maltreatment. Children who do not experience consistently positive nurturing parenting are more vulnerable, and this vulnerability often extends into every facet of their lives, impacting their emotional, social/emotional, and cognitive development.

My experience as a former school principal provided me with invaluable insights into the direct correlation between a child's home environment and their educational as well as social-emotional outcomes. I consistently observed that children who feel safe, loved, and secure at home are far more likely to engage positively in the learning process, demonstrate resilience, and achieve their full potential. I also witnessed the heartbreaking reality of how difficult it is for children to focus on learning when they are living in environments characterized by instability, neglect, or abuse. Through this role, my team and I actively sought and implemented strategies to foster strong teacher-student connections, recognizing that these relationships could, to some extent, compensate for deficiencies in the home environment and create a more supportive and nurturing school culture. These efforts invariably yielded positive results, underscoring the critical importance of relational health in a child's life.

My work developing and implementing the New Brunswick Provincial Improvement Framework for Schools, further solidified my understanding of the systemic factors that influence child well-being. The Framework was designed to equip school personnel with the necessary knowledge, processes, and data analysis skills to drive effective school improvement planning. This experience developed my skills in program development, implementation, and evaluation, skills that I believe are directly applicable to supporting the S.T.A.R.S. Children's Home project.

The S.T.A.R.S. Children's Home's commitment to providing training for caregivers aligns perfectly with my belief in the power of education and intervention. The program's design,

which includes extensive in-community training, follow-ups, and rigorous evaluation of both caregiver reflective capacity and child well-being, demonstrates a comprehensive and evidence-based approach. I am particularly impressed with the emphasis on measuring reflective capacity, as this is a key indicator of a caregiver's ability to provide sensitive and responsive care.

Therefore, drawing on my extensive experiences in education and my expertise in program development and evaluation, I am proud to offer my support to the S.T.A.R.S. Children's Home in the following ways:

- I will provide a peer review of the intended project, ensuring its alignment with best practices and research-based principles.
- I will contribute to the review of data and reporting throughout the project's duration, offering insights and recommendations to optimize its effectiveness.
- I will assist in the evaluation process at the conclusion of the project, helping to assess its impact and identify key learnings for future initiatives.

I firmly believe that this project has the potential to create lasting, positive change in the lives of vulnerable children and families in New Brunswick. By working together, we can make a significant difference and build a stronger, healthier future for the children of Canada.

Respectfully yours,

Dr. Francis P. Bennett

May 13/2025

To Whom It May Concern,

I am happy to offer a letter of support and collaboration to the S.T.A.R.S. Children's Home and the Outreach Program for Preventing Family Violence and Addressing child maltreatment using the BUGK (Bringing up Great Kids) Methodology, which teaches reflective and mindful parenting grounded in relationships.

As a business owner, nurse, and mom, I have engaged with STARS Children's Home to gain more of an understanding of the dire need of the children in Fredericton and am delighted to help the STARS team connect to resources such as car seats, clothing, sensory toys, and connections in the city to move the STARS Children's Home ahead. I am keen to see our city continue to develop resources for the vulnerable and love to help various organizations especially when children are involved.

It is my sincere privilege to provide a letter of support to STARS Children's Home as they launch these needed services for children in our community.

Sincerely,

Alessa Martin



Alessa Martin
OWNER

📍 1221 Prospect St. Fredericton, NB E3B 3B9
☎ (506) 206-5867
🌐 www.jumpingjackscb.com

2023 & 2024 Community Votes
Platinum winner for:

- Children's clothing
- Maternity clothes
- Toys
- Second hand clothing
- Baby products

2024 Desjardins
Goodspark Grant recipient

📷 @jumpingjackscb 📘



To Whom It May Concern,

I am pleased to offer a letter of support and collaboration to the S.T.A.R.S. Children's Home and the Outreach Program for Preventing Family Violence and Addressing child maltreatment using the BUGK (Bringing up Great Kids) Methodology, which teaches reflective and mindful parenting grounded in relationships.

I have served as an MLA since 2018 and served as the Minister of Public Safety from 2022-2024. I have advocated since 2020 for reform in legislation to address child poverty. In 2024 as Minister of Public Safety, I supported the acceleration of child protection cases in the courts. As a continuation of my commitment to the children of New Brunswick, I am proud to offer this letter of support to the STARS Children's Home.

A stylized handwritten signature in black ink, likely belonging to MLA Kris Austin.

MLA Kris Austin
Fredericton - Grand Lake
Constituency Office / bureau de circonscription:
112 Main Street / 112 rue Main
Minto NB Unit 4 / 4 Local 4 E4B 3M2
Tel / Tel: (506) 440-9542



Richard Bragdon
Member of Parliament
Tobique-Mactaquac

May 6, 2025

Subject: Letter of Support

To Whom It May Concern,

I am pleased to offer a letter of support for S.T.A.R.S. Children's Home (S.T.A.R.S.) in its request for funding an outreach program aimed at preventing family violence and addressing child maltreatment.

Since my election as a Member of Parliament in 2019, I have been a strong advocate for addiction recovery. One of my key achievements in this regard has been the introduction and passage of Private Member's Bill C-228, *An Act to Establish a Framework to Reduce Recidivism*. As part of my ongoing commitment to supporting marginalized individuals and pursuing effective solutions, I am pleased to provide this letter in support of S.T.A.R.S.'s funding request which will allow the organization to expand its valuable programming for parents and caregivers of at-risk children. As you are well aware, there is a clear connection between unresolved childhood trauma, unmet relational and emotional needs, and addiction.

The work of organizations like S.T.A.R.S. is immensely important in addressing these complex social issues. Government alone cannot solve these multifaceted challenges without the dedicated efforts of organizations like S.T.A.R.S. that are deeply attuned to the needs of those they serve. This is precisely why additional funding for this outreach program is essential to ensuring the continued success and expansion of these important investments in at-risk families.

In the spirit of continuing to support marginalized individuals and provide them with the advocacy and resources they need to build healthier life paths, I am pleased to offer my wholehearted support for S.T.A.R.S. Children's Home's request for funding to allow them to expand outreach programming focussed on preventing family violence and addressing child maltreatment.

Warmest regards,

Richard Bragdon, MP
Tobique-Mactaquac

1-800-671-6160 / Richard.bragdon@parl.gc.ca
P.O. Box 1144, Nackawic, NB, E6G 1G9



May 7, 2025

To Whom It May Concern,

I am pleased to offer a letter of support and collaboration to the S.T.A.R.S. Children's Home and the Outreach Program for Preventing Family Violence and Addressing child maltreatment using the BUGK (Bringing up Great Kids) Methodology, which teaches reflective and mindful parenting grounded in relationships.

As the owner of RE/MAX East Coast Elite Realty in Fredericton, and longtime community member, I am committed to the well-being of the children in New Brunswick. I am happy to be able to assist STARS Children's Home and programs by connecting them with community members and partners who can help move this needed resource for children ahead.

It is my sincere privilege to provide a letter of support to STARS Children's Home as they launch these much needed services for children in our community.

Sincerely,

Jason Munn
Owner/Broker
RE/MAX East Coast Elite Realty

RE/MAX | *East Coast* **ELITE**

283 St. Mary's Street, Fredericton, NB E3A 2S5 Phone: (506) 452-9888

May 16, 2025

To Whom It May Concern,

I am honoured to offer a letter of support and collaboration to the S.T.A.R.S. Children's Home and the Outreach Program for Preventing Family Violence and Addressing child maltreatment using the BUGK (Bringing up Great Kids) Methodology, which teaches reflective and mindful parenting grounded in relationships.

As a career teacher, I can assure you that this program is needed in our community, and I have already met with the ED of STARS to learn about the details of the STARS program. Too often I see students who are affected by family violence, neglect and verbal abuse, which makes a classroom challenging for these children. Their lives are filled with anxiety and uncertainty, making learning difficult, but I feel strongly that education is the way out of this harmful cycle.

By giving fathers and mothers the tools needed to raise healthy children whose parents have a healthy, nurturing relationship with them will not only benefit them as a unit but society beyond their home, including the school system.

It is my sincere privilege to provide a letter of support to STARS Children's Home as they launch these much-needed services for children in our community. I hope that you will seriously consider this program.

Thank you.

A handwritten signature in black ink, appearing to read 'Annette Stehouwer', with a long horizontal flourish extending to the right.

Annette Stehouwer

From: [Christin Swim](#)
To: [PLANNING AND DEVELOPMENT](#)
Subject: File: PR: 70/25 S.T.A.R.S Children"s Home
Date: Tuesday, August 19, 2025 10:56:08 AM
Attachments: [20250819103614249.pdf](#)
Importance: High

External email: Do not follow instructions, click links, open attachments, forward or respond to the email unless you recognize the sender and know the content is safe.

Please see the attached letter of support for the Temporary use variance at 348 Gibson Street.

Christin Swim
General Manager
Skigin-Elnoog Housing Corp.
Phone (506) 459-7161
Fax (506) 459-1289
Toll Free (800) 561-4024

NIHKANAPOLTIPON - We are looking forward to the future



Skigin-Elnoog Housing Corporation

366 Gibson Street • Fredericton, New Brunswick • E3A 4E6

Telephone (506) 459-7161 • Toll Free 1-800-561-4024

Facsimile (506) 459-1289

Community Planning
397 Queen St. (City hall)
Fredericton NB E3B 1B5

RE: S.T.A.R.S. Children's Home – Temporary Use Variance

August 19, 2025

Dear Planning Advisory Committee

I am writing to express my strong support for the proposed temporary use variance for 348 Gibson Street, which was put forward by S.T.A.R.S Children's Home. Strong communities and supportive neighborhoods are built on foundations that allow for everyone to feel supported and have access to the tools and services necessary for this to be accomplished. The business model and proposal put forward by S.T.A.R.S children home helps close a gap in services for Youth that will allow for healing and treatment services within a community-based environment.

As General Manager of Skigin-Elnoog Housing Corporation, I have seen firsthand the importance of early intervention for children that have experienced trauma. "Early intervention can help children build resilience and buffer the negative impact of adverse experiences"-S.T.A.R.S.

I believe the goal of this care home aligns very strongly with Skigin-Elnoog values, being a housing provider for over 50 years our organization understands the difficulties of being part of the "solution" to the parts of social and community issues being presented to vulnerable and underrepresented portions of our community. This Children's home and care center will fit beautifully into our community and be an excellent resource to any family who is struggling to succeed.

I urge the Planning Advisory Committee to approve this temporary use variance, as they will have a lasting positive impact on Fredericton's future. Thank you for your time and consideration, and I look forward to seeing our city continue to thrive.

Sincerely,

Christin Swim
General Manager
Skigin-Elnoog Housing Corp.

PLANNING REPORT



PAC – August 20, 2025
File No.: V-16-2025 P.R. No.68/25

To: Planning Advisory Committee

From: Melisa Tang Choy, Planner

Proposal: Similar or compatible use variance application to permit a Day Care for up to 75 children in the Residential Zone under the *Noonan Local Service District and Lower St Mary's Planning Area Rural Plan Regulation – Community Planning Act*

Property: **148 Mataya Drive (PID 75395194)**

OWNER: Creative Childcare Inc. (c/o Javed Khan)
110 Nottingham Street
Fredericton, NB E3B 4W9

APPLICANT: Same as above

SITE INFORMATION:

Location: Corner lot at the intersection of Mataya Drive and Marian Way

Context: Low-rise residential neighbourhood composed primarily of single-detached dwellings

Ward No: 6

Rural Plan: Noonan Local Service District and Lower St Mary's Planning Area Rural Plan Regulation – Community Planning Act

Zoning: Residential

Existing Land Use: Commercial day care

Previous Applications: SCF-7-14 (Capital Regional Service Commission file number)

EXECUTIVE SUMMARY:

The Applicant is proposing to add 15 childcare spaces to the existing day care facility on the subject property, for a total of 75 children. The property is zoned Residential (R) in the Noonan Local Service District and Lower St Mary's Planning Area Rural Plan Regulation – Community Planning Act. The subject property had been used as a daycare for several years, with the property owners living on the top floor of the building. The property owners ceased to live on the property, and in 2014, a Similar Use variance application was approved for a commercial daycare, with the intent of limiting the number of children to 60 on the property. No exterior expansion of the building is being proposed.

Staff are of the opinion that the proposal is in keeping with the intent of the policies and regulations of the Noonan Local Service District and Lower St Mary's Planning Area Rural Plan Regulation – Community Planning Act and provides a needed amenity to the area. Staff do not anticipate any adverse impacts to neighbouring property owners and support the proposal subject to terms and conditions.

APPLICATION:

Javed Khan, on behalf of Creative Childcare Inc., has made application to permit commercial day for up to 75 children in the Residential zone on property located at 148 Mataya Drive.

PLANNING COMMENTS:

Background:

- The subject property is part of the areas that were amalgamated to the City of Fredericton in 2023 as part of the Local Government Reform. The City has continued to implement existing planning approvals and land-use regulations, the latter of which contain both the zoning standards and the land-use policies. The subject property is under the Noonan Local Service District and Lower St Mary's Planning Area Rural Plan Regulation – Community Planning Act, hereby referred to as the Noonan Plan.
- In 2014, the Planning Review and Adjustment Committee (PRAC) of the Regional Service Commission 11 approved a Similar or Compatible Use variance application to permit a commercial daycare on the subject property. The subject property had previously been used as a daycare, with previous property owners living on the upper floor of the house. The Residential (R) zone permits "community care home" use as a secondary use, which includes a day care, but one of the requirements is that the owner lives in the dwelling.
- At the time, the then-Applicant noted that the subject property was no longer being used for residential purposes, and that they were looking to increase in the number of children to 60. City Staff confirmed with the Capital Regional Service Commission that the intent of the term and condition of the approval regarding the maximum number of children was to limit the number to 60, as per the proposal submitted. Thus, any increase in the number of children would necessitate a planning application.

Proposal:

- The Applicant is looking to add 15 childcare spaces to the existing day care facility at 148 Mataya Drive, bringing the total number of children to 75 on the subject property. The current daycare is licensed for 60 children (9 infants, 41 preschool spaces and 10 after school spaces), with 9 full-time staff providing support. The additional 15 spaces would be for 3 infant spaces and 12 preschool spaces, which would add 2 full-time staff to the day care.
- The day care operates from Monday to Friday, 7:30 am – 5:30 pm and no changes are being proposed to the operating hours. The entirety of the building would continue to be used for the daycare, and no changes are being proposed for the exterior. As shown on Map II, there already is an existing fenced playground at the rear of the property, and the existing parking lot can accommodate up to 19 vehicles. The Applicant has indicated that its current on-site sewage has capacity to accommodate up to 75 children and that they

have been in contact with the Department of Justice and Public Safety on this. The Applicant has been in conversation with the Department of Education and Early Childhood Development, and the total number of childcare spaces would be divided between two day care licenses. No changes to the exterior of the building are being proposed.

- The Applicant is requesting the additional childcare spaces, as they currently have over 200 families on their infant space waitlist and 150 families on their preschool waitlist.

Noonan Local Service District and Lower St Mary's Planning Area Rural Plan Regulation – Community Planning Act:

- The Noonan Plan outlines as part of its policies related to Commercial uses, the plan notes that “it is a policy to encourage land uses that create employment opportunities and provide access to goods and services”.
 - The subdivision is almost exclusively residential, with no other uses except for a playground to the south of the property on Shallon Lane. Commercial and light industrial uses are located along Greenwood Drive, with one commercial daycare located at the corner of Fernwood Drive and Greenwood Drive. The daycare has been a well-utilised resource in the community and the additional childcare spaces would result in the addition of two new staff.
- The subject property is zoned Residential and is surrounded by single-detached dwellings. Related to Residential uses, the Noonan Plan notes the following:

It is a policy to enhance and maintain attractive and safe neighbourhoods and discourage the intrusion of incompatible uses into established residential areas.

It is a policy to allow residential development that reflects the existing character of the area.

- The subject property has been used as a commercial day care for several years, and Staff are not aware of any complaints that have arisen from its operations. The subject property is a double lot, and the building is located closer to the corner of Mataya Drive and Marian Way, with a treed buffer on the neighbouring properties that provide screening. The daycare has continued to maintain the residential character of the building and no exterior changes to the building are being proposed.
- The Rural Plan does not outline standards for the design of parking lots, as long as all parking spaces can be accommodated within the property. The circular driveway at the front of the property is used to drop-off children. The parking spaces on Marian Way are screened by a hedge, whereas the parking spaces on Mataya Drive would not be. The previous planning approval had a term and condition directed at retaining the residential character of the property. As such, Staff would request that the parking fronting on Mataya Drive be visually screened from the public street.
- Staff would note that, had this application be taken under the Zoning By-law, it would have been a rezoning or zone amendment application, as the property is zoned Residential. In

those instances, the maximum number of children would have been noted in a term and condition, and such condition would have been amended to permit the requested increase. However, the original planning application was a Similar or Compatible Use variance, and Staff are continuing the original process that permitted the commercial daycare in the first place. In the case of variances, the terms and conditions cannot be amended and necessitate a new application. Staff will evaluate the zoning of the property during the process of incorporating the rural plans into the Zoning By-law in the next year.

Engineering & Operations:

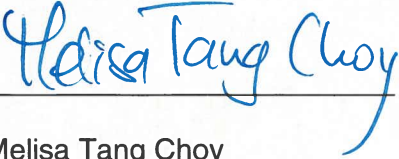
- Engineering & Operations have reviewed the proposal and have no issues. The subject property is located at the intersection of Mataya Drive and Marian Lane with access to Route 10 via Rolling Hill Drive, Rockwood Way, and Fernwood Drive. The street network in the area including Route 10 are owned and operated by the provincial government.
- The property lies outside of the municipally serviced water and sanitary sewer area. Water and sewer servicing is to meet provincial regulatory standards and approvals.
- Overall, Staff are of the opinion that the proposed variance is appropriate in this case and meets in the intent of the Noonan Plan. The proposal continues to be compatible with the surrounding residential uses, and provides a needed resource in the community. Staff do not anticipate any negative impacts to neighbouring properties, and support the application subject to terms and conditions.

RECOMMENDATION:

It is recommended that the application submitted by Javed Khan, on behalf of Creative Childcare Inc., for a variance application to permit a daycare for up to 75 children on property located at 148 Mataya Drive be approved subject to the following terms and conditions:

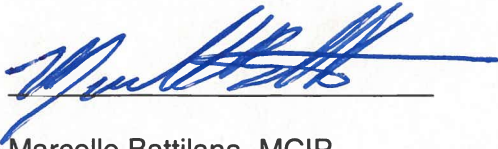
- a) The site be developed generally in accordance with Map II attached to P.R. 00/13, including the provision of visual screening of the parking lot on Mataya Drive from the public street, to the satisfaction of the Development Officer;
- b) Any exterior signage to be in accordance with the signage regulations in the Noonan Local Service District and Lower St Mary's Planning Area Rural Plan Regulation – Community Planning Act;
- c) The building retains the character of a residential property;
- d) That the exterior lighting be located, arranged or shielded as not to interfere with local traffic or with nearby landowners;
- e) The childcare centre be limited to a maximum of 75 children; and,
- f) The Applicant obtain all necessary licensing for operation of the child care centre from the New Brunswick Department of Education and Early Childhood Development.

Prepared by:

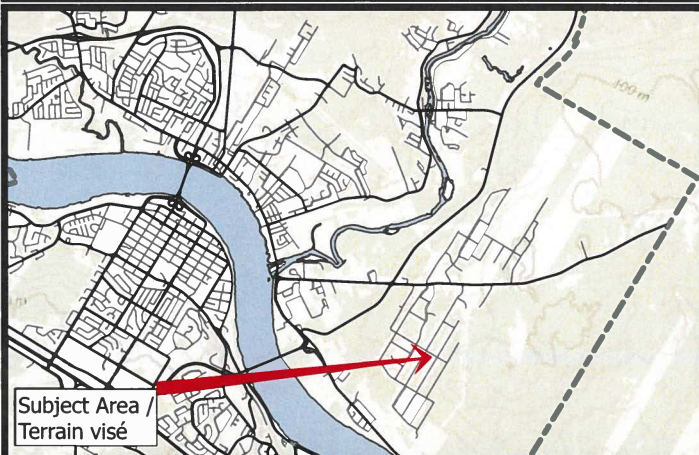
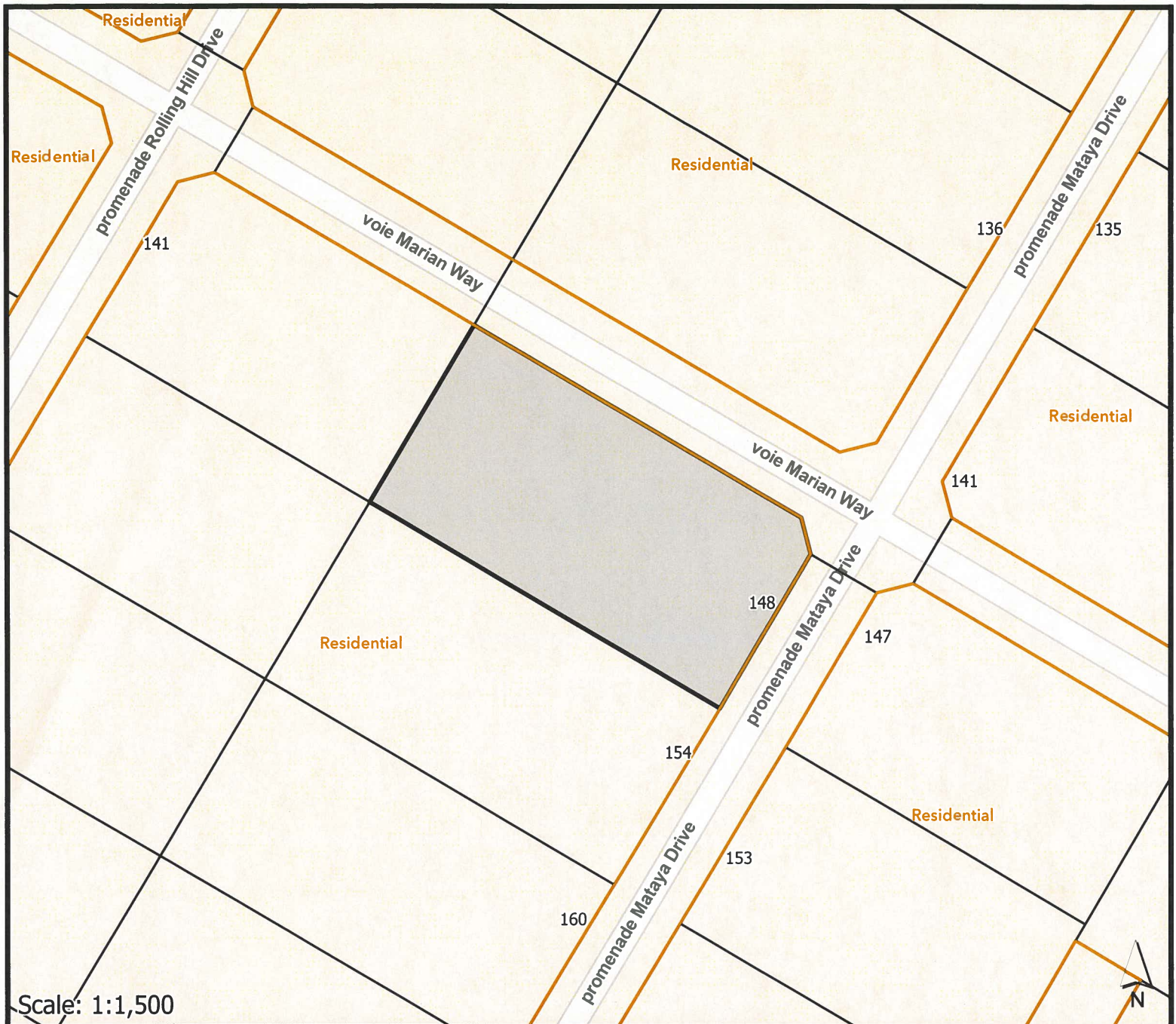
A handwritten signature in blue ink that reads "Melisa Tang Choy". The signature is written in a cursive style and is positioned above a horizontal line.

Melisa Tang Choy
Planner, Community Planning

Approved by:

A handwritten signature in blue ink that appears to read "Marcello Battilana". The signature is written in a cursive style and is positioned above a horizontal line.

Marcello Battilana, MCIP
Assistant Director, Planning & Development



Subject Property / Propriété visée
 Similar Use Variance application to add 15 more childcare spaces to the existing commercial daycare in the Residential Zone under the Noonan Lower St Mary's Rural Plan, for a total of 75 childcare spaces.

Demande de dérogation pour usage similaire visant à ajouter 15 places supplémentaires en garderie commerciale existante dans une zone résidentielle, conformément au Plan rural de Noonan Lower St. Mary's, pour un total de 75 places en garderie.

Fredericton

Community Planning
Planification urbaine

Map \ carte # I

File \ fiche: PR-68-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: prom 148 Mataya Drive

Creative Childcare in Pepper Creek Inc.
(c/o Javed Khan)



Site Plan / Plan du site

Fredericton

Community Planning
Planification urbaine

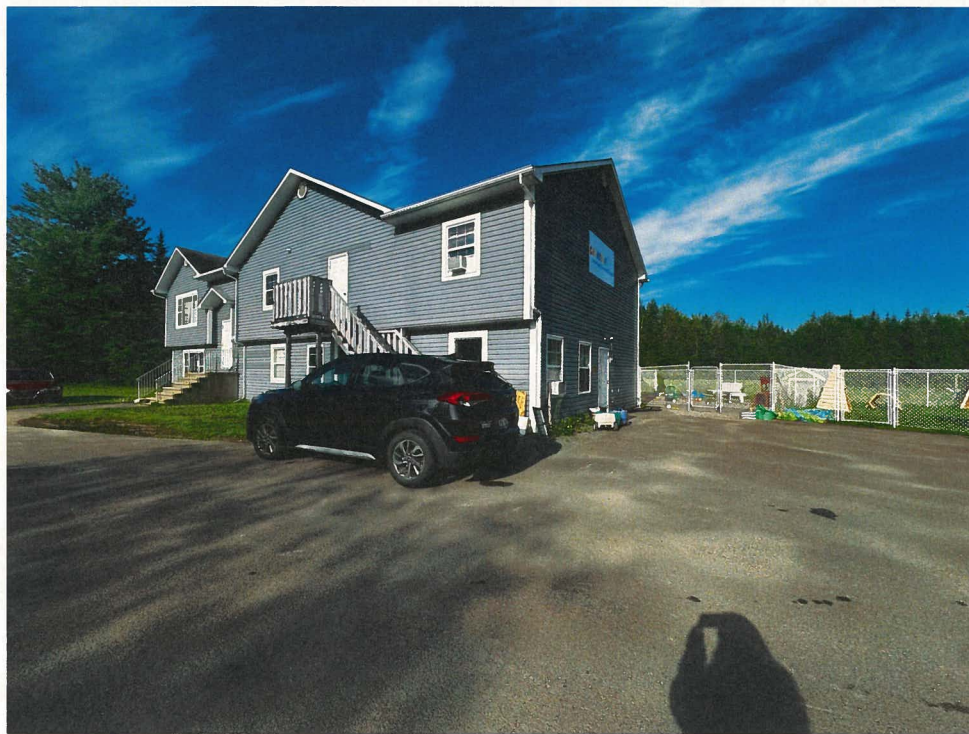
Map \ carte # II

File \ fiche: PR-68-2025

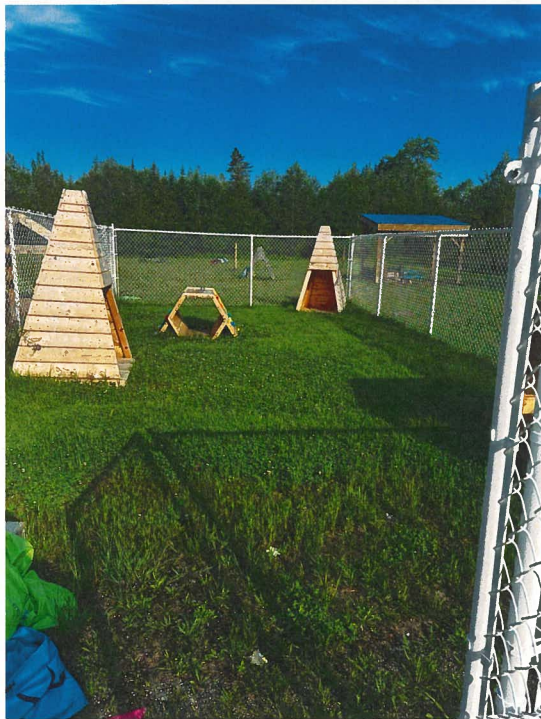
Date \ date: août \ August 20, 2025

Subject \ sujet: prom 148 Mataya Drive

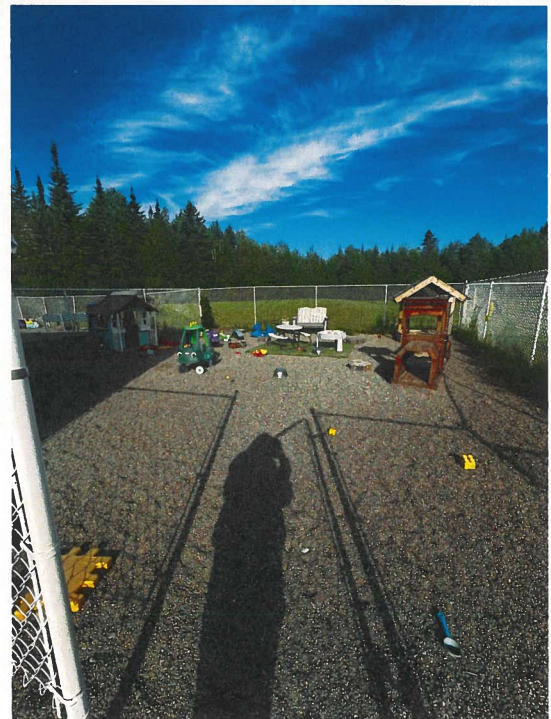
Creative Childcare in Pepper Creek Inc.
(c/o Javed Khan)



Elevation / Élévation



Infant Playground / Aire de jeux
pour nourrissons



Preschool Playground / Aire de jeux
pour pouspons et enfants
d'âge préscolaire

Elevations and Photos / Élévations et photos

Fredericton

Community Planning
Planification urbaine

Map \ carte # III

File \ fiche: PR-68-2025

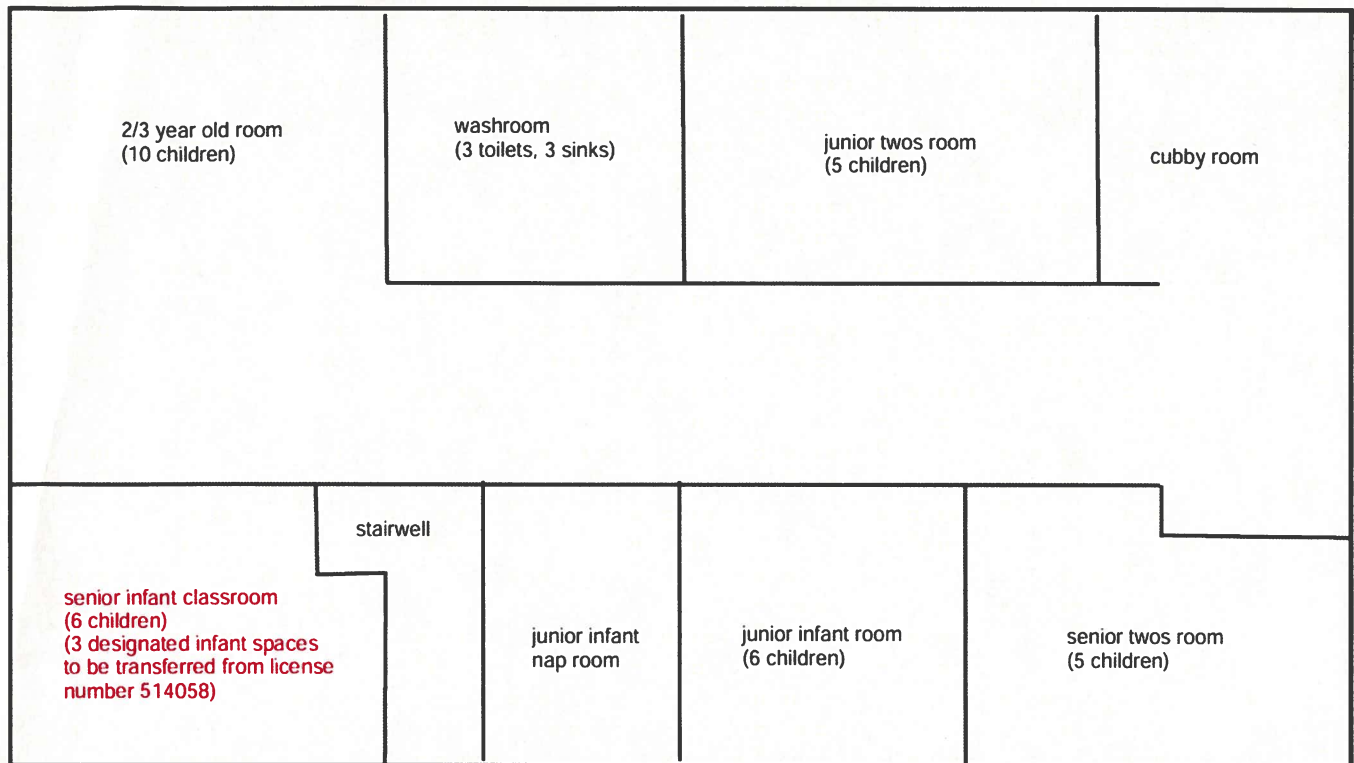
Date \ date: août \ August 20, 2025

Subject \ sujet: prom 148 Mataya Drive

Creative Childcare in Pepper Creek Inc.
(c/o Javed Khan)

Main Floor - License #2024110

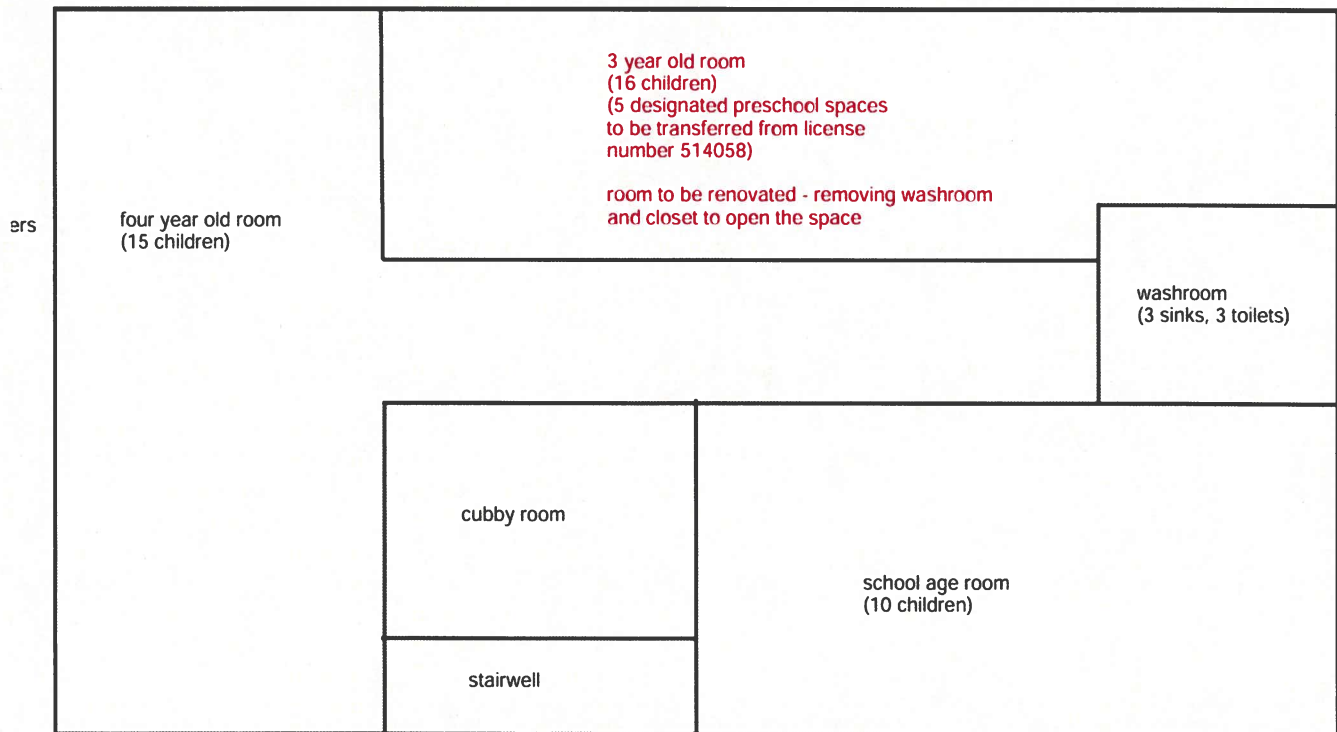
29 designated spaces + 3 designated infant spaces transferred from license #514058



Main Floor / Rez-de-chaussée

Top Floor - License #

10 school age licensed spaces + 25 designated spaces + 5 designated spaces transferred from license #514058



Second Floor / Premier étage

Floor Plans / Plans d'étage



Community Planning
Planification urbaine

Map \ carte # IV

File \ fiche: PR-68-2025

Date \ date: août \ August 20, 2025

Subject \ sujet: prom 148 Mataya Drive

Creative Childcare in Pepper Creek Inc.
(c/o Javed Khan)

From: [Éric Boudreau](#)
To: [PLANNING AND DEVELOPMENT](#)
Subject: Comments Regarding Creative Child Care in Pepper Creek
Date: Wednesday, August 20, 2025 11:59:25 AM

External email: Do not follow instructions, click links, open attachments, forward or respond to the email unless you recognize the sender and know the content is safe.

Here are some concerns about the expansion of the day care center:

1. Increase in noise in a residential neighbourhood;
2. Increase in traffic at peak circulation times, close to a busy intersection;
3. Potential negative impact on surrounding wells because of an increase in water usage;
4. Proposed 11 space parking spot close to property line. What happens with snow removal?

BUILDING INSPECTION REPORT FOR July 2025

	<u>PERMITS</u>	<u>VALUE</u>	<u>VALUE OF CONST.</u> <u>YEAR TO DATE</u>	<u>SAME PERIOD</u> <u>LAST YEAR</u>
<u>RESIDENTIAL</u>				
<u>SINGLE DETACHED DWELLING</u>				
New	15	\$5,026,000	\$20,130,500	\$21,831,074
Additions	7	\$408,000	\$1,674,175	\$1,505,000
<u>MINI HOMES</u>				
New	3	\$360,000	\$1,412,000	\$1,420,800
Additions	-	-	-	-
<u>SEMI-DETACHED / DUPLEX</u>				
New	2	\$1,076,000	\$4,138,000	\$8,325,280
Additions	-	-	-	-
<u>SECONDARY DWELLING UNIT</u>				
New Basement Apt	-	-	\$465,000	\$170,000
New Accessory Apt	1	\$143,000	\$230,000	-
New Garden Apt	-	-	\$30,000	-
New Garage Apt	-	-	-	-
<u>TOWNHOUSES</u>				
New	1	\$1,040,000	\$1,647,000	\$2,590,400
Additions	-	-	-	-
<u>APARTMENT BLDG.</u>				
New	1	\$16,500,000	\$135,362,712	\$23,985,813
Additions	-	-	\$16,000	-
<u>3 - 4 UNIT APARTMENT BUILDING</u>				
New	-	-	-	-
Additions	-	-	\$250,000	-
<u>ACCESSORY BLDG</u> * Includes Development Permits				
Storage Building *	11	\$72,950	\$616,465	\$350,946
Carport/Garage *	7	\$184,250	\$629,250	\$619,357
Swimming Pool	7	\$193,000	\$1,083,265	\$802,205
Deck	7	\$36,500	\$154,100	\$484,200
<u>RENOV/REPAIRS</u>				
	39	\$875,385	\$5,786,206	\$14,770,614
<u>TOTAL RESIDENTIAL</u>	101	\$25,915,085	\$173,624,673	\$76,855,689

	<u>PERMITS</u>	<u>VALUE</u>	<u>VALUE OF CONST.</u> <u>YEAR TO DATE</u>	<u>SAME PERIOD</u> <u>LAST YEAR</u>
<u>COMMERCIAL</u>				
New	1	\$1,100,000	\$14,203,000	\$7,337,360
Additions	-	-	\$1,314,000	\$3,243,086
Renov/Repairs	5	\$488,000	\$12,606,649	\$21,548,589
<u>INDUSTRIAL</u>				
New	-	-	-	-
Additions	-	-	-	-
Renov/Repairs	1	\$1,281,000	\$1,502,500	-
<u>GOVERN (Mun.)</u>				
New	-	-	-	\$527,500
Additions	-	-	-	-
Renov/Repairs	1	\$688,725	\$748,725	\$132,518
<u>GOVERN (Fed/Prov)</u>				
New	-	-	-	\$73,030,000
Additions	-	-	-	\$413,000
Renov/Repairs	-	-	\$1,469,000	\$4,486,300
<u>INSTITUTIONAL</u>				
New	1	\$61,000	\$61,000	\$66,300,000
Additions	1	\$113,000	\$333,000	-
Renov/Repairs	4	\$1,934,314	\$3,283,224	\$10,526,148
Others	-	-	\$19,000	-
<u>DEMOLITION</u>				
	2	-	-	-
TOTALS	117	\$31,581,124	\$209,164,771	\$264,400,190

(\$16,040,782 - July 2024)

DEMOLITIONS

181 Burpee Street - Accessory Building

203 Carney Street - Detached Garage

NUMBER OF NEW DWELLING UNITS

	July 2025	THIS YEAR TO DATE	July 2024	SAME PERIOD LAST YEAR
SINGLE DETACHED DWELLING	17	58	8	66
SEMI-DETACHED / DUPLEX	4	17	0	24
TINY / MINI HOMES	3	12	2	13
SECONDARY DWELLING UNIT	1	14	3	9
APARTMENT UNITS	115	717	42	206
TOWNHOUSE UNITS	11	15	4	9
TOTAL	151	833	59	327

NUMBER OF PERMITS ISSUED**BUILDING PERMITS**

<u>July 2025</u>	<u>TO DATE</u>
101	471

PLUMBING PERMITS

<u>July 2025</u>	<u>TO DATE</u>
42	243

SIGN PERMITS

<u>July 2025</u>	<u>TO DATE</u>
5	54

DEVELOPMENT PERMITS

<u>July 2025</u>	<u>TO DATE</u>
16	56

FEES FOR BUILDING PERMITS ISSUED

<u>July 2025</u>	<u>TO DATE</u>	<u>July 2024</u>	<u>SAME PERIOD LAST YEAR</u>
\$257,972.00	\$1,693,951.02	\$137,234.00	\$2,143,018.00

FEES FOR PLUMBING PERMITS ISSUED

<u>July 2025</u>	<u>TO DATE</u>	<u>July 2024</u>	<u>SAME PERIOD LAST YEAR</u>
\$32,120.00	\$199,100.00	\$16,940.00	\$110,280.00

FEES FOR SIGN PERMITS

<u>July 2025</u>	<u>TO DATE</u>	<u>July 2024</u>	<u>SAME PERIOD LAST YEAR</u>
\$625.00	\$6,525.00	\$500.00	\$7,500.00

FEES FOR DEVELOPMENT PERMITS

<u>July 2025</u>	<u>TO DATE</u>	<u>July 2024</u>	<u>SAME PERIOD LAST YEAR</u>
\$1,500.00	\$5,700.00	\$400.00	\$3,150.00

TOTAL FEES - BUILDING/PLUMBING/SIGNS

<u>July 2025</u>	<u>TO DATE</u>	<u>July 2024</u>	<u>SAME PERIOD LAST YEAR</u>
\$292,217.00	\$1,905,276.02	\$155,074.00	\$2,263,948.00

SUBMITTED BY:

APPROVED BY:





Growth & Community Services Department

Building Inspections Division
397 Queen St, P.O. Box 130
FREDERICTON, NB E3B 4Y7
Phone: 460-2029 / Fax: 460-2126

Issued Building Permits
Sorted by Structure Type

July 2025

Permit #	PID	Issue	Project Location	Applicant	Ward	Structure Type:	Type of Work:	Permit Fee	Value	Description
2025BP0453	01412501	Jul-04-25	181 Burpee St	Yvonne Bland	Ward 1	Accesory Building	Demolition	\$50.00	\$0.00	Demolish existing 12'x20' accessory building for an SDD as per plans submitted.
2025BP0496	75507558	Jul-25-25	705 Clements Dr	GreenFoot Energy Solutions	Ward 1	Accesory Building	New	\$290.00	\$30,000.00	Install a 56'x12' ground mount solar panels for an SDD as per plans submitted.
2025BP0152	01500461	Jul-18-25	80 Sami St	Cedar Valley Investments Ltd.	Ward 12	Apartment Building	New	\$132,050.00	\$16,500,000.00	Construct a new 110-unit apartment building as per plans submitted
2025BP0275	75447987	Jul-30-25	130 Lian St	Prospect Building Contractors (2004) Ltd.	Ward 9	Apartment Building	Renovation	\$434.00	\$48,000.00	Construct renovation to repair columns on a condo building as per plans submitted.
2025BP0382	01551175	Jul-03-25	76 River St	Van Wart Management Ltd.	Ward 5	Apartment Building	Renovation	\$74.00	\$3,000.00	Construct a rear exit stair for a 4-unit apartment building as per plans submitted
2025BP0462	01428960	Jul-23-25	733 Irvine St	Scott Dayton	Ward 6	Apartment Building	Renovation	\$146.00	\$12,000.00	Construct renovation to replace foundation drain tile for a 5-unit apartment building as per plans submitted.
2025BP0462	01428960	Jul-23-25	733 Irvine St	White Rock Developments	Ward 6	Apartment Building	Renovation	\$146.00	\$12,000.00	Construct renovation to replace foundation drain tile for a 5-unit apartment building as per plans submitted.
2025BP0510	01459197	Jul-24-25	355 Saint John St	Neil Wishart	Ward 11	Apartment Building	Renovation	\$250.00	\$25,000.00	Construct renovation to replace a roofing for a 3 unit apartment as per information submitted.
2025BP0523	75402875	Jul-24-25	175 Reynolds St	M.C.I. Hodgin Construction	Ward 2	Apartment Building	Renovation	\$66.00	\$1,500.00	Construct renovation to replace the two lower support posts for the balconies of the unit #8 as per photo attached.
2023BP0852	01500800	Jul-29-25	1715 Woodstock Rd	Laureat Pepin Inc.	Ward 12	Commercial Building	Renovation	\$2,338.00	\$286,000.00	Construct interior renovation to existing gas station including a new barrier-free washroom as per plans submitted.
2025BP0162	75396549	Jul-22-25	1033 Prospect St	Hot N Spice Inc	Ward 9	Commercial Building	Renovation	\$530.00	\$30,000.00	To construct a fit up renovation for a new restaurant as per the drawings submitted.
2025BP0264	75422451	Jul-17-25	85 Woodside Ln	WPM Holdings Inc.	Ward 9	Commercial Building	New	\$8,850.00	\$1,100,000.00	Construct new commercial building for audiology clinic fit-up and 4 additional vacant suites as per plans submitted.
2025BP0459	01441559	Jul-23-25	1149 Smythe St	Today's Homes Northrup Homes	Ward 9	Commercial Building	Renovation	\$202.00	\$19,000.00	Construct renovation to reconfigure for 6 offices, 1 meeting room and a reception area on the lower level of the commercial building as per plans submitted.
2025BP0497	75367581	Jul-29-25	570 Queen St	The Armour Group	Ward 11	Commercial Building	Renovation	\$1,130.00	\$135,000.00	Construct renovation to fit-up a law office on the 3rd floor of a commercial building, as per plans submitted
2025BP0539	01442904	Jul-28-25	351 Aberdeen St	RW Morton Construction Inc.	Ward 10	Commercial Building	Renovation	\$194.00	\$18,000.00	Construct renovation to replace siding on driveway side of the daycare as per information submitted.
2025BP0364	75560623	Jul-17-25	21 Main St	White Rock Developments Ltd.	Ward 4	Commercial/Residential	Renovation	\$1,970.00	\$120,000.00	Construct renovation to create 5 additional dwelling units to existing commercial/residential building as per information submitted.
2024BP0469	01512706	Jul-31-25	56 Fairview Dr	Bicknell's Roofing	Ward 12	Deck	Renovation	\$114.00	\$8,000.00	Construct a 9' x 16' covered deck attached to an SDD as per plans submitted
2024BP0624	01447200	Jul-28-25	271 Victoria St	Government of New Brunswick	Ward 10	Deck	Renovation	\$74.00	\$2,500.00	Construct a 4' x 11' attached deck on an SDD as per plans submitted
2025BP0339	75003327	Jul-02-25	19 Hatheway St	Custom Carpentry By Mark Inc.	Ward 7	Deck	New	\$74.00	\$3,000.00	Construct a 3' x 6.5' front deck on SDD as per plans submitted.
2025BP0451	75391151	Jul-08-25	173 McGibbon St	Conrad Gaudet	Ward 7	Deck	New	\$122.00	\$9,000.00	Construct a 11'x15' attached deck replacement for an SDD as per plans submitted.
2025BP0452	75473553	Jul-09-25	593 Brookside Dr	Gullison Flooring and Renovations	Ward 2	Deck	New	\$90.00	\$4,500.00	Construct a 8' x 12' attached deck for an SDD as per plans submitted
2025BP0479	75422394	Jul-17-25	43 Red Maple Crt	Distinctive Sunrooms & Patio Enclosures	Ward 9	Deck	New	\$74.00	\$3,000.00	Construct detached deck to serve sunroom under permit 2025BP0084 as per information submitted.
2025BP0489	01492685	Jul-22-25	48 Cowperthwaite St	JT Builders Inc	Ward 5	Deck	New	\$106.00	\$6,500.00	Construct a 12'x12' attached deck for an SDD as per plans submitted
2025BP0433	75568253	Jul-09-25	41 Sandlewood Ln	S. Dykeman & Son Endeavors Inc.	Ward 1	Detached Garage	New	\$290.00	\$30,000.00	Construct detached garage to serve SDD as per information submitted.
2025BP0460	01426113	Jul-07-25	203 Carney St	Daniel Caissie	Ward 6	Detached Garage	Demolition	\$50.00	\$0.00	Demolish detached garage and associated slab as per aerial view submitted.
2025BP0324	01466655	Jul-14-25	1050 College Hill Rd	King Construction Ltd.	Ward 11	Industrial	Renovation	\$10,298.00	\$1,281,000.00	Construct a renovation to the existing ventilation system as per drawings submitted
2025BP0306	75325571	Jul-16-25	1371 Lincoln Rd	L. Sanford and Sons Ltd.	Ward 7	Institutional	New	\$1,076.00	\$61,000.00	Construct sewage lift station as per plans submitted.
2025BP0338	01463660	Jul-08-25	692 Montgomery St	Prospect Building Contractors (2004) Ltd.	Ward 11	Institutional	Addition	\$954.00	\$113,000.00	Construct a connecting link for a modular classroom for the Montgomery Street School as per plans submitted
2025BP0424	01470848	Jul-08-25	2 Garland Crt	Dowd Roofing Inc.	Ward 11	Institutional	Renovation	\$2,290.00	\$279,950.00	Construct renovation to replace built-up roofing on east half of Incutech Building #1 (UNB) as per information submitted.
2025BP0447	01446319	Jul-08-25	497 Queen St	Vintage Brick & Stone Inc.	Ward 11	Institutional	Renovation	\$4,442.00	\$548,600.00	Construct renovation to repoint brick, repair concrete sills/headers and reseal all windows and doors for the NBCCD building as per plans submitted.
2025BP0502	01470848	Jul-22-25	21 Pacey Dr	Bird Construction Group	Ward 11	Institutional	Renovation	\$8,178.00	\$1,015,764.00	Construct renovation to existing Social Club area in Student Union Building to office space as per plans submitted.
2025BP0512	01470848	Jul-24-25	9 MacAuley Ln	Paar Renovations Ltd.	Ward 11	Institutional	Renovation	\$770.00	\$90,000.00	Construct renovation to an existing classroom #224 at UNB as per plans submitted.
2025BP0464	75568469	Jul-08-25	95 Burns St	Today's Homes Northrup Homes	Ward 2	Mini Home	New	\$1,010.00	\$120,000.00	Install a 16'x74' mini home as per plans submitted
2025BP0469	75568360	Jul-14-25	59 Fearnley St	Today's Homes Northrup Homes	Ward 2	Mini Home	New	\$1,010.00	\$120,000.00	Install a 67' x 16' mini-home as per plans submitted.
2025BP0524	75568402	Jul-28-25	35 Fearnley St	Today's Homes Northrup Homes	Ward 2	Mini Home	New	\$1,010.00	\$120,000.00	Install a 16'x74' mini home as per plans submitted
2025BP0457	01446244	Jul-23-25	311 Queen St	Julmac Contracting Ltd.	Ward 10	Municipal	Renovation	\$5,562.00	\$688,725.00	Construct renovation to repair the parking surface above the parkade of the Fredericton Police Station as per plans submitted.
2025BP0444	75382721	Jul-07-25	1583 Lincoln Rd	Robert Curtis	Ward 7	Pool	New	\$210.00	\$20,000.00	Install a 21' above-ground swimming pool and detached deck for an SDD as per plans submitted
2025BP0456	01502426	Jul-14-25	16 Riverside Crt	Donnie Hynes	Ward 12	Pool	New	\$234.00	\$23,000.00	Install a 15' diameter pool, construct a 4'x12' pool deck for an SDD as per plans submitted.
2025BP0463	75449447	Jul-14-25	54 Jackstraw Crt	Derek Foster	Ward 12	Pool	New	\$450.00	\$50,000.00	Construct a new 13'x26' inground fiberglass pool as per information submitted.
2025BP0471	75464750	Jul-16-25	185 Gilridge Dr	Gary Godfrey	Ward 1	Pool	New	\$130.00	\$10,000.00	Install pool to serve SDD as per information submitted.
2025BP0542	75436279	Jul-30-25	3 Emma Crt	Stephane Esculier	Ward 2	Pool	New	\$210.00	\$20,000.00	Install pool to serve SDD as per information submitted.
2025BP0543	75454140	Jul-30-25	148 Randolph St	Rene Boudreau	Ward 2	Pool	New	\$370.00	\$40,000.00	Install new pool to serve SDD as per information submitted.
2025BP0545	75133223	Jul-30-25	530 Fulton Ave	Sean O'Shea	Ward 3	Pool	New	\$290.00	\$30,000.00	Install a 18' diameter pool to serve SDD as per information submitted.
2025BP0379	75558247	Jul-30-25	34 Leavery St	Martell Custom Homes	Ward 2	Semi Detached	New	\$4,258.00	\$526,000.00	Construct a semi-detached dwelling for future subdivision as per plans submitted
2025BP0445	75557876	Jul-14-25	76 Dewitt Acres	Gerges Developments LTD	Ward 6	Semi Detached	New	\$4,450.00	\$550,000.00	Construct new semi-detached dwelling as per plans submitted for subdivision into separate parcels.
2025BP0200	01413574	Jul-03-25	95 Embleton Ave	R.I Builders	Ward 1	Single Detached Dwelling	New	\$1,650.00	\$200,000.00	Construct new SDD (with unfinished basement) as per plans submitted.
2025BP0208	01452473	Jul-17-25	79 Pembroke Cres	Brian Milthorp	Ward 9	Single Detached Dwelling	Renovation	\$210.00	\$20,000.00	Construct renovation to re-locate kitchen and re-configure bathroom as per floor plan submitted.
2025BP0273	75500348	Jul-03-25	735 Wetmore Rd	Lynne Theriault Sehgal	Ward 8	Single Detached Dwelling	Renovation	\$306.00	\$32,000.00	Construct renovation to SDD for attached deck as per information submitted.
2025BP0340	01408665	Jul-07-25	108 Longwood Dr	Birdseye Custom Carpentry Ltd	Ward 3	Single Detached Dwelling	Renovation/Addition	\$810.00	\$95,000.00	Construct renovation and addition to SDD as per plans submitted.
2025BP0343	75550533	Jul-08-25	39 Winterberry Dr	Tosin Aina	Ward 7	Single Detached Dwelling	New	\$3,250.00	\$400,000.00	Construct new SDD (unfinished basement) as per plans submitted.
2025BP0362	75553768	Jul-07-25	29 Sylva Crt	Sherico Developments Ltd.	Ward 7	Single Detached Dwelling	New	\$2,450.00	\$300,000.00	Construct new SDD as per information submitted.
2025BP0367	01503705	Jul-10-25	17 Robin Dr	McKinley Builders	Ward 12	Single Detached Dwelling	Renovation/Addition	\$530.00	\$60,000.00	Construct an addition onto SDD as per plans submitted.
2025BP0398	01429653	Jul-02-25	723 MacLaren Ave	Janzen Roofing	Ward 6	Single Detached Dwelling	Addition	\$690.00	\$80,000.00	Construct 16' x 25' addition onto SDD as per plans submitted.
2025BP0401	01509660	Jul-31-25	1435 Woodstock Rd	Chris Boulter	Ward 12	Single Detached Dwelling	Addition	\$930.00	\$110,000.00	Construct a front addition to the SDD as per plans submitted.

2025BP0412	01410166	Jul-03-25	112 Leeland Way	Jill Durling	Ward 4	Single Detached Dwelling	New	\$250.00	\$25,000.00	Construct phase one (foundation) of SDD as per plans submitted.
2025BP0412	75553180	Jul-03-25	112 Leeland Way	Jill Durling	Ward 4	Single Detached Dwelling	New	\$250.00	\$25,000.00	Construct phase one (foundation) of SDD as per plans submitted.
2025BP0415	75390609	Jul-03-25	18 Shore St	D.B. Phillips Builders Inc.	Ward 11	Single Detached Dwelling	Renovation	\$82.00	\$4,000.00	Construct renovation to replace from exterior steps as per information submitted.
2025BP0421	01407394	Jul-24-25	99 Autumn St	Golden Star Developments Inc	Ward 3	Single Detached Dwelling	Renovation	\$170.00	\$15,000.00	Construct renovation to replace drain tile on existing SDD as per information submitted.
2025BP0422	75499608	Jul-23-25	15 Arbor Dr	Kent Homes	Ward 5	Single Detached Dwelling	New	\$1,730.00	\$210,000.00	Construct new SDD (unfinished basement) as per plans submitted.
2025BP0426	75565606	Jul-18-25	45 DeMerchant Dr	Habitat for Humanity Fredericton Area	Ward 4	Single Detached Dwelling	Renovation	\$146.00	\$12,000.00	Construct renovation to finish basement of SDD under permit 2024BP0311 as per plans submitted.
2025BP0428	01459098	Jul-08-25	261 Saint John St	Curtis McCarthy	Ward 11	Single Detached Dwelling	Renovation	\$82.00	\$1,500.00	Construct renovation to replace front exterior door and replace small portion of siding as per information submitted.
2025BP0431	75568253	Jul-08-25	41 Sandlewood Ln	S. Dykeman & Son Endeavors Inc.	Ward 1	Single Detached Dwelling	New	\$2,050.00	\$250,000.00	Construct new SDD (unfinished basement) as per plans submitted.
2025BP0439	01500461	Jul-07-25	36 Stacho St	BL McLean Rentals	Ward 12	Single Detached Dwelling	New	\$3,250.00	\$400,000.00	Construct new SDD (finished basement) as per plans submitted.
2025BP0448	75422600	Jul-07-25	198 Brookside Dr	Targett's Windows and Doors	Ward 3	Single Detached Dwelling	Renovation	\$122.00	\$9,000.00	Construct renovation to replace 6 windows (3 br) in SDD as per information submitted.
2025BP0449	75003988	Jul-03-25	39 Pugsley St	NWB Custom Creations Inc	Ward 7	Single Detached Dwelling	Addition	\$74.00	\$3,000.00	Construct renovation to build canopy addition on rear of SDD as per plans submitted.
2025BP0454	75473553	Jul-09-25	593 Brookside Dr	Capital City Roofing (2007) Ltd.	Ward 2	Single Detached Dwelling	Renovation	\$210.00	\$20,000.00	Construct renovation to replace all windows, two doors and vinyl siding on an SDD as per information submitted
2025BP0455	75328807	Jul-08-25	72 Edward St	Targetts Window and Door	Ward 7	Single Detached Dwelling	Renovation	\$122.00	\$9,000.00	Construct renovation to replace existing front door and garage entry door for an SDD as per plans submitted.
2025BP0458	01425461	Jul-07-25	29 Gulliver Dr	Stephanie Calhoun	Ward 6	Single Detached Dwelling	Renovation	\$146.00	\$12,000.00	Construct renovation to replace drain tile for an SDD as per information submitted.
2025BP0461	75559799	Jul-14-25	48 Sandlewood Ln	M.C.I. Hodgins Construction	Ward 1	Single Detached Dwelling	New	\$3,050.00	\$375,000.00	Construct new SDD with unfinished basement as per plans submitted
2025BP0466	75513622	Jul-21-25	356 Neill St	Andrew Cormier	Ward 6	Single Detached Dwelling	New	\$2,850.00	\$350,000.00	Construct new SDD as per plans submitted
2025BP0473	01468057	Jul-17-25	106 Grey St	Aaron Taylor	Ward 11	Single Detached Dwelling	Renovation	\$154.00	\$13,000.00	Construct renovation to replace 18 windows (3 bedrooms) on first two storeys of existing SDD as per information submitted.
2025BP0474	01430586	Jul-16-25	267 Medley St	Paul Arseneault	Ward 6	Single Detached Dwelling	Renovation	\$650.00	\$75,000.00	Construct renovation to replace existing porch, windows and doors (3 bedrooms), install 1 1/2" rigid foam and vinyl siding.
2025BP0477	75492124	Jul-29-25	181 Country Wood Ln	XOLAR INC	Ward 6	Single Detached Dwelling	Renovation	\$370.00	\$40,000.00	Construct the installation of roof mounted solar array on SDD as per information submitted.
2025BP0478	01494871	Jul-14-25	267 Longwood Dr	Jessica Storey	Ward 3	Single Detached Dwelling	Renovation	\$98.00	\$5,100.00	Construct renovation to remove and replace existing deck as per plans submitted.
2025BP0480	75008169	Jul-14-25	6 Ascot Crt	Sunly Energy Corp	Ward 8	Single Detached Dwelling	Renovation	\$226.00	\$21,021.00	Install a solar PV system for an SDD as per plans submitted.
2025BP0482	01401876	Jul-11-25	292 Sunset Dr	Brent Allan Hachey	Ward 1	Single Detached Dwelling	Renovation	\$82.00	\$1,400.00	Construct renovation to replace bay window and portion of framing as per information submitted.
2025BP0484	01446848	Jul-17-25	379 Saunders St	Wolfgang Handwerk	Ward 10	Single Detached Dwelling	Renovation/Addition	\$130.00	\$10,000.00	Construct renovation to SDD to build roofed over entry as per plans submitted.
2025BP0487	01494202	Jul-24-25	32 Bates St	Kraniel Construction	Ward 3	Single Detached Dwelling	Addition	\$450.00	\$50,000.00	Construct renovation to replace existing deck with a 13'8"x14' covered deck and a 8'x12' mudroom addition for an SDD as per plans submitte
2025BP0488	01462027	Jul-28-25	606 Kitchen St	Tyler Lynn	Ward 11	Single Detached Dwelling	Renovation	\$146.00	\$12,000.00	Construct renovation to replace drain tile on existing SDD as per information submitted.
2025BP0490	01477116	Jul-23-25	50 Ashfield St	Eric Megarity	Ward 6	Single Detached Dwelling	Renovation	\$186.00	\$16,800.00	Construct renovation to repair the front portion of an existing garage foundation for an SDD as per information submitted.
2025BP0491	75529016	Jul-14-25	797 Woodstock Rd	Esson Construction Ltd.	Ward 12	Single Detached Dwelling	Renovation	\$114.00	\$8,000.00	Construct 2nd floor bathroom renovation to SDD - no fixture change.
2025BP0495	01439082	Jul-29-25	123 Beechwood Cres	Samira Kohdarehei	Ward 9	Single Detached Dwelling	Renovation	\$290.00	\$30,000.00	Construct renovation to finish basement of SDD as per information submitted.
2025BP0498	75025569	Jul-16-25	11 White Crt	Shift Energy Group Inc.	Ward 5	Single Detached Dwelling	Renovation	\$282.00	\$28,434.00	Install solar panel array on an SDD as per plans submitted.
2025BP0499	01467976	Jul-21-25	123 Grey St	Shift Energy Group Inc.	Ward 11	Single Detached Dwelling	Renovation	\$338.00	\$35,201.00	Install solar panel array on an SDD as per plans submitted.
2025BP0500	75171892	Jul-18-25	57 Carrington Ln	Shift Energy Group Inc.	Ward 3	Single Detached Dwelling	Renovation	\$330.00	\$34,597.00	Install solar panel array on an SDD as per plans submitted.
2025BP0501	75479428	Jul-18-25	67 Ridgeline Cres	Shift Energy Group Inc.	Ward 9	Single Detached Dwelling	Renovation	\$306.00	\$31,312.00	Install solar panel array on an SDD as per plans submitted.
2025BP0503	75393694	Jul-18-25	99 Mataya Dr	Epic Energy Solutions Ltd	Ward 6	Single Detached Dwelling	Renovation	\$346.00	\$36,978.00	Install roof mounted solar panel array on an SDD as per plans submitted.
2025BP0506	75513812	Jul-31-25	13 Geyssen Crt	Rob Stairs	Ward 1	Single Detached Dwelling	New	\$3,490.00	\$430,000.00	Construct a new SDD with unfinished basement as per plans submitted.
2025BP0516	75556977	Jul-29-25	29 Donmac Ct	R. Foster Developments Ltd.	Ward 2	Single Detached Dwelling	New	\$4,050.00	\$500,000.00	Construct new SDD (unfinished basement) as per plans submitted.
2025BP0532	75495143	Jul-29-25	764 Route 10 Hwy	Dead End Auto	Ward 6	Single Detached Dwelling	New	\$2,666.00	\$327,000.00	Construct a new SDD as per plans submitted.
2025BP0210	01448166	Jul-11-25	263 York St	Molly Furzer	Ward 10	Single Detached Dwelling/Ap	New	\$2,602.00	\$319,000.00	Construct a new SDD (including a secondary suite under 80 square meters) as per drawings submitted.
2025BP0212	01459668	Jul-25-25	517 Tweedsmuir St	Colpitts Developments	Ward 11	Single Detached Dwelling/Ap	New	\$2,770.00	\$340,000.00	Construct new SDD with secondary suite as per plans submitted.
2025BP0347	01527803	Jul-02-25	19 Emerald St	Donald McCarty	Ward 1	Single Detached Dwelling/Ap	Renovation	\$146.00	\$12,000.00	Construct renovation to create a second dwelling unit (secondary suite) as per plans submitted.
2025BP0370	75000935	Jul-17-25	44 Burnham Crt	Trudy Donahue	Ward 12	Single Detached Dwelling/Ap	Addition	\$1,194.00	\$143,000.00	Construct a 2nd floor addition over the existing attached garage to be a secondary suite in the SDD as per plans submitted
2025BP0383	01459411	Jul-08-25	240 Saint John St	A.Totallee Home Renovations Ltd.	Ward 11	Single Detached Dwelling/Ap	Renovation	\$754.00	\$87,150.00	Construct renovation to replace kitchen cabinetry, one kitchen window and flooring to an SDD as per plans submitted
2025BP0432	01459411	Jul-07-25	240 Saint John St	A.Totallee Home Renovations Ltd.	Ward 11	Single Detached Dwelling/Ap	Renovation	\$194.00	\$17,892.00	Construct renovation to install new kitchen/flooring, add second layer of 1/2" drywall to bedroom wall, and upgrading interior stair railing to NB
2025BP0486	75562819	Jul-24-25	48 Salisbury Cres	R. Foster Developments Ltd.	Ward 8	Single Detached Dwelling/Ap	New	\$4,850.00	\$600,000.00	Construct new SDD with basement apartment (Secondary Suite) as per plans submitted.
2025BP0395	01454727	Jul-08-25	515 Albert St	Francis Morissette	Ward 10	Townhouse	Renovation/Addition	\$74.00	\$3,000.00	Construct new fire escape for multi-unit townhouse as per plans submitted.
2025BP0396	01454727	Jul-08-25	515 Albert St	Francis Morissette	Ward 10	Townhouse	Renovation	\$74.00	\$3,000.00	Construct renovation to replace portion of drain tile on multi-unit townhouse as per information submitted.
2025BP0407	75459735	Jul-08-25	62 Huntingdon Cir	Jon Lownds	Ward 8	Townhouse	Renovation	\$82.00	\$4,000.00	Construct a 10 x 10 attached deck (same size as existing) for a townhouse dwelling as per plans submitted
2025BP0443	75426742	Jul-25-25	610 Saint Mary's St	Colpitts Developments	Ward 4	Townhouse	New	\$8,370.00	\$1,040,000.00	Construct new 11-unit townhouse as per plans submitted.
2025BP0514	75434761	Jul-27-25	10 Westbrook Ln	Nicole Sawler	Ward 2	Townhouse	Renovation	\$82.00	\$3,500.00	Construct renovation to replace a 10'x8' attached deck for a townhouse as per plans submitted.
TOTALS								\$258,222.00	\$31,408,924.00	